

**Finding of No Significant Impact
and
Programmatic Section 4(f) Evaluation
Section 4(f) De Minimis Finding**

State Route 115 (U.S. 129, Alcoa Highway)
From I-140 (Pellissippi Parkway) to
North of the Cherokee Trail Interchange
Blount and Knox Counties, Tennessee

Submitted pursuant to 42 U.S.C. 4332(2)(C)

by the

U.S. Department of Transportation
Federal Highway Administration, Tennessee Division

and

Tennessee Department of Transportation
Environmental Division

Cooperating Agencies:

Tennessee Valley Authority
U.S. Army Corps of Engineers, Nashville District
U.S. Coast Guard, Eighth Coast Guard District

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The FHWA has determined that the selected build alternative will not have any significant impact on the human environment. This Finding of No Significant Impact is based on the approved Environmental Assessment (EA), which has been independently evaluated by the FHWA and determined to adequately and accurately discuss the need, environmental issues, and impacts of the proposed project and appropriate mitigation measures. It provides sufficient evidence and analysis for determining that an Environmental Impact Statement is not required. The FHWA takes full responsibility for the accuracy, scope, and content of the EA approved on February 27, 2013. This FONSI should not be reviewed independent of the approved EA.

7 March 2014

Date of Approval

Pamela M. W. Westbrook

Federal Highway Administration

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EXECUTIVE SUMMARY

The Tennessee Department of Transportation (TDOT) proposes to widen a section of State Route 115/U.S. 129/Alcoa Highway (SR-115, hereinafter) between Interstate 140 (I-140)/Pellissippi Parkway (Pellissippi Parkway, hereinafter) in Blount County and the Cherokee Trail Interchange in Knox County, a distance of 8.4 miles. The proposed project is intended to address transportation needs that have been identified through coordination with local officials, agencies, and the public. The purpose of the proposed project is to:

- Increase the capacity of SR-115 and the level of service,
- Correct roadway deficiencies, and
- Reduce crashes/improve safety.

This Finding of No Significant Impact (FONSI) document has been prepared pursuant to the National Environmental Policy Act (NEPA). On February 27, 2013, the Federal Highway Administration (FHWA) approved a NEPA Environmental Assessment (EA).

Outreach

A NEPA public hearing was held in the project area for the proposed SR-115 improvement project on June 20, 2013. Approximately 220 members of the public and local officials attended. The purpose of the hearing was to inform the public of the findings of the EA and to provide the public with an opportunity for input into the project. This input was intended to assist TDOT in the selection of an alternative. TDOT presented a No-Build Alternative and one Build Alternative at the hearing. The project has also been coordinated with local government, state and federal agencies and other interested parties.

Alternatives

Existing SR-115 within the project limits is primarily a four-lane roadway with a mixture of at-grade intersections and grade separations. The nature of the proposed project (i.e., widening of an existing roadway) limits the options for build alternatives. Two alternatives were considered in the EA, a single build alternative and the No-Build Alternative. The No-Build Alternative was not selected by TDOT because it does not meet the proposed project's purpose and need.

The Build Alternative (Selected Alternative, hereinafter) follows the existing alignment and consists of widening the existing facility to six lanes (three lanes in each direction), except for the section between Singleton Station Road and Topside Road (SR-333), where it will be widened to accommodate three travel lanes in each direction and two auxiliary lanes. Seven interchanges, a series of collector/frontage roads, and a center median barrier will be constructed along the route. Two sets of new parallel bridges will be constructed adjacent to I.C. King Park.

Environmental Impacts

While the Selected Alternative would have potential adverse impacts, the analyses undertaken for the EA and the FONSI did not identify any significant environmental impacts. The Selected Alternative would have beneficial impacts to transportation, safety and to bicycles and pedestrians.

Areas of Controversy and Unresolved Issues

There are no known major areas of controversy or unresolved issues related to the proposed highway improvement.

Other Major Actions

Two other programmed projects are in the project area. The first is the proposed Alcoa Highway Bypass project. This proposed roadway will bypass a section of SR-115 to the east and is proposed from Hall Road (SR-35) on the south, across Pellissippi Parkway and ending on the north near Singleton Station Road, which is within the proposed SR-115 project area. Right-of-Way plans are currently under development for the section from Hall Road to the proposed interchange serving McGhee Tyson Airport. This section is south of Pellissippi Parkway and south of the proposed SR-115 project.

The second project is the proposed Pellissippi Parkway Extension. This proposed project would extend Pellissippi Parkway east on a new four-lane route from its existing eastern terminus at SR-33/Old Knoxville Highway to Lamar Alexander Parkway (US 31/SR-73).

Commitments

The project includes commitments to mitigate impacts to I.C. King Park, to avoid or minimize impacts to the federally-endangered Indiana bat and to conduct a survey to identify asbestos-containing materials on any bridges that are to be modified or demolished.

Required Federal Actions

The following permits may be required:

- Aquatic Resource Alteration Permits (ARAP),
- Section 401 Water Quality Certification,
- Section 9 and Section 10 Navigable Waterways permits from the US Coast Guard,
- Individual or Nationwide Section 404 USACE permits,
- Tennessee Valley Authority (TVA) 26a permit or letter of no objection,
- National Pollutant Discharge Elimination System permit,
- General or individual ARAP permits, and
- Nationwide or individual permits from the USACE pursuant to Section 404 of the Clean Water Act.

Statute of Limitations

The FHWA may publish a notice in the *Federal Register*, pursuant to 23 U.S.C. 139(I), indicating that one or more Federal agencies have taken final actions on permits, licenses, or approvals for a transportation project. If such notice is published, claims seeking judicial review of those Federal agency actions will be barred unless such claims are filed within 150 days after the date of publication of the notice, or within such shorter time period as is specified in the Federal laws pursuant to which judicial review of the Federal agency action is allowed. If no notice is published, then the periods of time that otherwise are provided by the Federal laws governing such claims will apply.

LIST OF ACRONYMS

AADT	Average Annual Daily Traffic
APR	Advance Planning Report
ARAP	Aquatic Resource Alteration Permit
BMP	Best Management Practices
CBER	Center for Business and Economic Research
CSRP	Conceptual Stage Relocation Plan
DBH	Diameter at Breast Height
EA	Environmental Assessment
EJ	Environmental Justice
FHWA	Federal Highway Administration
FONSI	Finding of No Significant Impact
IAC	Interagency Consultation Group
LRTP	Long Range Transportation Plan
MPC	Metropolitan Planning Commission
MSAT	Mobile Source Air Toxics
NEPA	National Environmental Policy Act
NPDES	National Pollutant Discharge Elimination System
NRHP	National Register of Historic Places
ROW	Right-of-Way
SHPO	State Historic Preservation Office
SIP	State Implementation Plan
SR	State Route
TACIR	Tennessee Advisory Commission on Intergovernmental Relations
TDEC	Tennessee Department of Environment and Conservation
TDOT	Tennessee Department of Transportation
TEER	Tennessee Environmental Evaluation Report
TIP	Transportation Improvement Program
TPO	Transportation Planning Organization
TVA	Tennessee Valley Authority
TWRA	Tennessee Wildlife Resources Agency
USACE	US Army Corps of Engineers
USCG	US Coast Guard
USFWS	US Fish & Wildlife Service
UT	University of Tennessee

Environmental Commitments

Commitments are involved on the project.

List of Environmental Commitments

1. To mitigate the project's impacts for the Section 4(f) use from I.C. King Park:
 - The northern SR-115 bridge adjacent to the park will be designed to accommodate a greenway crossing over the railroad track in addition to under the bridge, so that I.C. King Park and the park's existing parking lot will be connected to the Knox/Blount Greenway.
 - The southern bridge on SR-115 that crosses over Knob Creek will have a greenway trail added to the bridge design on the east side of the road adjacent to the park, extending from the park's old (southern) entrance to the newer, northern entrance to the park. This will connect the two sections of the park and users will have bike and pedestrian access throughout the park and can connect to existing and planned greenways.
2. Two segments of the Knox-Blount County greenway will be built as part of the proposed project, within the project right-of-way on the west side of SR-115.
3. Removal of trees with a diameter at breast height (DBH) greater than five inches shall be limited to the period between October 15 and March 31 to minimize potential harm to the Indiana bat.
4. A survey will be conducted prior to project construction to identify asbestos-containing materials on any bridges that are to be modified or demolished.

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- Attachment A Knoxville Regional Transportation Organization Transportation Improvement Program (TIP) Excerpt
- Attachment B Conceptual Stage Relocation Plan (CSRP)
- Attachment C Air Quality and Noise Coordination; Updated Air Quality Evaluation
- Attachment D Section 106 Coordination
- Attachment E Section 7 Coordination
- Attachment F Hazardous Materials Coordination
- Attachment G Agency Comments Received on February 2013 Environmental Assessment

List of Appendices

(Hard copies of FONSI contain Appendix CD on back cover—digital copies have “Appendix” PDF file)

- Appendix A Preliminary Plans
- Appendix B June 20, 2013, TDOT Public Hearing Summary and Comments
- Appendix C February 27, 2013 Environmental Assessment (EA) (includes Appendix containing technical studies, CSRP, agency coordination)

1.0 INTRODUCTION

1.1 General Project Description

The Tennessee Department of Transportation (TDOT), in cooperation with the Federal Highway Administration (FHWA), proposes to improve a section of State Route 115/U.S. 129/Alcoa Highway (SR-115, hereinafter) between Interstate 140 (I-140)/Pellissippi Parkway (Pellissippi Parkway, hereinafter) in Blount County and the Cherokee Trail Interchange in Knox County, a distance of 8.4 miles.

The proposed project will be constructed in part with federal funding from FHWA and is therefore subject to the requirements of the National Environmental Policy Act (NEPA). This Finding of No Significant Impact (FONSI) is prepared to meet NEPA requirements. A NEPA Environmental Assessment (EA) was approved by FHWA on February 27, 2013 and is contained in an appendix to this FONSI (Appendix C)¹. The Tennessee Valley Authority (TVA), the U.S. Army Corps of Engineers (USACE) and the U.S. Coast Guard (USCG) are NEPA Cooperating Agencies for this proposed project.

1.2 Project Segments

The proposed project has been divided into four segments for funding purposes. These segments are described in Table 1-1 and are shown on the General Location Map (Figure 1-1).

The four project segments are included in the Knoxville Regional Transportation Planning Organization's (TPO) 2040 *Long Range Regional Mobility Plan* adopted by the TPO Executive Board on April 24, 2013. Three of the four segments (not the segment of Maloney Road to Woodson Drive) are included in the Fiscal Year 2014-2017 *Transportation Improvement Program* (TIP). The 2014-2017 TIP pages for the federally-funded segments are in Attachment A.

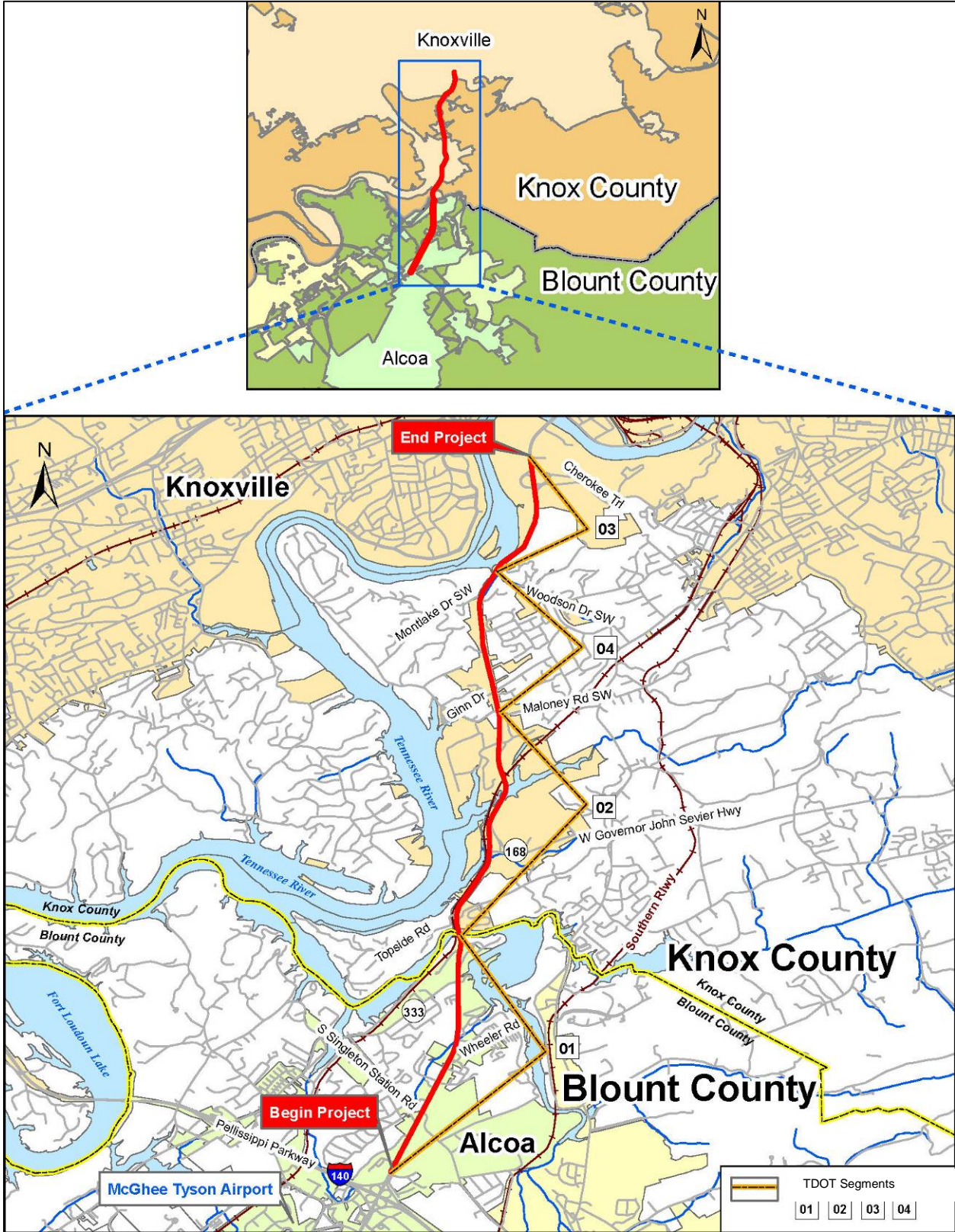
Table 1-1: Project Segments

SR-115 Segment (from south to north)	County	Length of Segment	TDOT PIN #	Federal Project #
Segment 1: Pellissippi Parkway (I-140, SR-162) to Knox/Blount County Line	Blount	3.2 miles	100241.01	STP-NHE-115(26)
Segment 2: Knox/Blount Co. Line to Maloney Road	Knox	2.2 miles	100241.02	STP-NHE-115(27)
Segment 3: Maloney Road to Woodson Drive	Knox	1.4 miles	100241.04	State-Funded
Segment 4: Woodson Drive to North of Cherokee Trail	Knox	1.6 miles	100241.03	STP-NHE-115(29)

Source: 2040 *Long Range Regional Mobility Plan* and 2014-2017 TIP.

¹ Hard copies of the FONSI have appendices included on a CD attached to the back cover. Digital copies have the appendix included as a digital file.

Figure 1-1: General Location Map



The segment from Maloney Road to Woodson Drive is state-funded, but because the improvements to this segment are related improvements within a programmed transportation facility and also because the segment is within the logical termini for the proposed project, the FONSI addresses the impacts along the entire length of the proposed project. This FONSI has been prepared to meet NEPA requirements for the federally-funded segments and serves as the Tennessee Environmental Evaluation Report (TEER) for the state-funded portion.

1.3 Purpose and Need

Existing SR-115 within the project limits is primarily a four-lane roadway with a mixture of at-grade intersections and grade separations. Through completion of two TDOT Advanced Planning Reports (APRs) and local coordination, the transportation needs of the project area have been identified. Described in more detail in the EA, the transportation deficiencies identified are:

1. Inadequate capacity for existing and future traffic resulting in poor level of service,
2. Bridge and roadway geometric deficiencies, and
3. Safety deficiencies.

The proposed project is intended to address the identified transportation deficiencies. The purpose of the proposed project is to:

- Increase the capacity of SR-115 and improve the level of service,
- Correct roadway deficiencies, and
- Reduce crashes/improve safety.

1.4 Consistency with Plans

The proposed project is consistent with state, regional, and local planning efforts. As previously discussed, all four segments are included in the *Long Range Regional Mobility Plan* and three sections are in the TIP. The fourth segment is slated for state funding.

The project is consistent with the plans for the proposed Alcoa Highway Bypass. TDOT will develop the plans for both projects and will ensure that the project design is coordinated. Also, the project is consistent with plans for the proposed Pellissippi Parkway Extension that would be located approximately two miles east of SR-115.

Both Knoxville-Knox County and Blount County have plans in place that specify future land use. A review of these plans indicates that the project does not conflict with the plans. Lastly, the project is consistent with the 2009 University of Tennessee (UT) *Cherokee Farm Campus Master Plan*. Cherokee Farm is located adjacent to the northern project terminus and is under development into a research park, named “Cherokee Farm Innovation Campus.” The campus infrastructure is complete and one research facility is currently under development, with an anticipated completion date of 2015. TDOT is working closely with UT on the development of the proposed interchange at SR-115 and Cherokee Trail that is part of the proposed project.

1.5 Logical Termini and Independent Utility

The project area studied in this FONSI is of sufficient size to address environmental concerns on a broad scope.

The proposed SR-115 improvement project has logical termini. At the southern terminus, the proposed project begins north of the roadway's intersection with Pellissippi Parkway, which is built west of SR-115 and partially built east of SR-115. The SR-115 alignment will provide access to Pellissippi Parkway and it will serve as an exit off that roadway for traffic desiring to access Knoxville and UT to the north and the cities of Alcoa and Maryville to the south. The project begins north of the interchange at a point where the interchange improvements end and the road narrows from six lanes to four lanes.

At the northern terminus, the proposed project ends north of the Cherokee Trail interchange. TDOT has completed improvements to the roadway from north of the Cherokee Trail interchange to the Tennessee River and the proposed project will tie into the improved roadway section. The improvements north of Cherokee Trail and the proposed project were included in the 2000 APR that addressed improvements to SR-115 from 250 feet south of Airbase Road in Blount County northward to the bridge over the Tennessee River in Knox County. (The segment of SR-115 from the south terminus of the proposed project southward to south of Airbase Road was also included in the APR.)

This proposed project demonstrates independent utility since it is not dependent upon implementation of any other transportation projects. This project would not restrict consideration of alternatives to other reasonably foreseeable transportation improvements, such as the proposed Alcoa Highway Bypass, the extension of Pellissippi Parkway, or improvements to other state or local roads.

2.0 SELECTED ALTERNATIVE

The alternatives under consideration in the EA included the No-Build and one Build Alternative. Several other potential build alternative options had been previously considered prior to development of the EA, but they were eliminated from further consideration due to either increased impacts or because they would not meet the purpose and need. The No-Build Alternative was eliminated subsequent to the approval and circulation of the EA and the public hearing, as the alternative did not meet the proposed project's purpose and need.

TDOT selected the Build Alternative as the preferred alternative to be carried forward into design (Selected Alternative, hereafter). There have been no modifications to the Selected Alternative since the approval of the EA. This alternative was developed to take into account engineering, social, and environmental considerations. Local coordination included the City of Alcoa and the Knoxville Regional TPO and the public. (The TPO includes representatives of the Knoxville-Knox County Planning Commission and the Blount County government.)

The Selected Alternative is a combination of full and partial access-controlled segments and follows the existing alignment. The project's purpose of congestion relief, correction of roadway deficiencies, and improved safety will be accomplished by construction of additional lanes on the existing facility and the proposed changes in access control. Figures 2-1A through 2-1C, 2-2 and 2-3 illustrate the alignment and features of the Selected Alternative.

As described in the EA, the Selected Alternative has three typical sections: (1) six lanes with mountable curb, (2) six lanes and a roadside ditch, and (3) six lanes with two auxiliary lanes (eight lanes total) and a roadside ditch. Each proposed typical section will have a 22-foot median with a concrete median barrier and at least three 12-foot lanes in each direction. At the beginning of the project just north of the SR-115 and Pellissippi Parkway Interchange, the proposed SR-115 improvements will be a six-lane section with a median barrier that will tie to the existing six-lane section approximately 630 feet north of the interchange ramps. At the end of the project north of the Cherokee Trail interchange, the proposed section ties to the existing section approximately 100 feet south of the SR-115 bridge over the Tennessee River. There are three lanes in each direction through the Cherokee Trail interchange. The entrance ramp becomes the fourth lane northbound and the fourth lane on southbound SR-115 becomes an exit only lane at this interchange.

The proposed SR-115 improvements will provide partial access control to this urban principal arterial. All left turns will be eliminated on SR-115 with a center median barrier running the length of the project. Access will be provided through grade-separated interchanges and a series of frontage and collector roads. Right turns will remain at selected locations. A series of collector roads is also proposed to facilitate local access.

New parallel bridges will be constructed as part of this project adjacent to I.C. King Park. On the west side of SR-115 across from the park, the project also includes construction of a segment of the Knox-Blount Greenway within the proposed Right-of-Way (ROW).² Because of the proposed barrier in the center of SR-115, the connection between the west side of SR-115 where the new greenway segment is to be located and the east side of SR-115 where the park

² The project includes construction of two segments of the Knox-Blount Greenway within the proposed ROW—refer to Figure 3.3)

Figure 2-1A: Selected Alternative, Beginning of Project to South of Little River

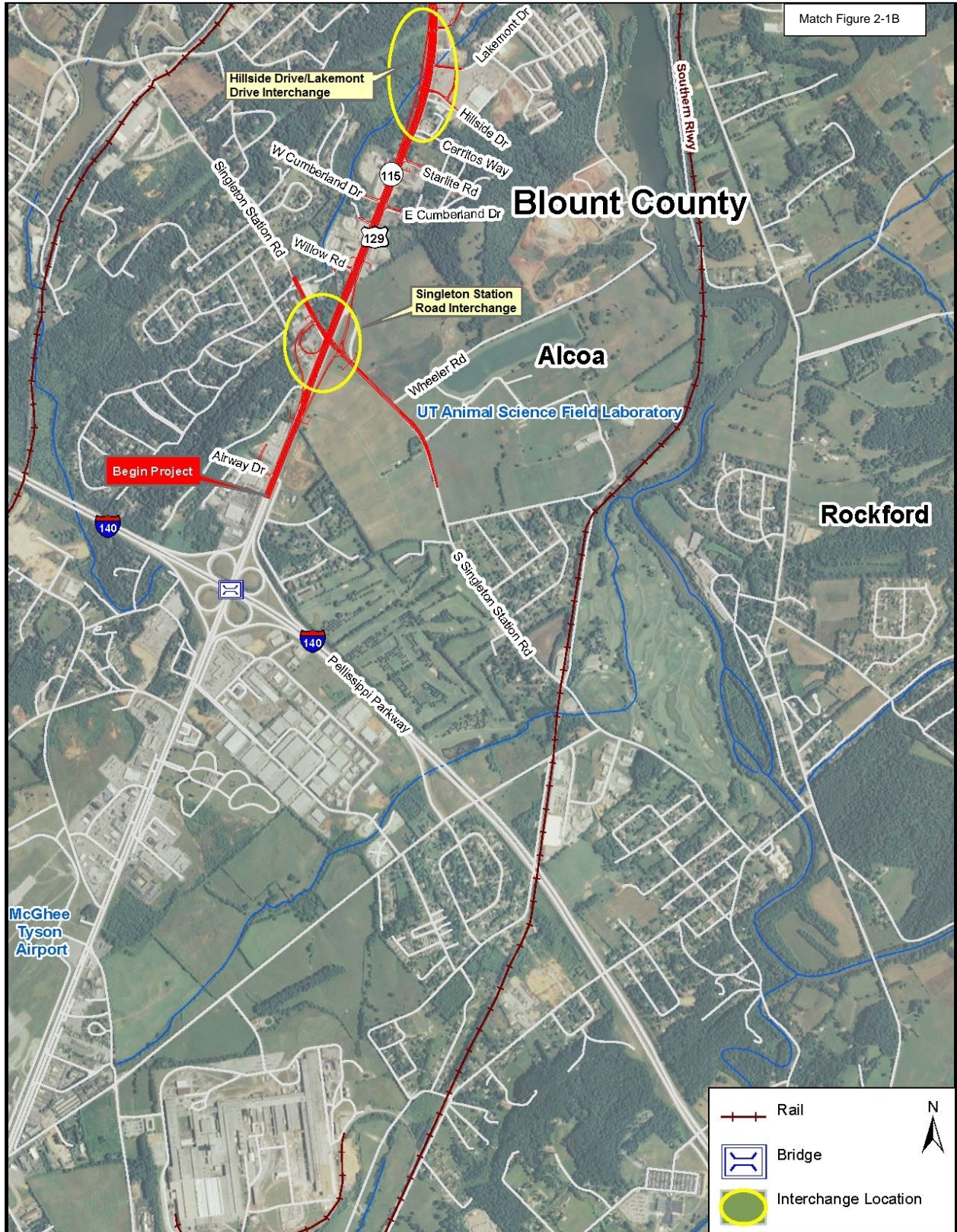


Figure 2-1B: Selected Alternative, South of Little River to North of Maloney Road

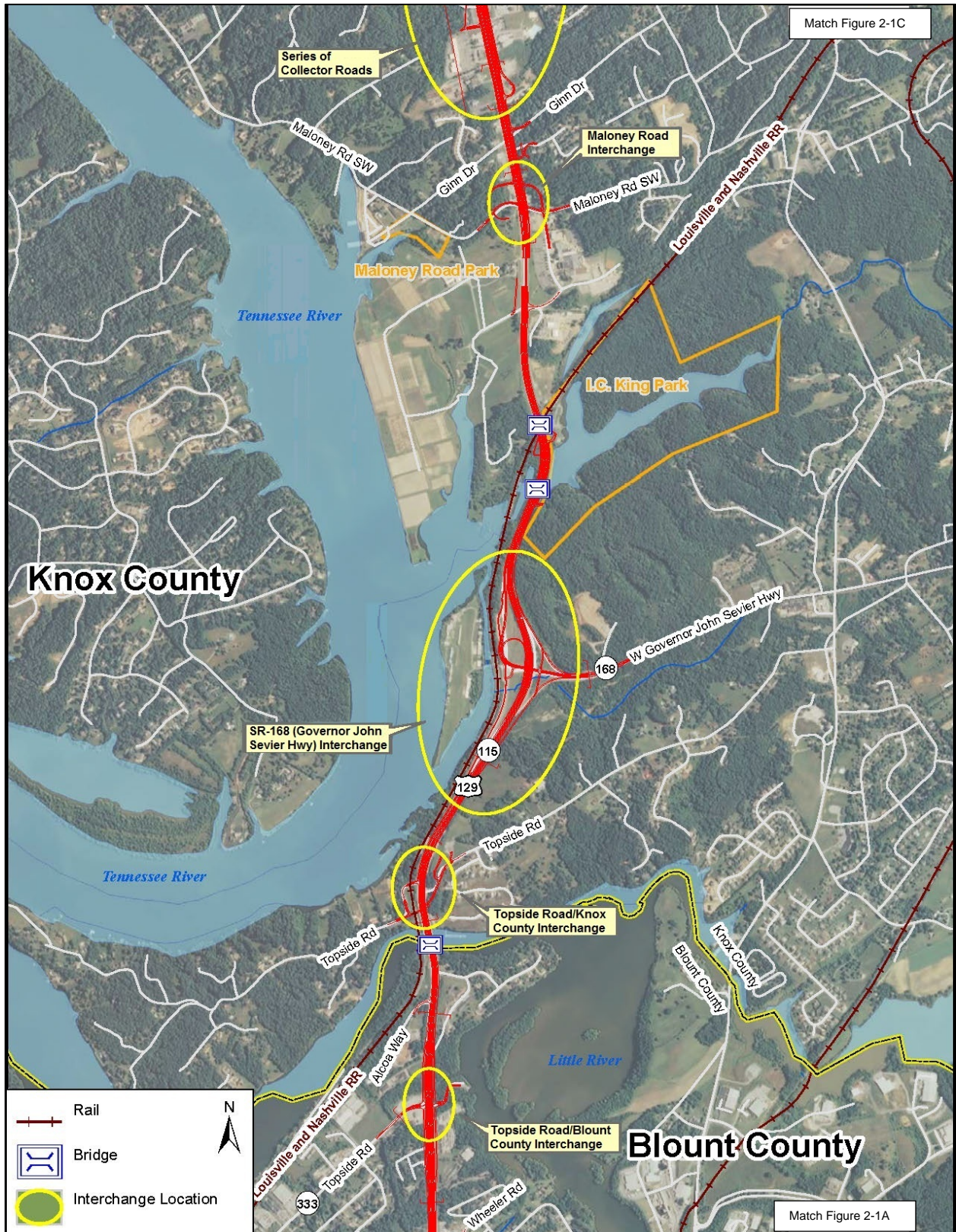


Figure 2-1C: Selected Alternative, North of Maloney Road to Project End

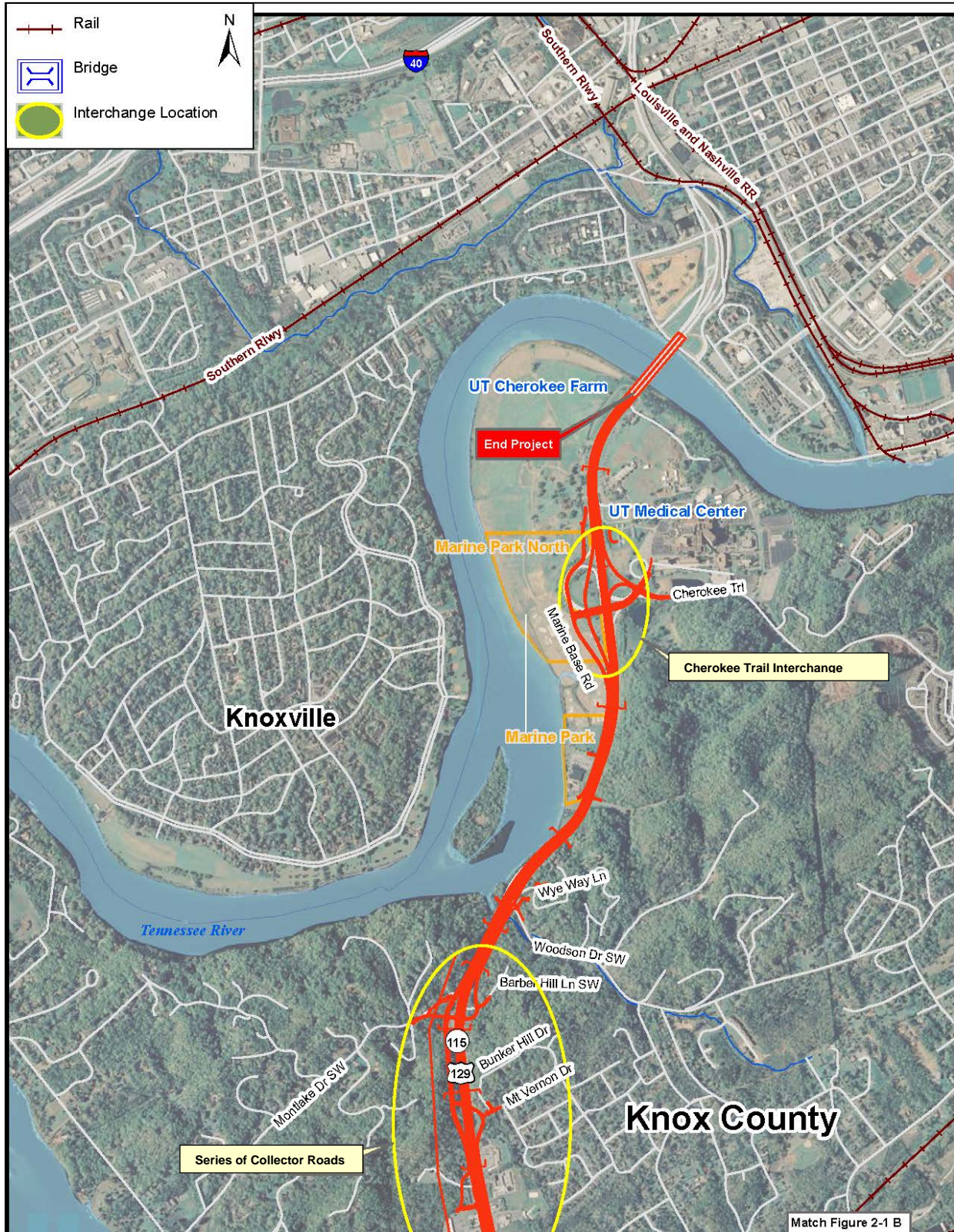
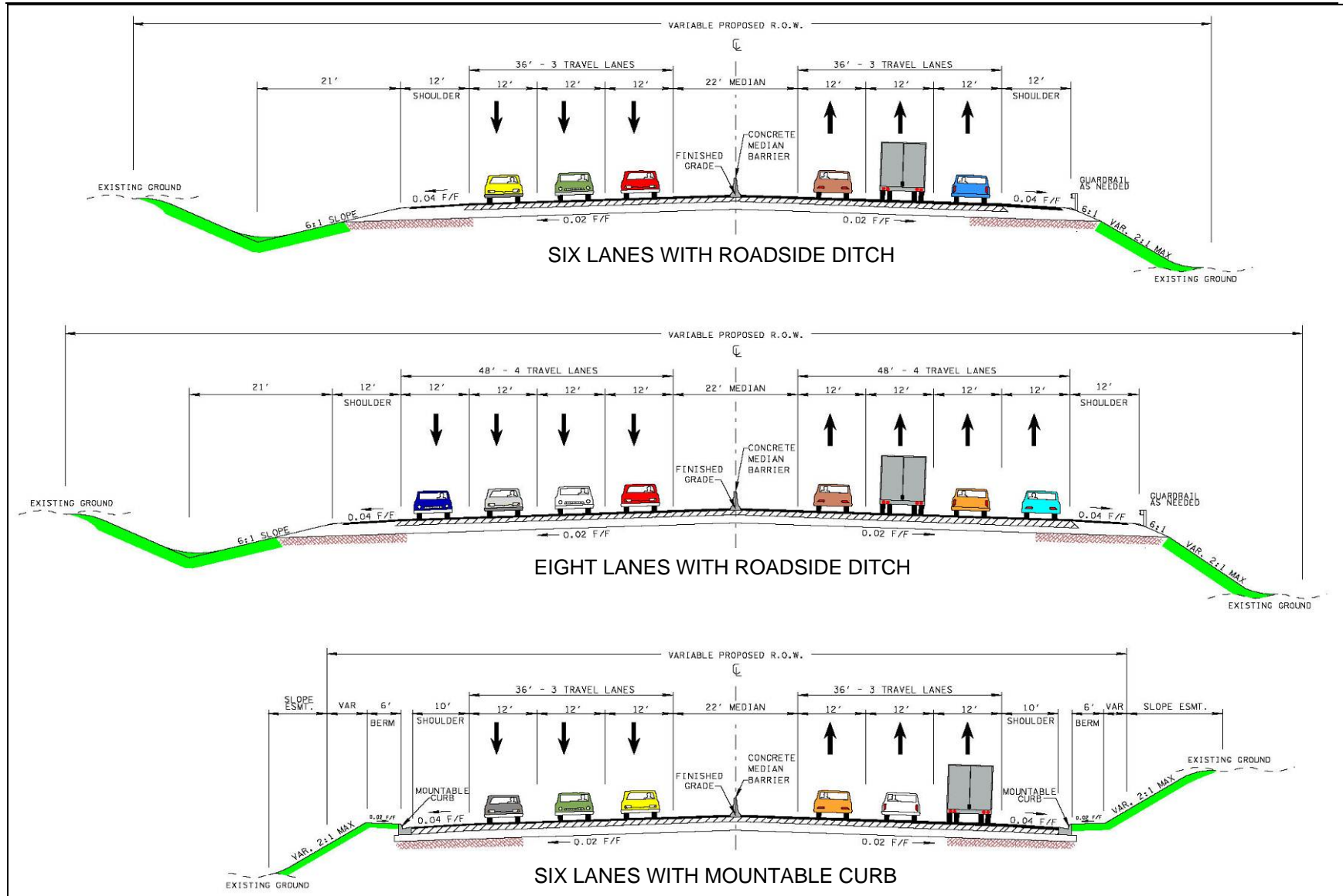
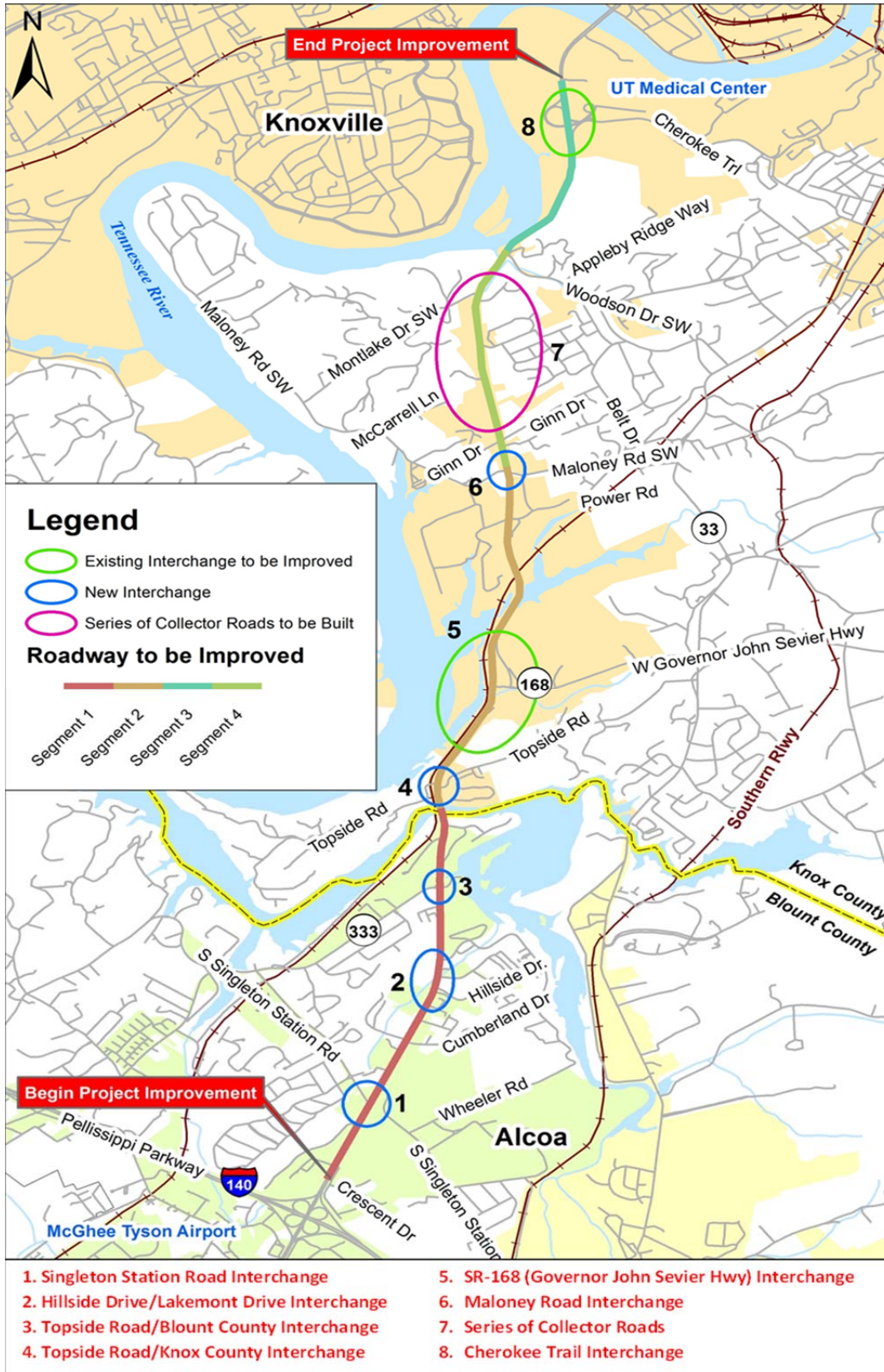


Figure 2-2: Proposed Typical Sections



The standard median barrier width is 28 inches at the bottom.

Figure 2-3: Location Map of Proposed Improvements for Interchanges and Collector Roads



is located will be lost. To provide access between the proposed greenway segment and the park, a connector trail will be built under the new bridge. The project would also eliminate the connection between the north and south portions of the park that are separated by Knob Creek. To restore this connection, the project will continue the connector trail on the east side of SR-115 up onto a sidewalk on the east side of the new bridge enabling pedestrians and bicyclists access between the two sections of the park. For more information, refer to Section 3.6, Bike and Pedestrian Impacts, and Figure 3.3.

Preliminary plans for the project are contained in Appendix A.

As reported in the 2014-2017 TIP, estimated construction costs in the proposed horizon year of 2019 are shown in Table 2-1.

Table 2-1: Allocated Funding, Right-of-Way Acquisition

Project Segment	Length of Segment (miles)	Horizon Year ¹	Cost ²	ROW Year	ROW ² Funding
Pellissippi Parkway to Knox / Blount County Line	3.2	2019	\$73,200,000	2015	\$36,200,000
Knox / Blount County Line to Maloney Road	2.2		\$34,459,500	2016	\$6,100,000
Maloney Road to Woodson Drive	1.4		\$41,997,741 ³	Not in TIP, Proposed for State Funding	
Woodson Drive to North of Cherokee Trail	1.6		\$41,200,000	2015	\$14,000,000
Total	8.4		\$190,857,241		\$56,300,000 ⁴

¹ 2040 Long Range Regional Mobility Plan, Knoxville Regional TPO Adopted April 24, 2013

² Knoxville Regional TPO 2014-2017 TIP

³ State-funded cost provided by TDOT

⁴ Total excludes state-funded section

3.0 SUMMARY OF ENVIRONMENTAL IMPACTS

Table 3-1 provides a summary of the potential impacts of the Selected Alternative. Text in the table that is highlighted in yellow indicates an area for which an update to the EA technical study was warranted and/or requested and subsequently undertaken for the FONSI. If an update from the EA was not warranted, the table states that the findings of the EA remain valid and references are made to TDOT verification of the validity, as applicable.

Table 3-1: Potential Environmental Impacts of Selected Alternative

(Yellow-highlighted rows indicate areas for which updates were completed for this document.)

Impact Category	Selected Alternative
ESTIMATED ROW ACQUISITION	128 acres
TRANSPORTATION	Improved Level of Service Improved regional transportation network Improvements to existing roadway will incorporate current safety standards Changes in access
LAND USE	Updated to reflect local government comments—Section 3.1 of FONSI Conversion of approximately 128 acres to highway ROW Potential indirect and cumulative impact of development of vacant land along project corridor
NAVIGATION	Discussion added per USCG comments—Section 3.2 of FONSI
FARMLAND	No change from EA; acquires 13 acres prime and/or unique farmland
SOCIAL AND ECONOMIC	
Community Cohesion	No adverse impact
Displacements	The CSRP completed for EA is valid as the concept has not changed and there is no new development that would be affected (see correspondence and CSRP in Attachment B)
Residential	Total Residential: 46 21 single family residences 4 mobile homes 21 tenants in 2 apartment buildings (one 15-unit building and one 6-unit building)
Business	Total Business: 24
Environmental Justice	Updated for 2010 Census Data—Section 3.3 of FONSI No disproportionately high and adverse human health or environmental effects to minority or low-income populations
Economic	No change from EA. Improved regional transportation network will enhance area for new and existing businesses

Table 3-1: Potential Environmental Impacts of Selected Alternative

(Yellow-highlighted rows indicate areas for which updates were completed for this document.)

Impact Category	Selected Alternative
AIR QUALITY	No Effect determination in EA remains valid. Air quality study updated in 2013—Section 3.4 of FONSI and Attachment C.
NOISE (number of affected sites)	EA Noise Study is valid as there is no change in the project alignment and no new development that would be affected (see Attachment C). 62 residences (52 single-family residences and 1 apartment building with 10 affected units) and 1 non-profit exceed noise abatement criteria; no substantial increases. Noise walls are not feasible or reasonable according to TDOT's 2011 noise policy.
CULTURAL RESOURCES	
National Register of Historic Places (NRHP) Eligible/Listed Architectural/Historical Sites	EA Architectural/Historical Study is valid as there is no change in the project alignment (see Attachment D for confirmation that the study is valid). No Adverse Effect
NRHP Eligible/Listed Archaeological Sites	EA Archaeology Study is valid as there is no change in the project alignment (see Attachment C for confirmation that the study is valid). No Adverse Effect
RECREATIONAL RESOURCES	No change in the impact assessment, but new information has been added. Refer to FONSI Section 3.6 and Figure 3.3.
SECTION 4(f) RESOURCES	The Programmatic Section 4(f) Evaluation for I.C. King Park and the Section 4(f) <i>De Minimis</i> finding for Marine Park North remain valid as there are no changes in the project concept. (See the EA in the Appendix C for both 4(f) documents)
SECTION 6(f) INVOLVEMENT	No change from EA. No 6(f)-funded improvements are involved.
NATURAL RESOURCES—EA Ecology Study is valid (see Attachment E for confirmation that the study is valid). Note information added to Threatened and Endangered Species assessment in FONSI (Section 3.5).	
Water Quality/Aquatic Resources	8 streams totaling 2,445 linear feet (l.f.) of impact, 5 wet weather conveyances totaling 645 l.f. of impact
Wetland	0.02 acre impact to 1 wetland
Channelization of Streams	None
Floodplains	13.27 acres

Table 3-1: Potential Environmental Impacts of Selected Alternative

(Yellow-highlighted rows indicate areas for which updates were completed for this document.)

Impact Category	Selected Alternative
Threatened and Endangered Species (Federal and State)	<p>Updated coordination with the US Fish and Wildlife Service (USFWS) occurred through the circulation of the EA. The USFWS sent a letter on April 18, 2013 confirming that the requirements of Section 7 have been met and that the project is "not likely to adversely affect" Indiana bat as stated in letters dated September 21, 2011 and November 15, 2011.</p> <p>The Tennessee Department of Environment and Conservation (TDEC) also commented on the EA in regard to state-listed species on May 9, 2013. The resultant analysis is included in FONSI Section 3.5. The USFWS and TDEC letters are in both Attachments E and G.</p>
Invasive Species	No change from EA. Invasive species identified are Privet (<i>Ligustrum</i> sp.), bush honeysuckle (<i>Lonicera maackii</i>), and multiflora rose (<i>Rosa multiflora</i>).
Wild and Scenic Rivers	None present
GEOTECHNICAL	No change from the EA as the concept is unchanged. Open rock outcrop between Montlake Drive and Woodson Drive may require stabilization or cutback; geotechnical studies may be necessary to address karst features beneath existing roadbed. (A rock slide occurred in the project area in March 2012.)
VISUAL	No change from the EA as the concept is unchanged. No adverse impact. Minor changes to viewshed along existing SR-115 corridor as additional lanes, a median barrier, and grade separations are constructed.
ENERGY	No change from the EA as the concept is unchanged. No adverse energy impact
HAZARDOUS MATERIALS	<p>EA Hazmat Study is valid as the alignment has not changed since the EA analysis. (See Attachment F for confirmation)</p> <p>Study reported 8 sites assigned low-risk ranking and 10 sites assigned high-risk ranking; additional surveys required prior to acquisition of ROW.</p>
PEDESTRIAN AND BICYCLE	Since the EA approval, TDOT has coordinated with local government and has agreed to include two greenway segments in the proposed project. These segments are part of the Knox-Blount Greenway, which will provide bike and pedestrian access between South Knoxville, UT and downtown Knoxville. TDOT will evaluate the inclusion of pedestrian facilities for bridge crossings over SR-115 at interchanges in the design phase. (Section 3.6)
CONSTRUCTION	No change from the EA as the concept has not changed. Temporary detours may occur at new interchanges; use of existing lanes and Best Management Practices (BMPs) will avoid or minimize most construction-related impacts.

Table 3-1: Potential Environmental Impacts of Selected Alternative

(Yellow-highlighted rows indicate areas for which updates were completed for this document.)

Impact Category	Selected Alternative
INDIRECT AND CUMULATIVE IMPACTS	As neither the project concept or land use in the area has changed, the analysis in the EA remains valid. The EA reported minor indirect and/or cumulative effects to land use and aquatic resources.
ENVIRONMENTAL COMMITMENTS	The commitments made in the EA remain valid and are included on the green Environmental Commitment sheet for this FONSI. Commitments to build two segments of the Knox-Blount County Greenway as part of the project and to survey the project prior to construction to identify asbestos-containing materials on any bridges that are to be modified or demolished have been added to the environmental commitments.

3.1 Land Use Impacts

The proposed project is located in the area south of the City of Knoxville and north of the cities of Alcoa and Maryville; the latter is the county seat of Blount County. The northern portion of the corridor is included within the Knoxville metropolitan area boundary, which encompasses all of Knox County, while the southern portion is within Blount County.

The project area is home to a variety of land uses, including developed (28 percent), transportation (30 percent), forest (24 percent), old field/agricultural (14 percent), and water (4 percent). The developed uses are primarily residential and commercial with some institutional uses and parkland. These uses are consistent with the overall land use types found in the region and depicted in the EA.

Construction of the proposed project will convert approximately 128 acres of land adjacent to SR-115 to highway ROW, changing the use of the land acquired to highway use. This land to be converted abuts existing SR-115 and is generally in commercial and residential uses. The Selected Alternative is not anticipated to affect future land use and is consistent with the land use plans and policies adopted by Knoxville-Knox County and Blount County as shown in the *Knoxville-Knox County General Plan* (2003), the *South County Sector Plan for Knox County* (2012), and the *Blount County Conceptual Land Use Plan* (2000). As the project advances, TDOT will continue to coordinate with local government representatives over impacts to future land use.

3.2 Navigation Impacts

Little River and Knob Creek within the project area are located within the embayments of the Tennessee River and are considered navigable waterways of the United States for bridge administration purposes at the bridge sites. Improvements to existing bridges that will alter the navigational clearances provided by the bridges or significantly alter the structure of the bridges will require a bridge permit or bridge permit amendment. This includes a Section 9 Navigable Waterways permit from the USCG.

If bridge permits are required for the project, the USCG will need a Water Quality Certificate from the appropriate state agency, which states that the project complies with the provisions of Section 401 of the Clean Water Act.

TDOT will coordinate with the Tennessee Department of Environment and Conservation (TDEC), which is the agency responsible for Section 401 permits. Additional permits related to water quality and pollution prevention include a Section 10 Navigable Waterways permit from the USCG and Individual or Nationwide Section 404 permits from the USACE.

The improvements to bridges along the route will be designed to minimize impacts to navigable waterways. Continued coordination will occur throughout the design and construction of the Selected Alternative.

3.3 Environmental Justice

The Environmental Justice analysis has been updated based on new demographic data (following release of the 2010 Census data) and to address agency comments provided on the NEPA EA.

3.3.1 Existing Social Conditions

The proposed project is located between the cities of Knoxville in Knox County and Alcoa in Blount County. Maryville, the Blount County seat, is located immediately south of Alcoa. The northern portion of the project corridor is within the Knoxville metropolitan area boundary, which is the Knox/Blount County Line. No other incorporated towns are located along the route.

Table 3-2 outlines general population data from the 1990, 2000 and 2010 US Census for Knox and Blount Counties and the Cities of Knoxville, Alcoa, and Maryville. The State of Tennessee is also included as a point of reference. As documented in Table 3-2, the percent growth for Blount County, Alcoa, and Maryville between 1990 and 2010 substantially exceeded that of Knoxville. It also exceeds that of Knox County and the State of Tennessee. The population data illustrate growth in the area, particularly the southern part of the area (Blount County). This growth contributes to increased traffic along SR-115, as the increased population density and related Blount County development generates traffic and contributes to congestion on area roadways.

According to aggregate population projections from the Tennessee Advisory Commission on Intergovernmental Relations (TACIR) and the Center for Business and Economic Research (CBER), the growth trend is likely to continue. In 2030, Blount County is predicted to have 164,211 residents, an annual growth of approximately 1.67 percent, while Knox County's predicted population in 2030 is 491,100—an annual growth of approximately 0.68 percent.

The project area (comprised of block groups that abut SR-115) is composed of block groups located within Census Tracts 35 and 56.01 in Knox County and Census Tract 103 in Blount County.

Table 3-2: Census Data, Population Growth

Census Unit	Population			Percent Change, 1990-2010
	2010	2000	1990	
Tennessee	6,158,953	5,689,283	4,877,185	26.2
Knox County	432,655	382,032	335,749	28.9
Knoxville, Knox County	183,032	173,890	165,121	10.8
Blount County	119,489	105,823	85,969	38.9
Alcoa, Blount County	8,613	7,734	6,400	34.5
Maryville, Blount County	26,602	23,120	19,208	38.5

Source: US Census Bureau, 2010, 2000 and 1990 Census.

Table 3-3 contains demographic estimates for minorities and low-income populations for the project area based on data from the 2010 US Census for Tennessee as a whole and for Knox and Blount Counties. Demographic data are from the 2010 American Community Survey, which is a part of the 2010 Census.

Table 3-3 provides minority population data for block groups within the project area in Blount and Knox Counties. According to the census data, minority population percentages for the block groups in the project area range from 0.0 percent to 33.9 percent. When comparing these percentages to the respective countywide averages, two block groups in Blount County exceeded the average and the block groups in Knox County fall well below the average. When compared with estimates for Tennessee, all but two block groups within the project area fall well

below the statewide average. The two block groups in Blount County that exceed their county and state averages are Census Tract 103, Block Group 5 (23.8 percent) and Census Tract 103, 6 (33.9 percent), respectively.

The 2010 Census estimates also show the percentages of persons below the poverty level, which range from 4.0 percent to 31.7 percent for block groups within the project area, as shown in Table 3-3. When comparing these percentages to their countywide averages, three block groups in Blount County and two block groups in Knox County exceeded their average. When compared with estimates for Tennessee, four block groups (two in Knox County and two in Blount County) are above the statewide average. The two block groups in Blount County that exceed their county and state averages are Census Tract 103, Block Group 5 (30.7 percent) and Census Tract 103, 6 (19.6 percent); the two in Knox County are Census Tract 35, Block Group 1 (31.7 percent) and Census Tract 56.01, Block Group 4 (17.7 percent), respectively.

3.3.2 Environmental Justice

The proposed project has been developed in accordance with Executive Order 12898, *Federal Actions to Address Environmental Justice (EJ) in Minority and Low-Income Populations*, which requires federal agencies to develop a strategy for its programs, policies, and activities to avoid disproportionately high and adverse impacts on minority and low-income populations with respect to human health and the environment.

Table 3-3: 2010 Total Population, Minority and Low-Income Population Characteristics (percentages rounded to one decimal point)

Geographic Area	Total Population 2010	Minority 2010		Poverty 2010	
		Minority Population	Minority Percentage	Persons Below Poverty Level	Poverty Level Percentage
Tennessee	6,158,953	1,281,062	20.8	991,591	16.1
Knox County	423,655	54,228	12.8	62,277	14.7
Census Tract 35 Block Group 1	764	0	0.0	242	31.7
Census Tract 35 Block Group 2	740	38	5.1	39	5.3
Census Tract 35 Block Group 3	1,310	143	10.9	39	5.3
Census Tract 56.01 Block Group 4	1039	0	0.0	184	17.7
Blount County	119,489	8,006	6.7	14,697	12.3
Census Tract 103 Block Group 3	1,533	75	4.9	61	4.0
Census Tract 103 Block Group 4	525	24	4.6	71	13.5
Census Tract 103 Block Group 5	1,752	417	23.8	540	30.8
Census Tract 103 Block Group 6	1,018	345	33.9	200	19.6

Source: US Census 2010 American Community Survey Summary File; US Census 2010 Summary File 1 (SF-1) and Summary File 3 (SF-3); EPA EJView website.

To determine the impacts of the Selected Alternative on minority and low-income populations, the analysis utilized US Census data for the project area, coordinated with local government and the TDOT Division of Civil Rights, and conducted a field review.

Figures 3-2 and 3-3 show, respectively, minority and low-income population in the project area. As previously stated, two block groups in Blount County exceed both the county and statewide minority percentage (Census Tract 103, Block Groups 5 and 6). Regarding income, two block groups in Knox County (Census Tract 35, Block Group 1; Census Tract 56.01, Block Group 4) exceed the county and statewide poverty level percentages. The same Blount County block groups listed above (Census Tract 103, Block Groups 5 and 6) also have a poverty level that exceeds the state and county percentages.

These block groups abut SR-115 and are located at various points along the project alignment. The project would have an impact to these populations as the roadway improvement will occur adjacent to the neighborhoods, but it will also have impacts to other block groups that abut SR-115 within the project corridor. Also, as is the case for all populations along the corridor, access to and from SR-115 would change due to the median barrier, but access to all residential, commercial, industrial, and institutional properties would remain.

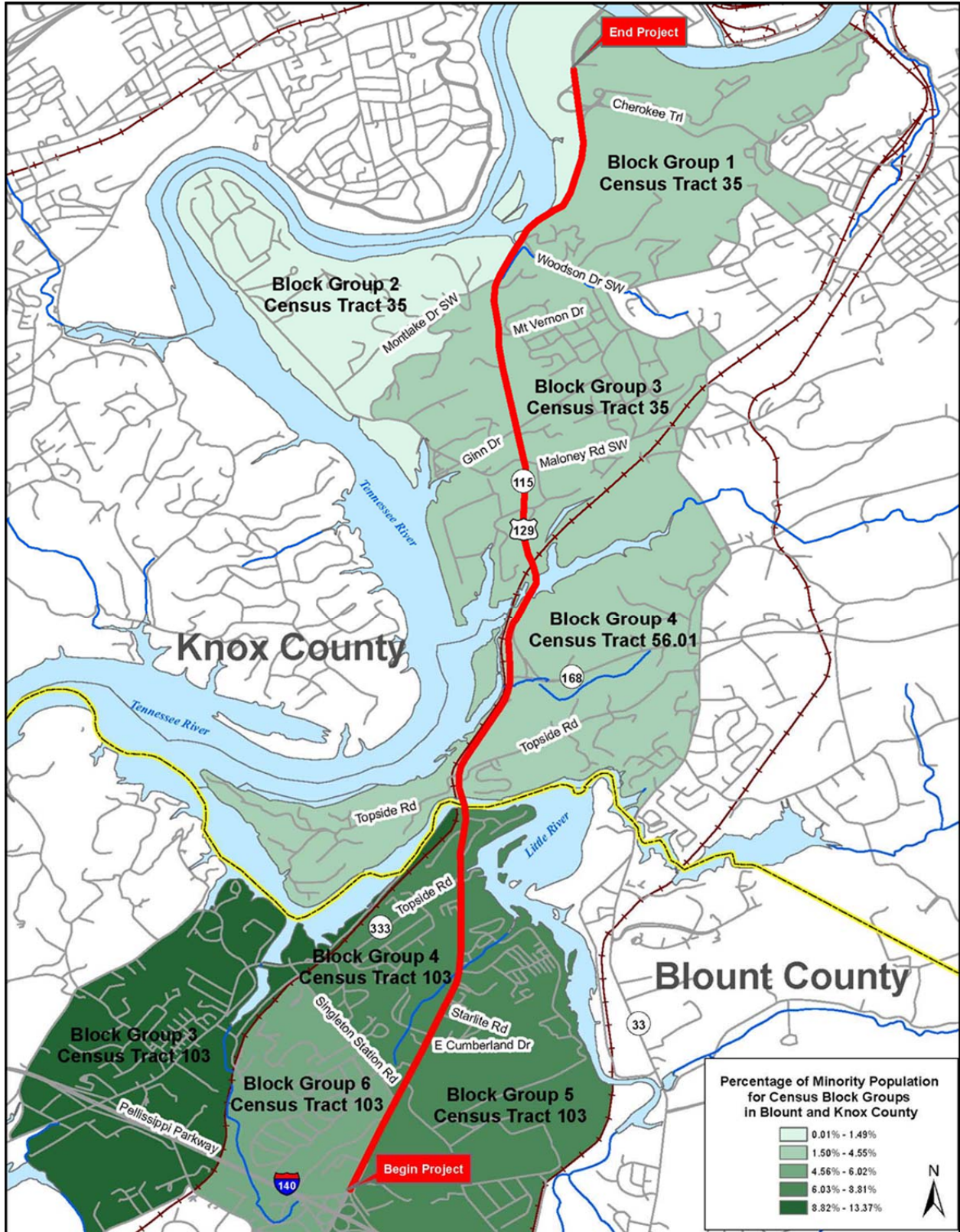
The areas that have minority or low income percentages that exceed county and/or state levels do not have high percentages of such populations (i.e., 33.9 percent is the highest minority percentage and 31.7 percent is the highest below poverty level percentage). The only impacts to the minority and below poverty level populations would be changes in access to SR-115, and some residences in Census Tract 103, Block Group 5 would incur noise impacts. These properties already receive noise from the existing SR-115 and the impacts would not be substantial according to the noise study undertaken for the proposed project. Additionally, this project does not disproportionately affect minority or low-income areas along the route as impacts, such as noise and access changes, occur to all populations along the route. The project would benefit all populations by providing safer access and better mobility on SR-115.

Additional research on minority populations was gathered from the 2040 *Knoxville Regional Mobility Plan*, which included a review of Title VI Assessment in the TPO planning area and entire Knoxville region. The TPO followed the methodology specified in the Federal Transit Administration (FTA) Circular 4701.01, which states that any census tract whose minority percentage is greater than the TPO area average is designated a Title VI minority census tract. Within the TPO planning area, minorities consist of 12.3 percent of the population.

According to the FTA evaluation tool, two of the census tracts (Census Tract 35 and 103) in the project corridor would be designated as a Title VI minority area. In addition, the 2040 *Knoxville Regional Mobility Plan* lists the project segment from Pellissippi Parkway to the Knox/Blount County line (PIN #100241.01, LRTP #216) as being in a Title VI assessment area. While Title VI-designated areas are in the project area, the project will not disproportionately affect minority populations because the project would involve improvements throughout the entire corridor and not solely within an area that has Title VI-designated areas. All populations would receive impacts and benefits from project implementation.

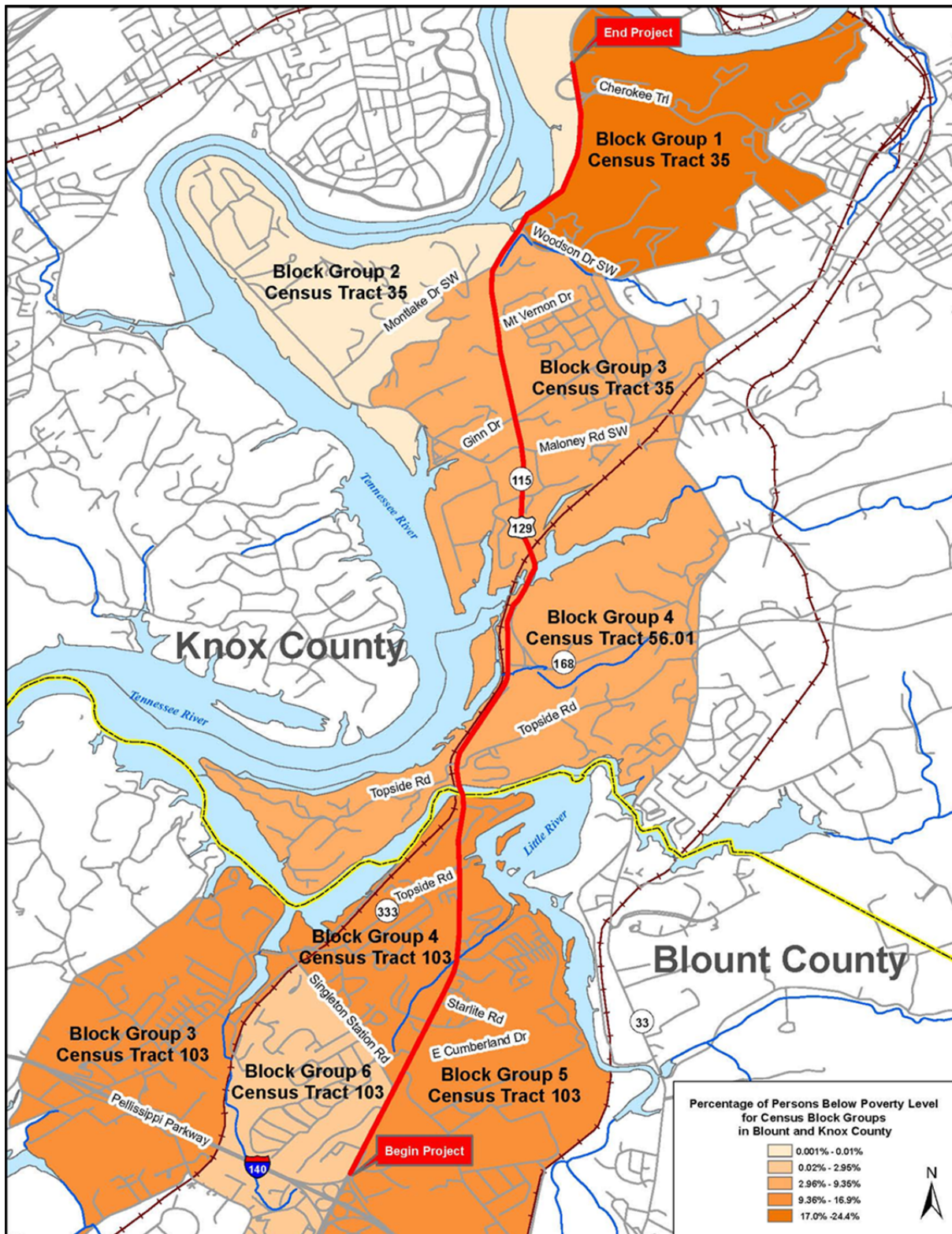
In correspondence dated July 18, 2005, the Knox County Housing Authority determined that the proposed project “does not interfere with any housing choice vouchers administered by the Knox County Housing Authority.” For early agency coordination, see the Technical Studies Appendix of the EA, which is included as Appendix C of this FONSI.

Figure 3-1: Percentage of Minority Population for Block Groups in Blount and Knox County (2010)



Source: US Census 2010 American Community Survey Summary File.

Figure 3-2: Percentage of Persons Below the Poverty Level for Block Groups in Blount and Knox County (2010)



Source: US Census 2010 American Community Survey Summary File.

In accordance with Title VI of the *Civil Rights Act of 1964*, the Department will comply with Title VI to ensure that "no person shall be, on the grounds of race, color or national origin, excluded from participation in, denied the benefits of, or subjected to discrimination under any program or activity receiving Federal assistance." The proposed project is consistent with Executive Order 12898, *Federal Actions to Address Environmental Justice in Minority Populations and Low-Income Populations*. TDOT will ensure that all residents, including non-English speaking populations, will have full access to the decision-making process by researching the existence of such populations in the project area and developing materials to reach these populations if they exist in the project area.

3.4 Air Quality Impacts

This section updates the air quality impact assessment in the EA, but it does not change the findings. This section summarizes the findings of the October 2013 Air Quality Evaluation in Attachment C.

3.4.1 Project-level Transportation Conformity and Particulate Matter (PM) 2.5 Coordination

The findings of the EA are valid regarding transportation conformity (See coordination in Attachment C). The proposed project is located in the Knoxville PM_{2.5} and Ozone nonattainment areas, therefore, conformity applies to the proposed project. Projects in nonattainment and maintenance areas are in conformity with the SIP if they are included in a fiscally constrained and conforming LRTP or TIP. As discussed in Section 1, all four segments of the proposed project are included in the 2040 *Long Range Regional Mobility Plan*, adopted on April 12, 2013. Three of the segments are included in 2014-2017 TIP. FHWA and FTA approved the Conformity Determination for the LRTP and TIP on November 5, 2010. Therefore, the proposed project conforms to the SIP.

TDOT completed for the EA a PM_{2.5} Hot-Spot Determination for the proposed project in accordance with TDOT's PM_{2.5} Hot Spot Determination Process and Procedures and concluded that the project is "Not a Project of Air Quality Concern." This determination was submitted to the Knoxville Area Interagency Consultation Group (IAC) on November 19, 2010. The Knoxville Area IAC members concurred that the SR-115 project is "Not a Project of Air Quality Concern" on the following dates: FHWA, November 29, 2010; TDEC, November 29, 2010; and Knox County, November 19, 2010. EPA did not respond. The PM_{2.5} Hot-Spot Determination and IAC concurrence responses developed for the EA are provided in Attachment C.

3.4.2 Mobile Source Air Toxics (MSATs)

On February 3, 2006, FHWA first released *Interim Guidance on Air Toxic Analysis in NEPA Documents*. This guidance was superseded on September 30, 2009, and most recently on December 6, 2012, by FHWA's *Interim Guidance Update on Air Toxic Analysis in NEPA Documents*. The purpose of FHWA's guidance is to advise on when and how to analyze MSATs in the NEPA process for highways. This guidance is interim because MSAT science is still evolving. As the science progresses, FHWA will continue to update the guidance.

Technical shortcomings of the emissions and dispersion models and uncertain science with respect to health effects, prevent meaningful or reliable estimates of MSAT emissions of the proposed project. However, even though reliable methods do not exist to accurately estimate the health impacts of MSATs at the project level, it is possible to qualitatively assess the levels of future MSAT emissions. The qualitative assessment has been prepared in accordance with FHWA's Interim Guidance derived in part from a study conducted by FHWA entitled *A Methodology for Evaluating Mobile Source Air Toxic Emissions Among Transportation Project*

Alternatives. Additional information regarding MSATs is provided in the updated Air Quality Evaluation Report located in Attachment C.

FHWA's Interim Guidance groups projects into three categories:

- Exempt Projects and Projects with no Meaningful Potential MSAT Effects,
- Projects with Low Potential MSAT Effects, and
- Projects with Higher Potential MSAT Effects.

FHWA's Interim Guidance provides examples of "Projects with Low Potential MSAT Effects." These projects include minor widening projects and new interchanges, such as those that replace a signalized intersection on a surface street or where design year traffic projections are less than 140,000 to 150,000 Average Annual Daily Traffic (AADT).

The Selected Alternative includes the widening of SR-115. The projected design year 2036 traffic projections that were used for the air quality analysis assume that the Alcoa Highway Bypass will be constructed and Pellissippi Parkway will be completed. This is considered the worst-case traffic scenario for the air quality analysis. The projected AADT on SR-115 between Pellissippi Parkway and Singleton Station Road is 26,070. The projected AADTs north of Singleton Station Road are higher and range from 84,540 between Singleton Station Road and Topside Road and 89,220 between the Knox/Blount County line and Maloney Road. These volumes are substantially lower than the FHWA criterion; therefore, the proposed project meets the criteria for a "Project with Low Potential MSAT Effects."

For both the No-Build and Selected Alternative, the amount of MSATs emitted would be proportional to the vehicle miles traveled (VMT), assuming that other variables such as fleet mix are the same for each alternative. The estimated AADTs for the Selected Alternative are 8 percent to 19 percent higher than the estimated AADTs for the No-Build Alternative. However, the calculated VMT for the No-Build Alternative may be low because the roadway for which traffic projections were developed did not include all the roads from which traffic would be diverted.

The emissions increase is offset somewhat by lower MSAT emission rates due to increased speeds; according to EPA's MOVES2010b model, emissions of all the priority MSAT decrease as speed increases. Because the estimated VMT under both alternatives is nearly the same it is expected there would be no appreciable difference in overall MSAT emissions among the various alternatives.

Also, regardless of the alternative chosen, emissions will likely be lower than present levels in the design year as a result of EPA's national control programs that are projected to reduce annual MSAT emissions by over 80 percent between 2010 and 2050. Local conditions may differ from these national projections in terms of fleet mix and turnover, VMT growth rates, and local control measures. However, the magnitude of the EPA-projected reductions is so great (even after accounting for VMT growth) that MSAT emissions in the study area are likely to be lower in the future in nearly all cases. Additionally, travel speeds for the Selected Alternative are expected to be higher than for the No-Build Alternative.

The additional travel lanes planned for the Selected Alternative will move some traffic closer to nearby residences and businesses; therefore, under the Selected Alternative there may be localized areas where ambient concentrations of MSATs could be higher than under the No-Build Alternative. However, as previously discussed, the magnitude and the duration of these

potential increases compared to the No-Build Alternative cannot be reliably quantified due to incomplete or unavailable information in forecasting project-specific MSAT health impacts.

When a highway is widened, the localized level of MSAT emissions for the Selected Alternative could be higher relative to the No-Build Alternative, but this could be offset due to increases in speeds and reductions in congestion (which are associated with lower MSAT emissions). Also, MSAT emissions will be lower in other locations when traffic shifts away from them. However, on a regional basis, EPA's vehicle and fuel regulations, coupled with fleet turnover, will over time cause substantial reductions that, in almost all cases, will cause region-wide MSAT levels to be significantly lower than today.

Substantial construction-related MSAT emissions are not anticipated for the proposed project, as construction is not planned to occur over an extended building period. However, construction activity may generate temporary increases in MSAT emissions in the project area.

3.5 Threatened and Endangered Species

The findings in the EA remain valid in regard to the Selected Alternative (see correspondence in Attachment E). The Threatened and Endangered Species section of the FONSI, however, has been updated to include coordination with the USFWS and TDEC that occurred through the EA circulation. The USFWS and TDEC letters are in Attachments E and G.

The USFWS sent a letter on April 18, 2013 confirming that the requirements of Section 7 have been met based on the best information available at this time and that the project is "not likely to adversely affect" Indiana bat as stated in letters dated September 21, 2011 and November 15, 2011. Measures to minimize impacts to the species were directed by USFWS and involved limiting cutting of trees to the period between October 15 and March 31. This has been included as a project commitment on the FONSI green sheet.

On May 9, 2013, a response letter from the TDEC Division of Natural Areas was received. In the correspondence, an updated list of threatened and endangered species from the state's natural heritage data program database was provided. (The list is in the TDEC letter, found in both Attachments E and G). A review of this list confirmed that the species listed in the EA are consistent with the most current list available, with the exception of the additional state-protected species presented in Table 3-4.

Table 3-4: Additional State-Protected Species Listed for Blount and Knox Counties Within Four Miles of Project Area

Common Name	Scientific Name	State Protection Status
Hellbender	<i>Cryptobranchus alleganiensis</i>	Deemed in Need of Management
Bald Eagle	<i>Haliaeetus leucocephalus</i>	Deemed in Need of Management
Budding Tortula	<i>Rhachithecium perpusillum</i>	Special Concern
Valley Flame Crayfish	<i>Cambarus deweesae</i>	State Endangered

Based on their review of the EA, TDEC concluded the vast majority of the species have been evaluated and determined to be unaffected or minimally affected by the Selected Alternative.

TDEC representatives also stated that since the completion of the ecology study, the valley flame crayfish (*Cambarus deweesae*) has been added to the state endangered species list.

According to TDEC, this species has been documented in southern Roane County and may also occur in Knox County in suitable wetlands or hydric soils.

TDEC requested additional information on wetland (WTL)-1 as described in the EA, including documentation of the presence of the crayfish species and site photos. A copy of the Ecology Report, including photos of WTL-1, was provided to TDEC for their files. WTL-1 is described as an emergent wetland and is delineated as approximately 40 feet by 60 feet in size and abuts STR-10 near the convergence with STR-11. The wetland provides water quality improvement functions. While TDEC records indicate that the species has been documented in southern Roane County, the species is considered not likely present within the proposed project's ROW, because it was not observed during the field visit conducted as part of the Ecology Report. In addition, WTL-1 likely does not provide a suitable habitat for this species because of its small size and it lacks a permanent body of water that is needed for burrowing crayfish. Finally, a desktop review of the Tennessee Natural Heritage Program database was conducted on October 1, 2013, which confirmed that the species has not been documented within a one- to four-mile radius of the project area. The ecology study is part of the EA in Appendix C.

As presently designed the proposed project will impact WTL-1, but impacts will be avoided during project design if feasible or minimized to the greatest extent practicable. Because the wetland is less than 0.1 acre in size, no mitigation efforts will be necessary, unless an investigation of the valley flame crayfish or other threatened or protected species is discovered by TDEC or TWRA at a later date.

3.6 Pedestrian and Bicycle Impacts

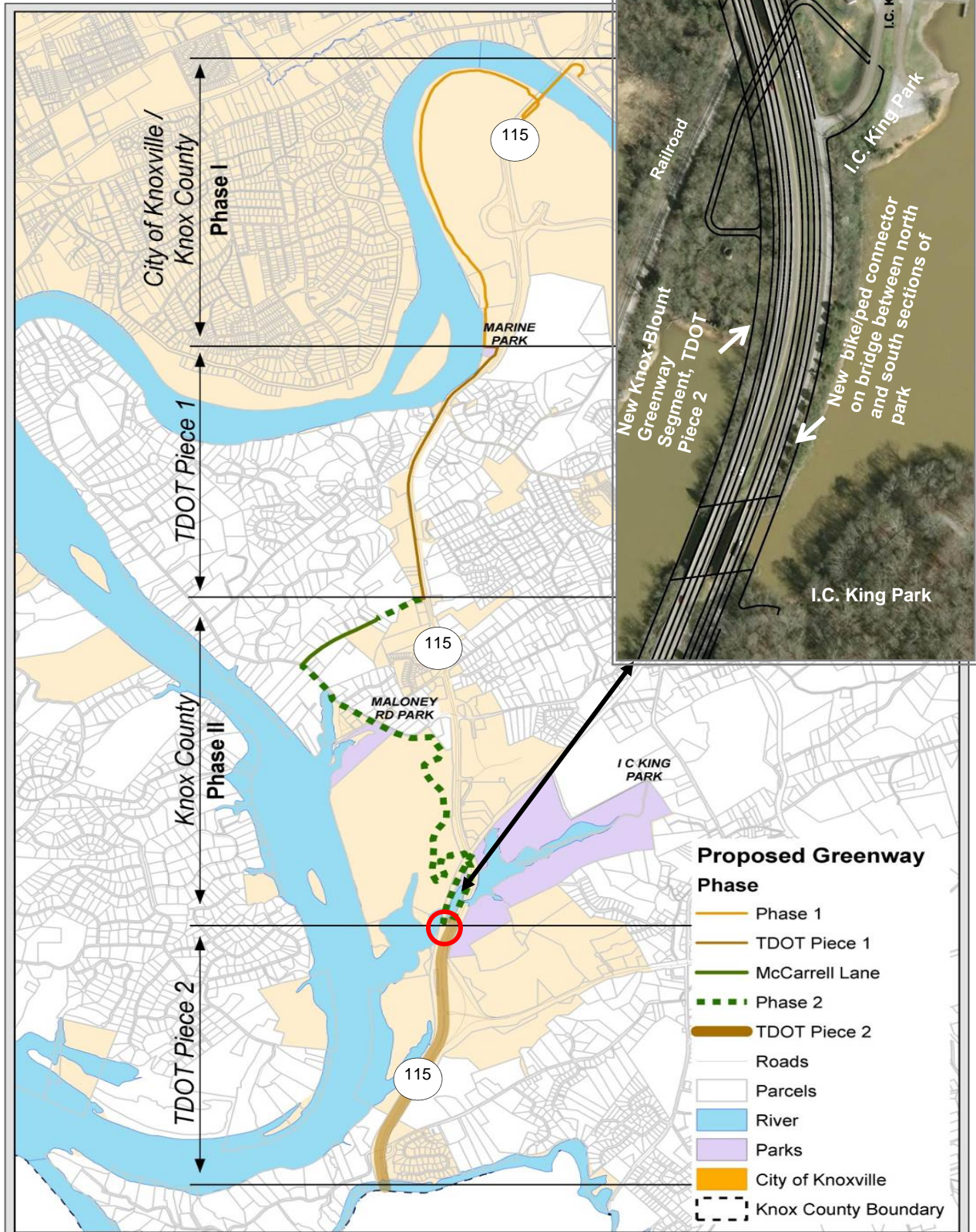
The Selected Alternative would have beneficial impacts to bicycles and pedestrians travelling within or through the corridor. As stated in the EA, the project will provide a connection between I.C. King Park and the proposed Knox/Blount Greenway, which will run northward along the west side of SR-115. To provide access between the proposed greenway segment and the park, a connector trail will be built under the new bridge. The project would also eliminate the connection between the north and south portions of the park that are separated by Knob Creek. To restore this connection, the project will continue the connector trail on the east side of SR-115 up onto a sidewalk on the east side of the new bridge enabling pedestrians and bicyclists access between the two sections of the park.

Since circulation of the EA, TDOT has coordinated with local government. Through this coordination, TDOT agreed that two segments of the Knox-Blount Greenway will be built as part of the proposed project (shown as TDOT Pieces 1 and 2 on Figure 3-3). These greenway segments will be constructed within the project ROW on the west side of SR-115 and will provide a critical connection to other sections of the Knox/Blount Greenway.

3.7 Environmental Commitments

In addition to the commitments made in the EA related to the Indiana bat and I.C King Park access, the FONSI includes two new commitments: 1) two segments of the Knox-Blount Greenway are to be built as part of the project (refer to Section 3.6 and Figure 3.3); and 2) a survey will be conducted prior to project construction to identify asbestos-containing materials on any bridges that are to be modified or demolished.

Figure 3-3: Knox-Blount Greenway and Proposed Connector Trail



Source: TDOT

4.0 PUBLIC OUTREACH

4.1 Hearing Summary

TDOT held a NEPA Public Hearing for the proposed project at Sevier Heights Baptist Church at 3232 Alcoa Highway on Thursday, June 20, 2013, from 5:00 PM to 7:30 PM. Approximately 220 members of the public and local officials attended. The purpose of the hearing was to afford the public with an opportunity to provide input into the project and for TDOT to report to the public on the findings of the EA before selecting a preferred alternative. TDOT presented a No-Build Alternative and one Build Alternative. The hearing summary and transcript is in Appendix B.

The public hearing also provided an opportunity for the public to comment on the Programmatic Section 4(f) Evaluation for I.C. King Park and to give notice of TDOT's intent to file a Determination of Section 4(f) *De Minimis* use related to the minor use of recreational lands associated with Marine Park North.

The format of the hearing included the following sessions:

- Informal session (5:00 PM to 5:30 PM): Attendees had the opportunity to look at exhibits of the Build Alternative, talk with representatives of TDOT and the project consultant team and to sign up to speak during the formal portion of the hearing,
- Formal session (5:30 PM to 7:00 PM): Team Introductions, a project overview and a brief PowerPoint presentation were given to report on the EA findings, including the Section 4(f) evaluations. Following the presentation, a question-and-answer session was held. In order to speak on the record, speakers were required to register ahead of time. The moderator called each speaker to the microphone in the order in which they registered. During the allotted time of three minutes each, 21 speakers were able to make their comment or ask questions. A panel of TDOT representatives and a moderator took questions and provided answers, and
- Informal session (7:00 PM to 7:30 PM): Although the meeting was slated to end at 7:00, TDOT continued the meeting until 7:30 to allow attendees to view exhibits and to talk one-on-one with TDOT representatives.

A court reporter was also available throughout the hearing to record the formal session discussion and to take individual comments following the formal session. Six individual oral comments were recorded at the hearing.

A handout was provided to all attendees containing information on the NEPA and ROW processes, details and depictions of the proposed project, and an explanation of the comment process. TDOT also provided comment forms for public use in submitting comments. Comment forms could be submitted at the meeting or mailed to TDOT prior to the July 11, 2013 deadline.

4.2 Public Comments Received

The public had several ways to comment on the proposed project and to have those comments included in the official public record:

- As a speaker during the hearing's formal comment session,
- By making an oral statement to the court reporter at the public hearing,
- By submitting a comment card either at the hearing or by mail, and
- By sending letters and emails.

During the comment period, 91 public comments were received. Table 4-1 provides a summary of the comments received by method of response and preference of alternative.

Table 4-1: Summary of Public Hearing Comments – Alternative Preference

Method of Commenting	No-Build Alternative	Build Alternative	No Preference*/ Other**	Total
Comments during Formal Portion of Hearing	1	2	18	21
Oral Comments to Court Reporter	0	0	5	5
Comment Forms	3	8	38	49
Individual Letters or Emails	0	6	10	16
Total Responses	4	16	71	91

* No preference in alternative and/or suggestions for improving proposed design.

** Comments regarding issues not associated with the project.

4.2.1 Public Comments Made During Formal Portion of the Hearing

Following the formal PowerPoint presentation, TDOT opened the floor for registered speakers to make a comment and/or ask a question. Twenty-one people registered to speak during this time. Of the 21 speakers, one expressed their opposition to the proposed project. Two comments were specifically in favor of the Build Alternative. The remaining comments/questions concerned the current conditions of the corridor and a request for additional detail regarding the design.

4.2.2 Oral Comments to Court Reporter

Five individuals provided statements to the court reporter after the formal portion of the hearing. The statements made to the court reporter did not specifically indicate their position for or against the project and the Build Alternative. Comments suggested modifications to the project design, concerns on project impacts, and the request for inclusion of a greenway as part of a larger greenway system in Knox and Blount Counties.

4.2.3 Comment Cards

Public comments could be submitted on the comment form that was distributed at the public hearing. This form was also reported as available for download from TDOT's SR-115 project website. In total, 49 completed comment forms were submitted. The majority of the comment forms received (38) did not specifically indicate a position for or against the project. The comments mainly focused on the current conditions of the corridor, suggested modifications to the design of the Build Alternative, and concerns about project impacts. Eight comments were in support of the Build Alternative, while three comments were against the project in its entirety.

4.2.4 Emails and Letters

TDOT received one letter and 15 emails during the comment period. Ten of the emails and the letter asked for modifications to the design of the Build Alternative and had concerns about

project impacts. Six of the emails expressed support for the Build Alternative. There were no emails in opposition.

4.2.5 Summary of Comments

Tables 4-2 and 4-3 summarize the comments made in the letters, emails, and comment forms (formal and informal) and during the public hearing, by those persons supporting the project and those opposed to the project. (It should be noted that some individuals provided multiple comments on their position). There were no comments related to the Programmatic Section 4(f) Evaluation for I.C. King Park or the Section 4(f) *De Minimis* Determination for Marine Park North.

Table 4-2: Summary of Public Comments Supporting the Project

Comment	Disposition
The project will address the safety, access and congestion issues of the corridor. The project improvements would slow traffic and make the corridor safer. (1 comment)	The proposed project intends to address transportation needs, which include increasing the capacity of SR-115 and the level of service, correcting roadway deficiencies, and reducing crashes/improving safety.
The project is needed for economic development. It is important for the future. (3 comments)	The proposed project improves the regional transportation network and will enhance the area for new and existing businesses.
Support for the Build Alternative and encouraging TDOT to move the project forward as soon as possible. (4 comments)	TDOT is working hard to move this project forward to construction.
The project as proposed should be extended to McGhee Tyson Airport. (1 comment)	There are no plans to extend the project southward to the airport at this time. However, improved access is included in the relocated Alcoa Highway Bypass project, which is currently under development by TDOT.
I like the concept of the proposed access roads and interchanges. (1 comment)	No response needed.
This will help to bring economic development to the Martha Washington Heights area. (1 comment)	The proposed project improves the regional transportation network and will enhance the area for new and existing businesses.
The project is a better solution than simply adding additional traffic lights. (1 comment)	No response needed.
The No-Build Alternative should not be considered as an option. (1 comment)	The No-Build Alternative is required by federal regulations to be evaluated in a NEPA EA as it provides a baseline for comparison to studied build alternatives. Your comment is noted.

Table 4-3: Summary of Public Comments Opposing the Project

Comment	Disposition
The project will not solve the traffic issues; rather it will turn Alcoa Highway into an interstate. (1 comment)	Traffic on the improved SR-115 roadway will not travel at interstate highway speeds, and streets and commercial areas that can be accessed now by a right turn will retain this access.
There is no need for – or do not see the benefit of – this project. (1 comment)	The project's purpose of congestion relief, correction of roadway deficiencies, and improved safety will be accomplished by construction of additional lanes on the existing facility, and the proposed changes in access control will benefit all roadway users.
The project will cause a decline in property values. (1 comment)	Notable decreases in property values are not anticipated as the project will improve the existing highway and properties are already adjacent to the highway. Access may change, but all properties will have access and the access will be safer.
The project is too expensive and will take too long to complete. (1 comment)	The analyses completed for this project have shown that the project need (improve safety, address future existing and future traffic congestion) justifies the expense. TDOT is working to move the project forward and plans to advance each section as funds become available.
The impact to traffic during construction and money involved are not worth the suggested benefit. (1 comment)	Traffic flow will be affected during construction; however, the roadway will remain open and access to properties will be retained during construction. These impacts are short-term and the benefit of the project is long-term.
This will eliminate the greenspace on Alcoa Highway and will have negative environmental impacts. (1 comment)	<p>Much of the greenspace along the corridor is undeveloped land that will not be affected by the project. The project includes building two segments of greenway that will link to other segments of the Blount-Knox Greenway.</p> <p>Technical studies undertaken in support of the NEPA EA have revealed no significant environmental impacts.</p>
This will cause negative impacts to the Martha Washington Heights neighborhood from additional traffic and noise. (1 comment)	TDOT has conducted technical studies that examine impacts from noise and congestion. The studies indicate that the proposed project would have no substantial noise increases to properties immediately adjacent to the road and no noise impacts were identified to the neighborhoods off SR-115. Additionally, with the frontage and collector roads, traffic in neighborhoods is not projected to increase.

Table 4-4 summarizes the comments made by those persons who did not specify opposition or support of the project. These comments included requests for additional information, suggestions for changes to the preliminary design, or suggestions for additional, non-design improvements to the project.

Table 4-4: Summary of Public Comments that did not Oppose or Support the Project

Comment	Disposition
There needs to be an acceleration/deceleration lane that links the project to I-140. (1 comment)	Through engineering analyses, TDOT has determined that neither acceleration or deceleration lanes are needed at the I-140 (Pellissippi Parkway) interchange. The existing interchange has two northbound and two southbound lanes under I-140. The proposed project would add a third lane in each direction to SR-115 on the north side of this interchange. The third southbound lane would be an "Exit Only" lane to I-140, so traffic does not have to decelerate to exit. The entrance ramp from I-140 becomes the third northbound lane of SR-115, so traffic does not have to accelerate and merge because it has its own lane.
Subdivisions along Alcoa Highway need to have a frontage road. (1 comment)	TDOT has been working with local residents since the late 1990s and in response to public comment has added frontage and collector roads at various locations along the proposed project. TDOT will continue to refine opportunities for access in the design phase if specific needs are identified.
The project needs to incorporate an element of safe pedestrian and biking access. (4 comments)	TDOT is evaluating the inclusion of pedestrian facilities on new bridges at interchanges crossing over SR-115. In addition, TDOT is including two segments of the Knox-Blount Greenway in project plans.
Will additional lanes cause speed limits to increase? (11 comments)	The proposed posted speed limit will not increase along the corridor. As is the case today, local government will be responsible for enforcing speed limits.
The access should be conducive to new and existing businesses. (2 comments)	Most existing right turns into businesses along the corridor will remain. Access via interchanges and frontage and collector roads will be provided as part of the project. Such access will be conducive to new and existing businesses.
How will the devaluation of property be assessed? (1 comment)	The TDOT Right-of-Way Division will have an independent appraisal done for each whole or partial property to be acquired. Experienced appraisers will personally visit each property to be acquired and will confer with property owners during the process of appraising the property.
The traffic congestion projections need to be updated to 2013. (1 comment)	The traffic study was updated in January 2013. On January 15, 2013, the Knoxville Regional TPO issued a letter to TDOT stating that they are in general agreement with the traffic numbers utilized for the traffic analysis presented in the SR-115 EA.
There are major safety concerns along the corridor. (6 comments)	Comment noted.
Consider temporary solutions while the project is being developed. (1 comment)	TDOT will continue to monitor the corridor and may consider spot improvement projects to address any critical safety issues identified while the SR-115 project is in the planning phase.

Comment	Disposition
Additional signalization is needed for the corridor. (4 comments)	The project contains no traffic signals as it is a partial access-controlled facility, with access at interchanges. The stop-and-go traffic flow that signals create would be in conflict with the project goals of improving traffic flow and safety and eliminating congestion.
Will properties lose their access? (3 comments)	TDOT will acquire any properties for which no access can be provided. For other properties, while access may change, it will not be eliminated.
Consider addressing signage issues; there needs to be more signage and roads with the same name should be eliminated. (1 comment)	TDOT will work to provide clear signage on roadway elements that are part of its project. Other local roadway signage is the responsibility of local government.
Consider using a grassy median instead of a concrete (median). (1 comment)	Use of a grassy median is not feasible as it would require too much ROW (and additional property impacts) to develop a median that would be wide enough to be safe.
Ensure there are appropriate turning radii for large/commercial trucks and equipment. (1 comment)	TDOT will ensure that appropriate turning radii are provided for large/commercial trucks and equipment.
When will residents and business owners know about property acquisition? (3 comments)	The schedule for property acquisition is not yet known, as it is contingent upon identification of funding for future project phases. Property acquisition issues will be addressed during the final design phase of the project once it has been determined the locations where additional ROW will need to be acquired. Following completion of ROW plans, a ROW meeting will be conducted where specifics of the appraisal and acquisition processes and the relocation program will be discussed with property owners and occupants of affected properties. TDOT will pay a fair market value for properties impacted by resident displacement/relocation and ROW requirements, and provide sufficient notice of intent to acquire the property to minimize any harm. The relocation of displaced households, businesses, and any other affected party will be administered in accordance with the provisions and procedures of the Tennessee Uniform Relocation Assistance Act of 1972 and the Uniform Relocation Assistance and Real Property Acquisition Act of 1970 (Public Law 91-646). Comparable replacement housing will be provided to all residential displacements under the provisions of the above laws.
Left turns are a major safety concern for the corridor. (1 comment)	As noted above, the proposed improvements include a greater level of access control through construction of new grade-separated interchanges and installing a median barrier for the length of the project. As such, left turns on to and off of SR-115 will be eliminated.
Consider adding more safety elements such as guardrails. (1 comment)	Comment noted. TDOT will install guardrail as warranted along the project alignment.

Comment	Disposition
What is the status of the Alcoa Bypass project? (2 comments)	The Alcoa Highway Bypass is a separate project that when complete, would intersection SR-115 at Singleton Station Road within the limits of the proposed SR-115 improvement project. Only the first section of the Alcoa Highway Bypass project (Hall Road to the proposed interchange serving McGhee Tyson Airport) is included in the TIP. The project is currently in the preliminary design phase.
Greenways were promised by previous administrations to be part of this project. Please make sure greenways are included in the design, which will be a part of a larger Knox/Blount County greenway system. (8 comments)	TDOT has informed local government that two segments of the Knox-Blount greenway are included in the project plans.
There is a bird sanctuary in the Martha Washington Heights neighborhood that should be protected from the project. (1 comment)	The project is not anticipated to impacts birds outside the project area.
Frontage roads should be extended through the entire corridor. (1 comment)	Frontage/collector roads are designed to provide access to residences and businesses along the corridor and access to SR-115 via the proposed and existing interchanges. TDOT will continue to explore frontage and collector road options in the design phase.
Confirm the eligibility of a historic home and determine if any impacts will occur at that location. (1 comment)	The Architectural/Historical study completed by TDOT in October 2001, determined the Barber House eligible for the National Register of Historic Places (NRHP). On November 6, 2001, the Tennessee State Historic Preservation Office (SHPO) concurred that the property would not be adversely affected under Section 106 of the National Historic Preservation Act (NHPA). (TDOT provided the property owner with additional information on NRHP eligibility and potential project impacts following the public hearing.)
What is the order for constructing the segments? (3 comments)	The project is divided into four segments. At this time, it is uncertain as to the order in which the segments will be built. Additional information will be made available as funding is identified.
Was the traffic analysis from Alcoa Highway Bypass Environmental Assessment used in this study? (1 comment)	As indicated at the public hearing, the traffic analysis is based on the entire regional model provided to TDOT through the Knoxville TPO, so it takes into consideration the traffic situation from both a localized and regional perspective. Proposed projects such as Alcoa Highway Bypass and the Pellissippi Parkway Extension are included in the regional model.
The project will need to consider impacts to neighborhoods for increased traffic, impacts to neighborhood entrances and flooding issues. (1 comment)	TDOT has studied the impacts of traffic on the neighborhoods and is of the opinion that, with the collector and frontage road system, the project will not have adverse impacts to neighborhoods. Regarding flooding, the project design must be developed so that there are no new flooding issues or increases to existing flooding issues as a result of the project. In general, existing flooding issues in neighborhoods need to be communicated to local government.

Comment	Disposition
What is the timing for the design phase? What other phases occur beyond design? (1 comment)	The NEPA phase is anticipated for completion in the Winter of 2014. At this time, only the design and ROW phases have been funded. The design phase is scheduled to begin in 2014. The construction phase is not yet funded.
Consider making the corridor a limited-access highway. (1 comment)	The proposed project is to be a limited access roadway.
Will noise walls be considered as part of the project? (1 comment)	TDOT has conducted technical studies that look at impacts from noise. The studies indicate that the proposed project would have no substantial noise increases. Studies also revealed that construction of barriers to minimize noise, such as noise walls, would not be "reasonable" according to TDOT 2011 Noise Policy, which follows federal policy.
Consider incorporating merge lanes as part of the project. (1 comment)	Merge lanes are incorporated in the proposed interchange ramps. Adding a merge lane at every intersecting road between interchanges (where right turns are allowed) would create a greater impact to neighborhoods and property adjacent to the project and increase the project cost. A larger turning radius is being proposed for many of these intersecting roads to allow traffic to enter or exit at a speed that will allow a safer merge or exit.
Will the transcript of the Public Hearing be made available and what agencies will be responsible for the final decisions of the project? (1 comment)	The transcript of the public hearing is available on TDOT's website, located at: http://www.tdot.state.tn.us/sr115/ . TDOT, considering impacts identified in the NEPA EA and public, local government, and agency input, selects an alternative, either the No-Build Alternative or the Build Alternative.
Consider including a map of all current projects in the region. (1 comment)	Comment noted.
Consider widening Maryville Pike, SR-33. (1 comment)	Comment noted.
Consider acquiring additional ROW for transit; coordinate with Knoxville Area Transit. (1 comment)	Comment noted.
Are impacts from rockfall and rockfall mitigation a part of the project? (1 comment)	As indicated at the public hearing, TDOT has a rockfall mitigation program throughout the state. Identified rockfall locations will be considered and addressed in project design.
How will temporary construction impacts be addressed? Where will equipment staging be located? (1 comment)	As with any major transportation project, short-term, construction-related impacts (e.g., noise and alterations in access and traffic patterns) will occur. A traffic management plan will be developed and implemented in the construction phase. Contractors will be required to comply with TDOT's <i>Standard Specifications</i> and local ordinances in regard to noise and hours of work. At this time, equipment staging areas have not been identified. This will occur in the construction process.

Comment	Disposition
An interchange for Woodson Drive and Montlake Drive is needed. (1 comment)	During the conceptual design of the proposed project, interchanges were determined unnecessary for Woodson Drive and Montlake Drive. Instead, a series of collector roads are proposed.
Consider design changes that would accommodate emergency vehicle access. (1 comment)	Construction of the proposed project is anticipated to improve traffic flow and, consequently, to shorten emergency response time. Emergency vehicle access is always considered by TDOT in project design and development.
TDOT and local government should incorporate higher design standards for businesses along Alcoa Highway. (1 comment)	Building design standards are the responsibility of local government regulations.
Consider intensive landscaping as part of the project. (1 comment)	Comment noted.
Coordinate with the (Knoxville-Knox County Metropolitan Planning Commission) South County Sector Plan. (1 comment)	TDOT has and will continue to work with local, state, and federal agencies in the design, ROW acquisition, and construction phases of the project.
Consider additional bridges to help with traffic crossing over the Tennessee River. Consider developing an alternative route for Cherokee Trail to cross the Tennessee (2 comments)	The bridge over the Tennessee River at the north end of this project has been recently improved. This project proposes no other improvements over the Tennessee River.
This project should address the entrance to the UT Medical Center. (1 comment)	TDOT is working with local entities on the design of the interchange at Medical Center Parkway so that safe access and improved traffic circulation is provided.
Concerns over the project's disruptions to the ecosystem. (2 comments)	TDOT has conducted technical studies for the natural environment and has identified no significant impacts to the ecosystem.
Consider the increased traffic congestion from the Cherokee Farm development. (1 comment)	The Knoxville TPO traffic model, which was used for the SR-115 traffic analysis, includes this development.
Concern over the loss of Alcoa Way Shopping Center and negative economic effects. (2 comments)	TDOT has provided interchanges and collector and frontage roads that will provide safe access to the shopping center.
Consider increased road widths for intersecting streets. (1 comment)	TDOT will make improvements at a number of intersections so that right turns can be more safely made off of and onto SR-115 and streets can be safely accessed from collector and frontage roads or interchanges.

5.0 COMMENTS ON THE ENVIRONMENTAL ASSESSMENT

Copies of the EA were mailed to 32 federal, state, regional, and local agencies and other interested parties. Ten agencies provided written comments on the EA: USCG, TDEC - Division of Solid Waste, TDEC - Division of Remediation, TDEC - Division of Air Pollution Control, TDEC - Division of Natural Areas, USFWS, US Department of Agriculture (USDA), Federal Aviation Administration (FAA), the TPO, and the Knoxville-Knox County Metropolitan Planning Commission (MPC). Table 5-1 summarizes the comments received and provides a response to/disposition of comments received. Copies of the correspondence provided by the agencies are in Attachment G.

Table 5-1: Summary of Agency Comments on the EA

Comment	Disposition
TDEC – Division of Solid Waste	
<p>No solid waste issues in the vicinity of this project location. Also, no hazardous waste Treatment-Storage-Disposal (TSD) facility located in the neighborhood of this project location. Requested continued coordination through later project phases.</p>	<p>Comment regarding solid and hazardous waste is noted. TDOT will continue to coordinate with TDEC.</p>
U.S. Coast Guard	
<p>Little River and Knob Creek are located within the embayments of the Tennessee River and are considered navigable waterways of the United States for bridge administration purposes at the bridge sites. Improvements to existing bridges that will alter the navigational clearances provided by the bridges or significantly alter the structure of the bridges will require a bridge permit or bridge permit amendment.</p>	<p>TDOT will request the proper permits for any improvements to existing bridges that will alter the navigational clearances provided by the bridges or significantly alter the structure of the bridges.</p>
<p>Ensure the U.S. Coast Guard is shown as a Cooperating Agency.</p>	<p>The FONSI includes USCG as a Cooperating Agency.</p>
<p>Include a section in the document entitled "Navigation Impacts".</p>	<p>The FONSI includes a section that discusses navigation impacts.</p>
<p>If bridge permits are required for the project, the Coast Guard will need a Water Quality Certificate from the appropriate state agency, which states that the project complies with the provisions of Section 401 of the Clean Water Act.</p>	<p>TDOT will coordinate with TDEC, which is the agency responsible for Section 401 permits.</p>

Comment	Disposition
TDEC – Division of Remediation	
Concluded that there are no known active sites on or adjacent to the property in question.	No response needed
USFWS	
Concurred with TDOT's determination of "not likely to adversely affect" for the Indiana bat due to negative survey results. This survey will be valid until April 1, 2014.	TDOT will continue to coordinate with USFWS.
USDA - Natural Resources Conservation Service	
Agency responded with no comments or questions to the document.	No response needed.
TDEC – Division of Air Pollution Control	
Identified corrections needed to the EA for National Ambient Air Quality Standards.	The FONSI includes an updated air quality assessment that satisfies the latest standards. TDOT will continue to coordinate with TDEC.
Request that the local air pollution control program have the opportunity to review the information on the project.	The FONSI contains an updated air quality assessment and includes coordination with the local air pollution program.
FAA	
Identified Sky Ranch Airport as the closest airport to the project. Requested to be notified as the project moves forward.	TDOT will continue to coordinate with FAA.
TDEC – Division of Natural Areas	
Provided the latest data on species listed in the state's natural heritage database and their proximity to the project.	The FONSI includes an updated species list as provided by TDEC.
Requested further coordination throughout the project; provided specifications in project design.	TDOT will continue to coordinate with TDEC throughout project permitting and design and will make design accommodations as feasible.
Knoxville Regional TPO	
The TPO, the City of Knoxville and Knox County understood that TDOT would build two segments of greenway along SR-115 as part of this project.	TDOT will include two segments of the greenway in the proposed project plans.

Comment	Disposition
<p>It would be preferred to limit access to the highway only to the proposed interchanges to the extent possible through the development of a more complete frontage road system that would tie existing public roads and private driveways together.</p>	<p>The project is currently based on a preliminary design. This design was derived from the 2000 APR, which was developed through coordination with the Knoxville Regional TPO and local government representatives.</p> <p>Frontage roads are designed to provide access to residences and businesses along the corridor and access to SR-115 via the proposed and existing interchanges. TDOT will continue to explore design options in the design phase, including the modification of frontage roads.</p>
<p>The TPO would like the project to accommodate future transit and rideshare use along the corridor by providing park-and-ride lots at key points, potentially on property that TDOT would already have to acquire for the road project.</p>	<p>Park-and-ride lots are not in the preliminary design. TDOT will coordinate with the local and regional transit agencies on this issue, but acquiring additional ROW is not considered a viable option at this time as part of the proposed SR-115 improvement.</p>
<p>The EA should more completely address coordination and timing between this project and the Alcoa Highway Bypass project.</p>	<p>The Alcoa Highway Bypass is a separate project but TDOT is coordinating development of both projects. Only the first section of the Alcoa Highway Bypass project (Hall Road to the proposed interchange serving McGhee Tyson Airport) is included in the TIP. The ROW acquisition phase was programmed for this first section for 2012. Coordination and timing will be influenced by funding availability.</p>
<p>Between Marine Park and Woodson Drive, the ROW for SR-115 is severely constrained. The EA should include additional discussion of the environmental impacts of widening in this area.</p>	<p>TDOT has studied the impacts to this area. Based on these studies, no significant impacts are predicted. The assessment was based on a number of technical studies ranging in scope from aquatic and terrestrial resources to geologic conditions and social impacts.</p> <p>While the potential for rockfall in this area is recognized, TDOT has a statewide rockfall mitigation program in place to handle any potential issues. Further, TDOT will design the project to address rockfall areas.</p>
<p>Bicycle and pedestrian accommodation along all new collector and access roads and through all interchanges is needed.</p>	<p>TDOT will continue to explore options in the design phase. To date, TDOT has agreed to include two segments of the Knox-Blount Greenway in the SR-115 project and has provided access to I.C. King Park from the greenway.</p>
<p>All improved streets and intersections should be Complete Streets that provide safe bicycle and pedestrian access and connectivity to neighborhoods.</p>	<p>Comment noted. See above reply.</p>

Comment	Disposition
<p>Identified several corrections needed and points of clarification to the EA regarding demographics, land use, bicycle accommodations and implementation of Complete Street policies.</p>	<p>Where warranted, corrections, clarifications, and revisions were made as part of the FONSI and are included in the Summary of Environmental Impacts section.</p>
<p>Knoxville – Knox County MPC</p>	
<p>There is a lack of coordination between the local land use plans and the proposed project.</p>	<p>TDOT has coordinated with the Knoxville Regional TPO and the Knoxville – Knox County MPC on a number of occasions. Coordination efforts for this project reach as far back as 2000 during the development of the APRs and feasibility studies that served as the foundation for the proposed project. TDOT will continue to coordinate with the MPC.</p>
<p>The Build Alternative only demonstrates a two-way collector road on the eastern side of the highway for access to commercial and office uses and a connection to the Martha Washington Heights neighborhood via collector 5.</p>	<p>Comment noted. TDOT will continue to refine access during the project design phase.</p>
<p>Identified several corrections needed and points of clarification to the EA regarding demographics, land use, bicycle accommodations and implementation of Complete Street policies.</p>	<p>Where warranted, corrections, clarifications and revisions were made as part of the FONSI and are included in the Environmental Impacts section.</p>

6.0 REFERENCES

Blount County

2000 *Blount County Conceptual Land Use Plan*

Cordeiro, J. and Thoma, R.F.

2010 *Cambarus deweesae*. In: IUCN 2013. IUCN Red List of Threatened Species. Version 2013.1. <www.iucnredlist.org>. Downloaded on 21 October 2013.

Federal Highway Administration (FHWA)

2006 *Interim Guidance on Air Toxic Analysis in NEPA Documents*, February 3, 2006

2012 *Interim Guidance Update on Air Toxic Analysis in NEPA Documents*, December 6, 2012

2012 *A Methodology for Evaluating Mobile Source Air Toxic Emissions Among Transportation Project Alternatives*

Federal Transit Administration (FTA)

Circular 4701.01

Knoxville Regional Transportation Planning Organization (TPO)

2003 *Knoxville-Knox County General Plan*

2010 *Fiscal Year 2014-2017 Transportation Improvement Program*

2012 *South County Sector Plan for Knox County*

2013 *Long Range Regional Mobility Plan*

Sain Associates

2012 *Updated Traffic Forecasts and Capacity Analysis*

2013 *Revision of Future Laneage memorandum*

Tennessee Department of Transportation (TDOT)

1989 *Feasibility Study*

2000 *Advanced Planning Report for SR-115 From 250 Feet South of Airbase Road to the Bridge Over the Tennessee River in Blount and Knox Counties*

2011 *Finding of No Significant Impact for State Route 115 (US 129) Alcoa Highway Bypass from Hall Road (State Route 35)/State Route 115 South of Airport Road to near Singleton Station Road, Approved August 8, 2011*

2013 *Top Management Report, October 2013*

University of Tennessee (UT)

2009 *Cherokee Farm Campus Master Plan*

Attachment A:
Knoxville Regional TPO
Transportation Improvement Program (TIP)
Excerpt

Knoxville Regional Transportation Planning Organization

Transportation Improvement Program

Fiscal Year 2014 – 2017

October 2013

Adopted by
The TPO Technical Committee on October 8, 2013
The TPO Executive Board on October 16, 2013

KNOXVILLE REGIONAL TRANSPORTATION PLANNING ORGANIZATION
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The Knoxville Regional Transportation Planning Organization ensures compliance with Title VI of the Civil Rights Act of 1964; 49 CFR, part 26; related statutes and regulations to the end that no person shall be excluded from participation in or be denied the benefits of, or be subjected to discrimination under any program or activity receiving federal assistance from the U.S. Department of Transportation on the grounds of race, color, sex, or national origin.

For additional information on Title VI and Environmental Justice please contact the TPO or see the information on our website at www.knoxtrans.org. Any person who believes he or she has been discriminated against should contact:

Knoxville Regional Transportation Planning Organization
Attention: Title VI Coordinator
400 Main Street, Suite 403
Knoxville, TN 37902
Telephone: (865) 215-2500

Knoxville Regional Transportation Planning Organization

TRANSPORTATION IMPROVEMENT PROGRAM FY 2014-2017

TIP No.	<input type="text" value="2014-003"/>	Revision No.	<input type="text" value="0"/>
TDOT PIN	<input type="text" value="100241.01"/>	Mobility Plan No.	<input type="text" value="09-216"/>
Project Name	<input type="text" value="Alcoa Hwy. (SR-115 / US-129)"/>		
Lead Agency	<input type="text" value="TDOT"/>		
Total Project Cost	<input type="text" value="\$73,200,000"/>		

Project Description	<input type="text" value="Reconstruct from 4 to 6 lanes with 2 auxiliary lanes between Singleton Station Rd and Topside Rd (SR 333)."/>		
Termini/Intersection	<input type="text" value="Pellissippi Pwy to Knox / Blount County line"/>		
Counties	<input type="text" value="Blount"/>		
City/Agency	<input type="text" value="Alcoa"/>		
Length	<input type="text" value="3.2"/> (miles)	Conformity Status	<input type="text" value="Non-Exempt"/>

Additional Details	<input type="text" value="Adjusted in 11/08 from 2010 to 2011."/>		
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Programmed Funds

FY	Type of Work	Funding Type	Total Funds	Federal	State	Local	Other
2015	ROW	NHPP	\$36,200,000	\$28,960,000	\$7,240,000	\$0	\$0
Total			\$36,200,000	\$28,960,000	\$7,240,000	\$0	\$0

Revision Date	<input type="text"/>
Revision Details	<input type="text"/>
Previous TIP No.	<input type="text" value="2008-002, 2011-003"/>



Knoxville Regional Transportation Planning Organization

TRANSPORTATION IMPROVEMENT PROGRAM FY 2014-2017

TIP No.	<input type="text" value="2014-004"/>	Revision No.	<input type="text" value="0"/>
TDOT PIN	<input type="text" value="100241.02"/>	Mobility Plan No.	<input type="text" value="09-628"/>
Project Name	<input type="text" value="Alcoa Hwy. (SR-115 / US-129)"/>		
Lead Agency	<input type="text" value="TDOT"/>		
Total Project Cost	<input type="text" value="\$34,459,500"/>		

Project Description	<input type="text" value="Widen from 4 to 6 lanes, including pedestrian and bicycle facilities."/>		
Termini/Intersection	<input type="text" value="Knox / Blount County line to Maloney Rd"/>		
Counties	<input type="text" value="Knox"/>		
City/Agency	<input type="text" value="Knoxville"/>		
Length	<input type="text" value="2.2"/> (miles)	Conformity Status	<input type="text" value="Non-Exempt"/>

Additional Details	<input type="text" value="Adjusted ROW in 11/08 from 2010 to 2011."/>		
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Programmed Funds

FY	Type of Work	Funding Type	Total Funds	Federal	State	Local	Other
2016	ROW	NHPP	\$6,100,000	\$4,880,000	\$1,220,000	\$0	\$0
Total			\$6,100,000	\$4,880,000	\$1,220,000	\$0	\$0

Revision Date	<input type="text"/>
Revision Details	<input type="text"/>
Previous TIP No.	<input type="text" value="2004-033, 2006-002, 2008-003, 2011-004"/>



Knoxville Regional Transportation Planning Organization

TRANSPORTATION IMPROVEMENT PROGRAM FY 2014-2017

TIP No.	<input type="text" value="2014-069"/>	Revision No.	<input type="text" value="0"/>
TDOT PIN	<input type="text" value="100241.03"/>	Mobility Plan No.	<input type="text" value="09-653"/>
Project Name	<input type="text" value="Alcoa Hwy. (SR-115 / US-129)"/>		
Lead Agency	<input type="text" value="TDOT"/>		
Total Project Cost	<input type="text" value="\$41,200,000"/>		

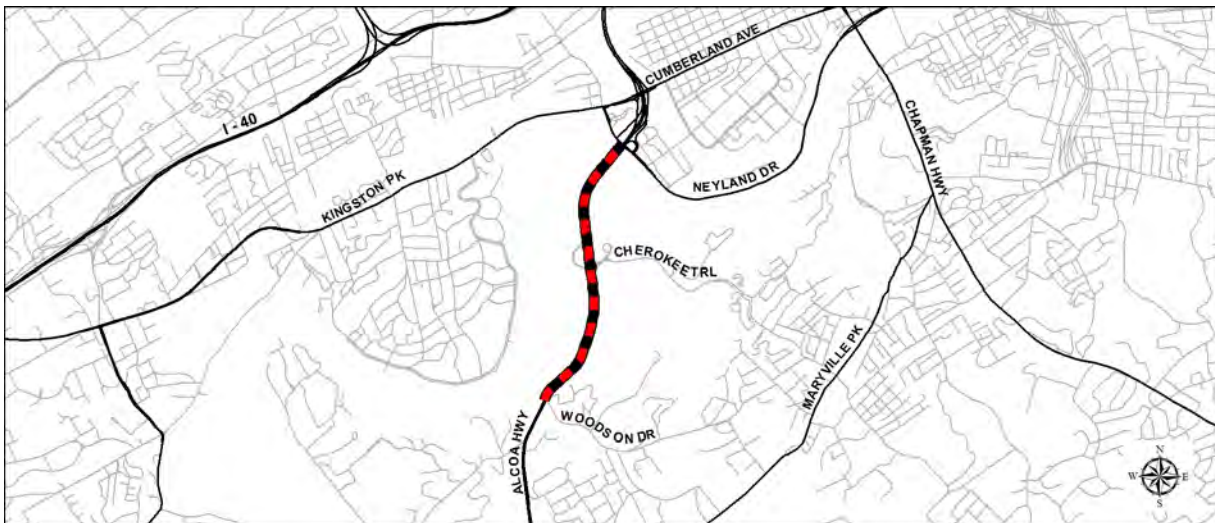
Project Description	<input type="text" value="Widen 4-lane to 6-lane including pedestrian and bicycle facilities."/>		
Termini/Intersection	<input type="text" value="From Woodson Dr. to Cherokee Trail interchange"/>		
Counties	<input type="text" value="Knox"/>		
City/Agency	<input type="text" value="City of Knoxville"/>		
Length	<input type="text" value="1.6"/> (miles)	Conformity Status	<input type="text" value="Non-Exempt"/>

Additional Details

Programmed Funds

FY	Type of Work	Funding Type	Total Funds	Federal	State	Local	Other
2015	ROW	NHPP	\$14,000,000	\$11,200,000	\$2,800,000	\$0	\$0
Total			\$14,000,000	\$11,200,000	\$2,800,000	\$0	\$0

Revision Date	<input style="width: 100%;" type="text"/>
Revision Details	<input style="width: 100%;" type="text"/>
Previous TIP No.	<input style="width: 500px;" type="text" value="2011-002"/>



Attachment B:

Conceptual Stage Relocation Plan (CSRP)

Hayes, Robbie

From: David S. Goodman <David.S.Goodman@tn.gov>
Sent: Tuesday, October 15, 2013 10:51 AM
To: Hayes, Robbie
Subject: RE: CSRP for Existing Alcoa Hwy SR-115 Blount & Knox; PIN 100241.00

Robbie,

I've looked over the CSRP in question and see that it was prepared, approved, and submitted in the later part of May of 2012.

In my opinion, the findings of this report are still valid.

David S. Goodman

Transportation Specialist 1
Tennessee Dept. of Transportation
505 Deaderick Street, Suite 600
Nashville, TN. 37243
Office: 615-253-1133
Fax: 615-532-1548
Email: David.S.Goodman@tn.gov

<http://www.tdot.state.tn.us/>



**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

RIGHT OF WAY DIVISION
SUITE 600, JAMES K. POLK BUILDING
505 DEADERICK STREET
NASHVILLE, TENNESSEE 37243-1402
(615) 741-3196

JOHN C. SCHROER
COMMISSIONER

BILL HASLAM
GOVERNOR

CONCEPTUAL STAGE RELOCATION PLAN

Blount & Knox Counties

SR-115 (Alcoa Hwy) from North of SR-140 to North of Cherokee Trail

	Pin Number	Federal No.	State Project No.
100241.00	100241.01	STP-NHE-115(26)	05005-1231-14
	100241.02	STP-NHE-115(27)	47026-1269-14
	100241.04		47026-1263-04 PE-D
			47026-2268-04 ROW
	100241.03	STP-NHE-115(27)	47026-1270-14

PROJECT INFORMATION: The Tennessee Department of Transportation (TDOT) is proposing to widen and improve 7.533± miles of SR-115 (Alcoa Highway) in order to improve safety and relieve traffic congestion. SR-115 is the major connector between the city of Maryville and the downtown area of Knoxville, 17 miles to the north in Knox County.

According to the submitted preliminary road plans, typical proposed sections will contain either three or four 12-foot traffic lanes in each direction with 12-foot outside stabilized shoulders, 10 foot inside shoulders, concrete median barriers, and right-of-way (ROW) of up to 180 feet, depending on construction requirements.

For more specific detail regarding typical sections and other information for each of the four project segments, refer to the separately attached “PLANS MARKED FOR CSRP.”

AREA INFORMATION: The subject area is located in the north central portion of Blount County and extends north into Knox County. Current land use in the project area is primarily commercial along with scattered residential, agricultural, and undeveloped areas. According to the U. S. Census Bureau, the population for Blount County in 2010 was 123,010. This reflects a 16.2% increase since the 2000 census. The population of Maryville in 2010 was 27,465 and reflects an 18.8% increase since the 2000 census.

According to the U. S. Census Bureau, the population for Knox County in 2010 was 432,226. This reflects a 13.1% increase since the 2000 census. The population of Knoxville in 2010 was 167,674 and reflects a 2.9% increase since the 2000 census.

DISPLACEMENTS:

RELOCATIONS	
SINGLE FAMILY UNITS	21
MULTI FAMILY UNITS	21
MOBILE HOMES	4
BUSINESSES	24

DISPLACEMENT EFFECTS AND ANALYSIS

Residential Altogether, construction is expected to cause 46 (forty six) residential displacements. The expected displacements consist of 21 (twenty one) single family residences, four mobile homes (described below), and two apartment buildings with a total of 21 tenants. Based field inspection, the single family residences appear to be typical for the area in terms of size and style. It is unknown if the occupants are owners or tenants.

Multifamily Construction is expected to displace 21 (twenty one) residential units contained in 1 (one) 15 unit apartment building and 1 (one) six unit apartment building.

Mobile Homes Construction is expected to displace 4 (four) mobile homes. Based on field inspection, these mobile home residences appear to be typical for the area in terms of size and style. It is unknown if the occupants are owners or tenants.

Businesses Construction is expected to displace 24 (twenty four) businesses consisting of a motel, two convenience markets, a truck driving school, metal fabricator, a motel, an engine rebuilder, two RV sales/rental centers, a fortune teller, a lawn and garden equipment sales center, a painting contractor, a used tire sales office, a music company, a mass mail marketing center, a printing company, and several other small office operations. Included in the total are three vacant commercial/office buildings which appear suitable for occupancy.

Other No farms or non-profit operations are expected to be displaced.

Availability of Replacement Housing

A survey of the Blount and Knox County residential real estate market using information obtained from the Knoxville Area Association of Realtors (www.kaarmls.com) and the Knoxville Apartment Association indicates that an adequate supply of housing exists to meet the 46 anticipated residential relocations.

The Blount and Knox County commercial real estate market in the immediate project area was also surveyed to determine the availability of commercial real estate for either sale or lease. Based on the survey, the supply of available commercial property in the

immediate project area appears to be adequate to satisfy the relocation requirements of the 24 affected businesses. Vacant sites suitable for commercial development are also available in the project area.

ENVIRONMENTAL: Although the proposed improvement will potentially displace 46 families and 24 businesses, the immediate area should experience only minor impact. When completed, no neighborhoods will be disrupted nor will access from areas east or west of the roadway be significantly affected.

During inspection, five locations with possible underground storage tanks were noted. The locations are indicated on the attached “PLANS MARKED FOR CSR” as follows:

Segment	Plan Sheets	Tracts
100241.01	6, 8, & 12	19, 57, & 103
100241.04	5 & 8	8 & 42

ASSURANCES: The Tennessee Department of Transportation will make relocation assistance available to all eligible persons impacted by this project, including residences, businesses, farm operations, non-profit organizations, and those requiring special services or assistance. The Regional Relocation Staff will administer the relocation program under the rules, policies, and procedures set forth in the Federal Uniform Relocation Assistance and Real Property Acquisition Policies Act of 1970 as amended, the Uniform Relocation Assistance Act of 1972, implementing federal regulations, TCA 13-11-101 through 119, The State of Tennessee Relocation Assistance Brochure and Chapter IX of the State of Tennessee Department of Transportation Right-of-Way Manual. TDOT’s relocation program is practical and will allow for the efficient relocation of all eligible displaced persons in accordance with State and Federal Guidelines.

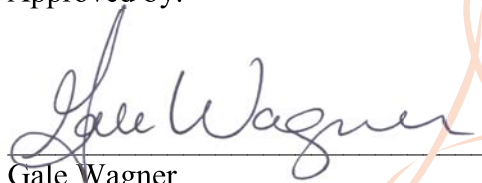
Prepared By:



David S. Goodman
Transportation Specialist 1

Digitally signed by David S. Goodman
DN: cn=David S. Goodman,
o=Tennessee Dept. of Transportation,
ou=Right of Way Office,
email=David.S.Goodman@tn.gov,
c=US
Date: 2012.05.24 15:10:44 -05'00'

Approved by:

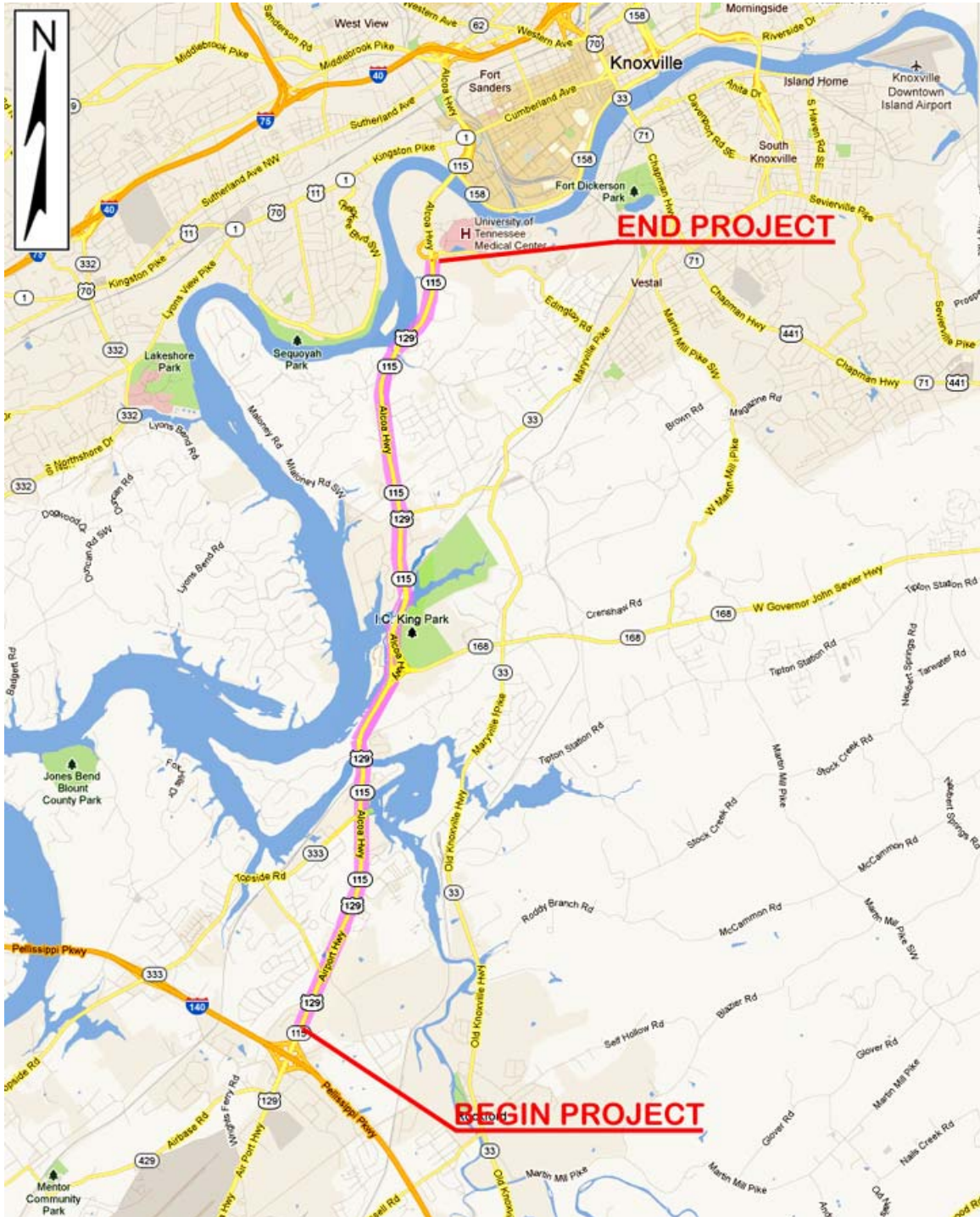


Gale Wagner
Transportation Manager 1

Digitally signed by Gale Wagner
DN: cn=Gale Wagner, o=TDOT,
ou=ROW Division,
email=gale.wagner@tn.gov, c=US
Date: 2012.05.25 08:17:05 -05'00'

Project Location Map

(For Illustration Only)



State of Tennessee
Department of Transportation
Right-of-way Estimate

State Project No.:	05005-1231-14 - PIN 100241.01 2268-04 - PIN 100241.04 100241.02 & 100241.03	47026- N/A - PIN	County(s)	BLOUNT & KNOX
Federal Project No.:	STP-NHE-115(26) - 100241.01 STP-NHE-115(27) - 100241.02 STP-NHE-115(29) - 100241.03 N/A - 100241.04		Description: (Includes all projects) Improvement of SR 115 (US 129 Alcoa Highway) SR-115 (Alcoa Highway) from N. of SR-140 (Pellissippi Pkwy) to N. of Cherokee Trail interchange	
State P.I.N. #	PIN(S): 100241.01, 100241.02, 100241.03 & 100241.04 Note all 4 encompasses PIN: 100241.00			
Estimated Right-Of-Way Cost				
Cost Items	PIN 100241.01	PIN 100241.02	PIN 100241.03	PIN 100241.04
Land Required	\$11,148,773	\$2,688,980	\$6,493,830	\$2,771,902
Acres	51.622	17.833	47.298	10.943
Improvements	\$11,236,169	\$259,232	\$2,300,255	\$1,321,748
Number	38	1	5	2
Damages	\$3,161,685	\$266,145	\$1,485,402	\$1,159,028
Number of Tracts	131	31	15	17
Incidentals	\$373,442	\$75,665	\$52,884	\$59,800
Residential Relocation	\$741,900	\$0	\$153,000	\$33,000
Number	21	0	5	1
Bus. & Farm Relocation	\$933,600	\$30,000	\$0	\$36,000
Number	17	1	0	1
Total Estimate Cost	\$27,595,570	\$3,320,023	\$10,485,371	\$5,381,478
ROUNDED COST	\$27,600,000	\$3,350,000	\$10,500,000	\$5,400,000
			Total:	\$46,850,000

NOTES:

The figures applied in forming this estimate should not be relied upon in the same degree as valuation conclusions which would likely result following a detailed appraisal inspection of the properties affected by this road improvement project.

PIN 100241.01: Note that areas were not provided for this section so this office estimated Fee & all easement areas which delayed the completion of the estimate. It is important to note, that the plans for this section were not complete and many tracts indicated were not used or had 0 acquisition. In addition, this office added tracts that were indicated as having fee with no tract number provided. Since the plans did not show construction easements, this office added CE areas behind slope areas for necessary working room. The plans did not show PDE or CA fencing which this office did not estimate; however, plans should be re-evaluated by the designer so that these items are not overlooked.

PIN 100241.02: The proposed layout for this project indicates a proposed driveway tie-in for "Tract 1". This site has access from "Shipwatch Lane" and is developed for exclusive use by owners within this subdivision. The proposed driveway should be deleted, as placement of the driveway will destroy the present use of the site, resulting in significant damages.


Underground tanks exist within this general "project area". (see form 100)

Amy M. Cooper & Roy O. McKay

Prepared By

March 30, 2012

Date


Fay Danker

Approved By

3/30/12

Date



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

Form LS-5

3/22/2012

UTILITY REPORT FOR LOCATION STUDY

PRELIM EST # R1-115-2155 ALTERNATE *

STATE ROUTE 115 PROJECT TYPE GRADE & DRAIN

COUNTY/S BLOUNT - KNOX DESCRIPTION FROM 76.2 METERS SOUTH OF AIRBASE ROAD TO THE BUCK KARNES BRIDGE.

UTILITY NAME	DEPARTMENT	ESTIMATE TOTAL	REIMBURSABLE
AT&T	PHONE	\$1,972,500.00	\$197,250.00
ATMOS ENERGY CORP. + K.U.B.	GAS	\$346,500.00	\$34,650.00
CHARTER COMMUNICATIONS	CATV	\$117,000.00	\$11,700.00
CITY OF ALCOA(ALCOA WATER SYSTEM)	ELECTRIC + K.U.B.	\$875,000.00	\$87,500.00
CITY OF ALCOA(ALCOA WATER SYSTEM)	SEWER + K.U.B.	\$142,500.00	\$14,250.00
CITY OF ALCOA(ALCOA WATER SYSTEM)	WATER + K.U.B.	\$1,753,500.00	\$175,350.00
EAST TENNESSEE NATURAL GAS, LLC	GAS	\$120,000.00	\$12,000.00
GRAND TOTALS:		\$5,327,000.00	\$532,700.00

RAILROAD YES NO

TOM FOLEY

for Utilities Section

3-22-12

Date

Index Of Sheets

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2-2D	TYPICAL SECTIONS
3-3B	PROPERTY MAPS
3C-3F	R.O.W. ACQUISITION TABLES
4-18	PRESENT LAYOUTS
4A-18A	PROPOSED R.O.W. LAYOUTS
4B-18B	PROPOSED LAYOUTS
4C-18C	PROFILES
19-19I	RAMP PROFILES
19J-19T	SIDE ROAD & PRIVATE DRIVE PROFILES

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

BLOUNT COUNTY

S.R. 115 (U.S. 129, ALCOA HIGHWAY)
FROM: S.R. 129 (PELLISSIPPI PARKWAY)
TO: BRIDGE OVER LITTLE RIVER
PRELIMINARY

TENN.	YEAR	SHEET NO.
	2004	1
FED. AID PROJ. NO.	STP-NHE-115(26)	
STATE PROJ. NO.	05005-1231-14	

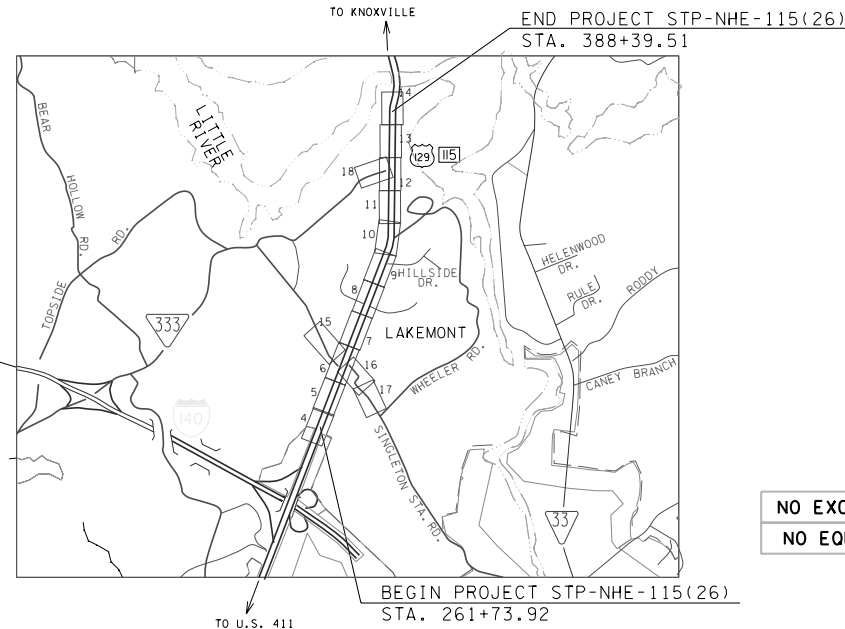


STATE HIGHWAY NO. 115 F.A.H.S. NO. 115

PLANS MARKED FOR CSRP

PROJECT
BLOUNT COUNTY

INDEX TO 100241.00
PIN 100241.01 begins on Page 1
PIN 100241.02 begins on Page 26
PIN 100241.04 begins on Page 44
PIN 100241.03 begins on Page 68



CAUTION !
PRELIMINARY PLANS
SUBJECT TO
CHANGE

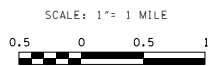
NO EXCLUSIONS
NO EQUATIONS

APPROVED: *[Signature]*
DIRECTOR, DESIGN DIVISION
DATE: _____
APPROVED: *[Signature]*
COMMISSIONER

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED MARCH 1, 1995 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT



C.E. MGR. 1 CLIFF STEWART
DESIGNED BY LOCHNER, INC.
DESIGNER JERRY LESLIE
P.E. NO. 05005-1231-14

PIN 100241.01

ROADWAY LENGTH : 2.399
BRIDGE LENGTH
BOX BRIDGE LENGTH
PROJECT LENGTH : 2.399

TRAFFIC DATA

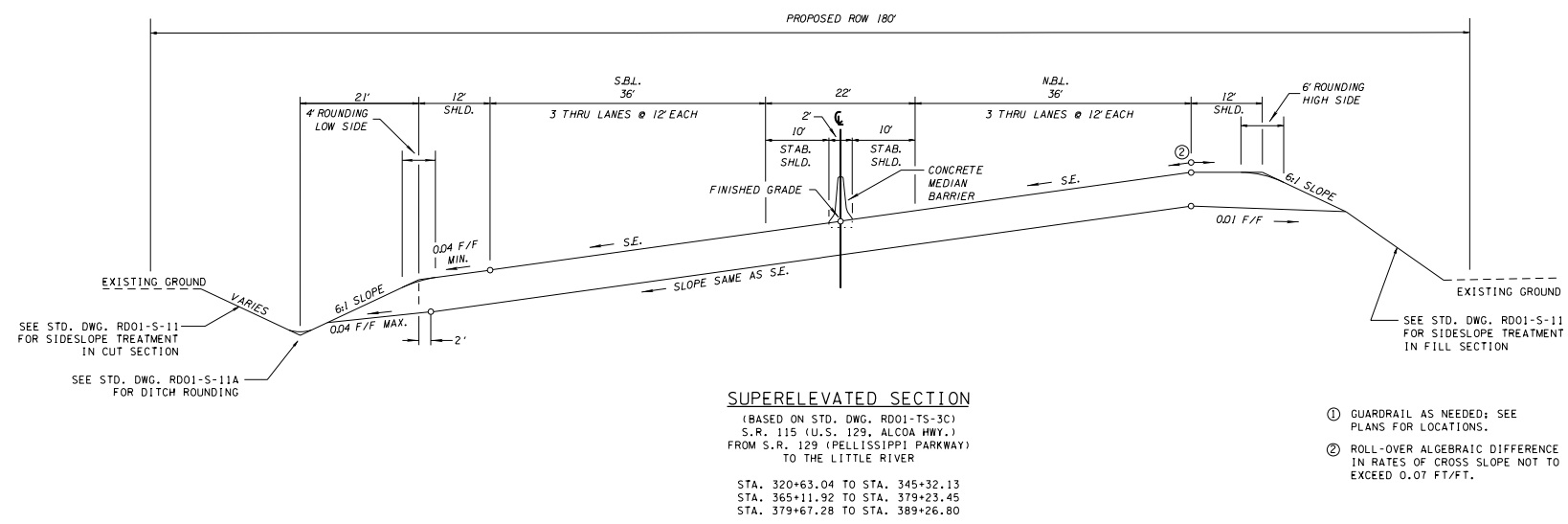
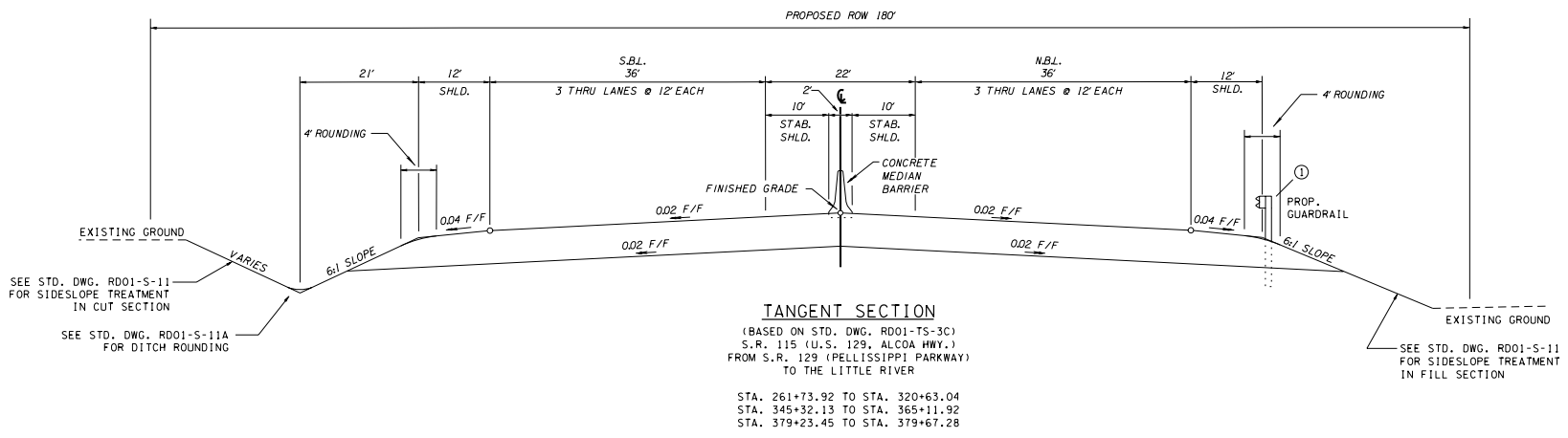
ADT (2007)	58,550
ADT (2027)	88,180
DHV (2027)	8,818
D	65 - 35
T (ADT)	6 %
T (DHV)	4 %
V	60 MPH

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION
APPROVED: _____
DIVISION ADMINISTRATOR DATE

t:\B2\pdm\don_saf\Loyour-2ndSheets\BTSR115\115-TitleSheet.dgn 10/13/2008



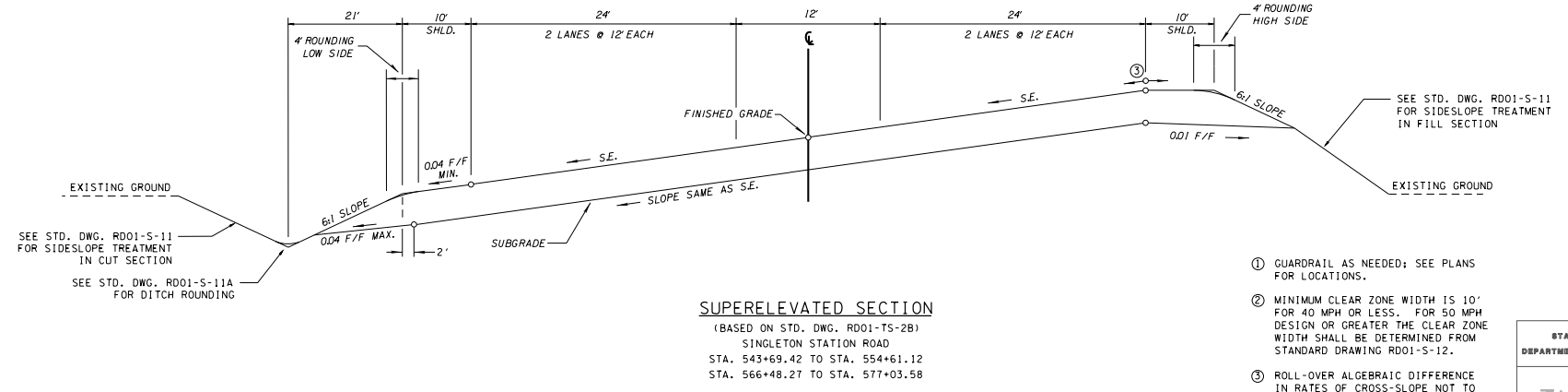
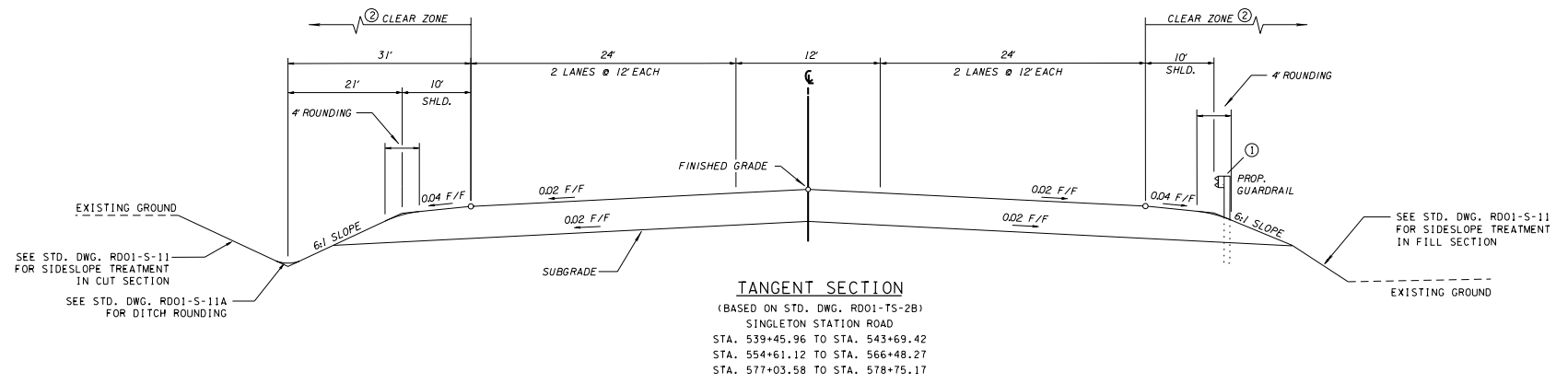
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PRELIM	2004	STP-NHE-115(26)	2
-	-	-	-
-	-	-	-
-	-	-	-



- ① GUARDRAIL AS NEEDED; SEE PLANS FOR LOCATIONS.
- ② ROLL-OVER ALGEBRAIC DIFFERENCE IN RATES OF CROSS SLOPE NOT TO EXCEED 0.07 FT/FT.

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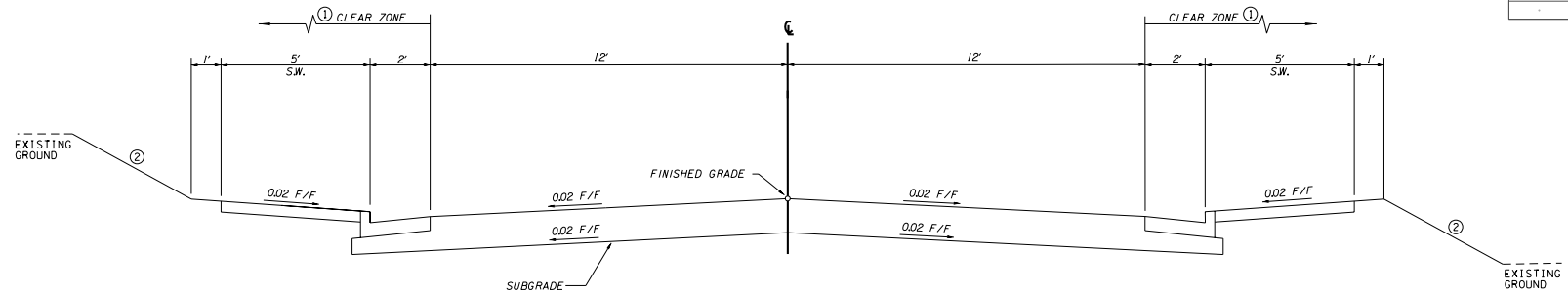
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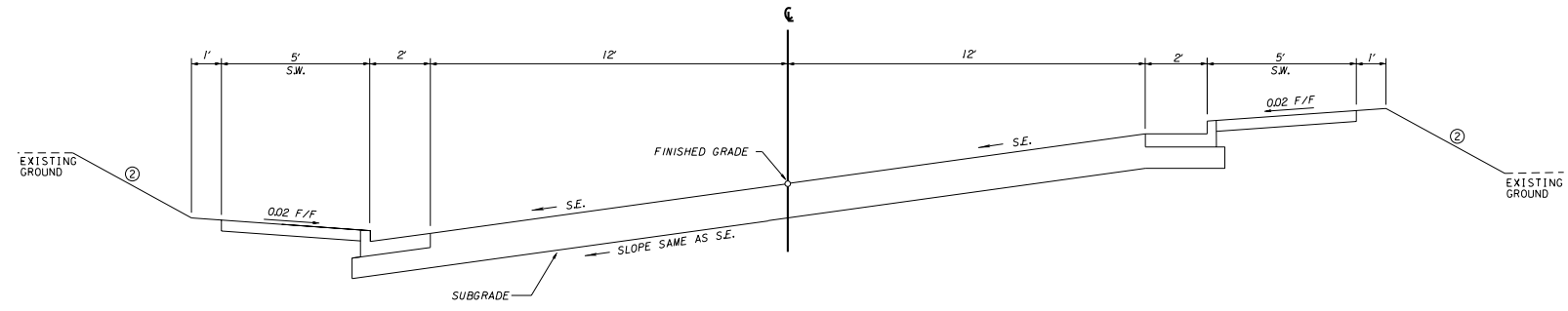
- ① GUARDRAIL AS NEEDED; SEE PLANS FOR LOCATIONS.
- ② MINIMUM CLEAR ZONE WIDTH IS 10' FOR 40 MPH OR LESS. FOR 50 MPH DESIGN OR GREATER THE CLEAR ZONE WIDTH SHALL BE DETERMINED FROM STANDARD DRAWING RD01-S-12.
- ③ ROLL-OVER ALGEBRAIC DIFFERENCE IN RATES OF CROSS-SLOPE NOT TO EXCEED 0.07 FT/FT

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-	-	-	-
-	-	-	-



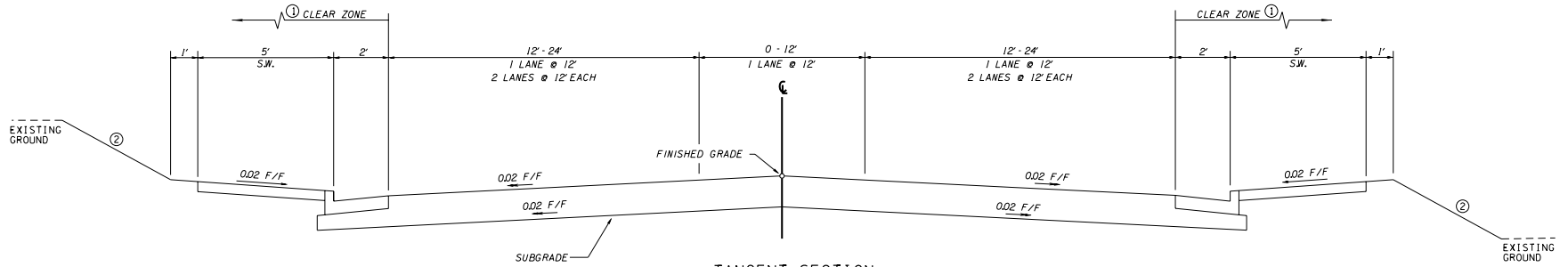
TANGENT SECTION
(BASED ON STD. DWG. RD01-TS-6A)
AIRWAY DRIVE
ALCOA WAY
E. CUMBERLAND DRIVE
HILLSIDE DRIVE
HILLSIDE FRONTAGE ROAD
LAKEMONT DRIVE
N. DOGWOOD LANE
STARLIGHT ROAD
VISTA ROAD
W. CUMBERLAND DRIVE
WHEELER ROAD
WILLOW ROAD



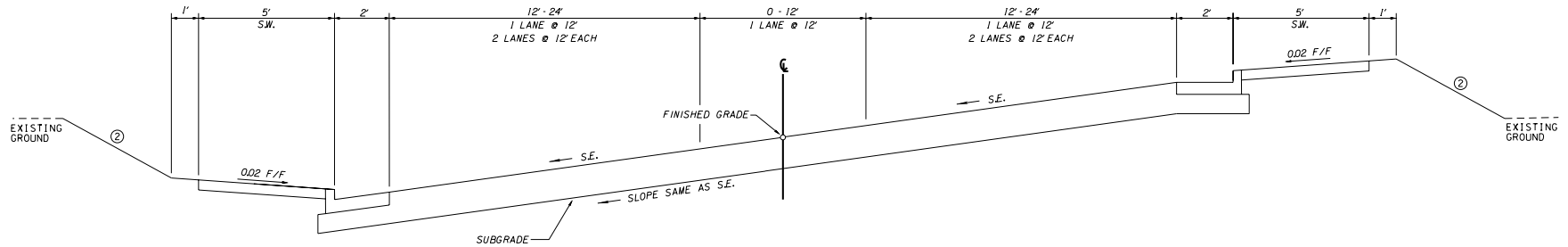
SUPERELEVATED SECTION
(BASED ON STD. DWG. RD01-TS-6A)
AIRWAY DRIVE
ALCOA WAY
E. CUMBERLAND DRIVE
HILLSIDE DRIVE
HILLSIDE FRONTAGE ROAD
LAKEMONT DRIVE
N. DOGWOOD LANE
STARLIGHT ROAD
VISTA ROAD
W. CUMBERLAND DRIVE
WHEELER ROAD
WILLOW ROAD

- ① MINIMUM CLEAR ZONE WIDTH IS 10' FOR 40 MPH OR LESS. FOR 50 MPH DESIGN OR GREATER THE CLEAR ZONE WIDTH SHALL BE DETERMINED FROM STANDARD DRAWING RD01-S-12.
- ② 3:1 OR FLATTER DESIRABLE

TYPE	YEAR	PROJECT NO.	SHEET NO.
PRELIM	2004	STP-NHE-115(26)	2C
-	-	-	-
-	-	-	-



TANGENT SECTION
 (BASED ON STD. DWG. RD01-TS-6A)
 TOPSIDE DRIVE
 STA. 909+65.27 TO STA. 914+39.42
 STA. 919+21.27 TO STA. 921+01.95
 STA. 922+25.66 TO STA. 923+81.35
 STA. 924+75.84 TO STA. 926+51.79



SUPERELEVATED SECTION
 (BASED ON STD. DWG. RD01-TS-6A)
 TOPSIDE DRIVE
 STA. 914+39.42 TO STA. 919+21.27
 STA. 921+01.95 TO STA. 922+25.66
 STA. 923+81.35 TO STA. 924+75.84

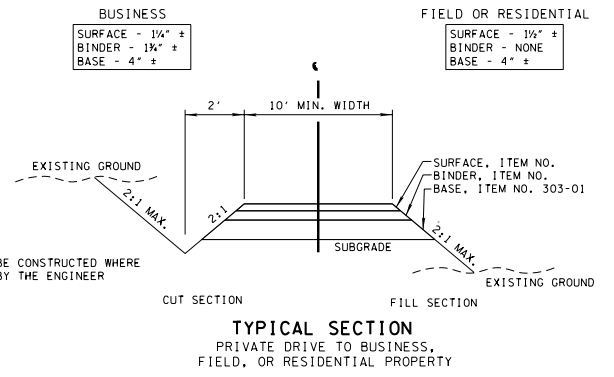
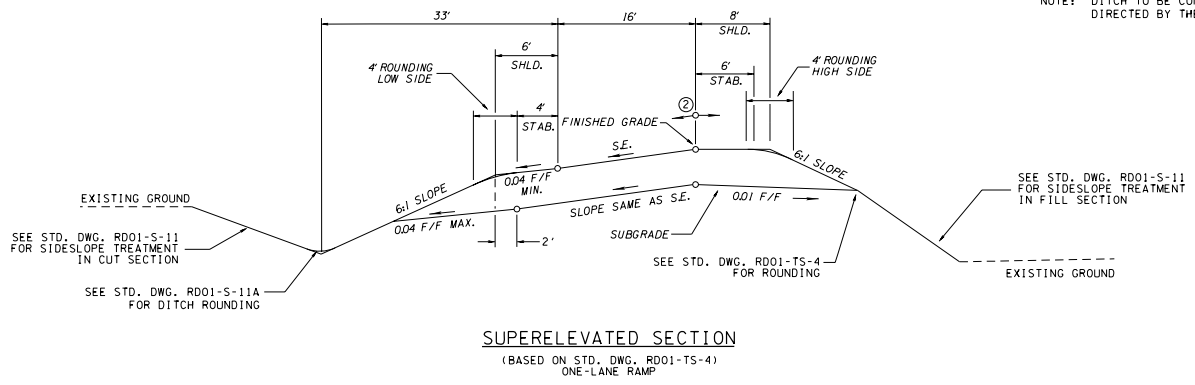
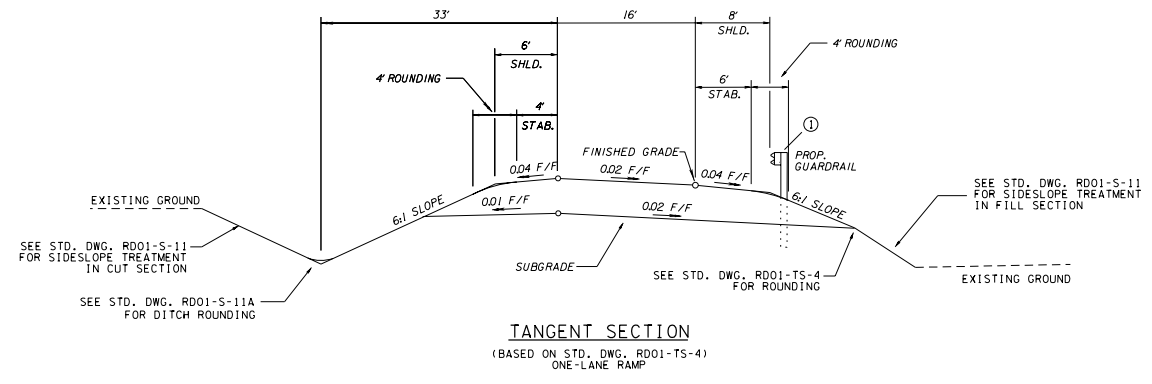
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② 3:1 OR FLATTER DESIRABLE

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

TYPICAL
 SECTIONS

TYPE	YEAR	PROJECT NO.	SHEET NO.
PRELIM	2004	STP-NHE-115(26)	2D
-	-	-	-
-	-	-	-
-	-	-	-



NOTE: DITCH TO BE CONSTRUCTED WHERE DIRECTED BY THE ENGINEER

- ① GUARDRAIL AS NEEDED; SEE PLANS FOR LOCATIONS.
- ② ROLL-OVER ALGEBRAIC DIFFERENCE IN RATES OF CROSS-SLOPE NOT TO EXCEED 0.07 FT/FT

TYPE	YEAR	PROJECT NO.	SHEET NO.
PRELIM.	2004	STP-NHE-115(26)	3C
-	-	-	-
-	-	-	-
-	-	-	-

R.O.W. ACQUISITION TABLE

TRACT NO.	PROPERTY OWNERS	COUNTY RECORDS				TOTAL AREA (ACRES)			AREA TO BE ACQUIRED (ACRES)			AREA REMAINING (ACRES)		EASEMENT (SQUARE FEET)		
		TAX MAP NO.	PARCEL NO.	DEED DOCUMENT REFERENCE		LEFT	RIGHT	TOTAL	LEFT	RIGHT	TOTAL	LEFT	RIGHT	PERM DRAINAGE	SLOPE	CONST.
				BOOK	PAGE											
1	PATTERSON, W L & FRANCES RICH	18	8	167	124	2.794		2.794								
2	PARKWAY DEVELOPMENT INC	18	7	492	752	2.675		2.675								
				473	278*			0 SF								
3	NANCE, WAYNE P & GRACE	18	6	260	334	2.232		2.232								
4	FISHER, THOMAS D & NORMA H	18	5	577	286	2.82		2.820								
				443	471			0 SF								
5	SHIELDS, WILLIAM R & JACKIE L	18	4	249	147	4.723		4.723								
6	RAMSEY, OTHNIEL L & IVA P	18	3	247	129	4.114		4.114								
7	MLR INC	18	2	551	108	4.543		4.543								
				176	35			0 SF								
9	JACOBS, DONALD C	18	1	497	283	164 SF	0.742									
				482	278			0 SF								
				372	239			0 SF								
10	OGLE, JOHN S	18	1.01	485	795	0.549		0.549								
				392	353			0 SF								
11	WILLIAMSON, BILL R	18	15	463	5			0 SF								
				456	350			0 SF								
12	CHUNG, YUNG CHI & LIANG HUNG	18	14	49	441			0 SF								
13	JIM WALTER HOMES INC	18	13	541	442	2.087		2.087								
				378	274			0 SF								
14	HENRY, CLIFFORD H JR. & JERRY L HODGE	18	9	480	211	9.721		9.721								
15	PINE LAKES GOLF & COUNTRY CLUB INC	18	82	334	240			0 SF								
15A	SCHOOL - UNIV OF TENN	18	25	134	32	50		50,000								
16		18	25.01	NO DEED				0 SF								
17	BLOUNT COUNTY	18	25.02	476	459	2.999		2.999								
18	WATSON, MARION WHALEY	9	115	163	563	4.046		4.046								
19	HOWARD, X H JR. & HELEN R	9	114	442	721	0.479		0.479								
20	CITY OF ALCOA	9	116	327	447	0.037		1612 SF								
21	SMITH, JOHN F & ORVIE	9-P/C	9	434	610	0.295		0.295								
22	SMITH, JOHN F & ORVIE	9-P/C	1	525	12	0.145		0.145								
23	SMITH, JOHN F & ORVIE	9-P/C	2	499	270	0.172		0.172								
	**			483	607			0 SF								
	*			471	733			0 SF								
24	ALLEN, TERRY MICHAEL	9-P/C	7	545	388	0.179		0.179								
				399	733			0 SF								
25	CO J S INC.	9-P/C	6	566	563	0.25		0.250								
				413	629			0 SF								
26	JOHNSON, ALICE F	9-P/C	3	554	340	0.258		0.258								
				417	734			0 SF								
27	WESTFALL, BEVERLY L. KASEMEYER	9-P/C	4	421	749	0.255		0.255								
28	SMITH, JOHN F & ORVIE	9-P/C	5	526	268	0.544		0.544								
				522	263			0 SF								
				442	240			0 SF								
29	BEAMM, ENTERPRISES	9-P/C	18	580	591	5.277		5.277								
				472	724			0 SF								
30	WAGGONER, FREDERICK LOWERY S & TROY BELLE L	9-P/C	19	506	600	1.975		1.975								
				460	529			0 SF								
31	CHILHOWEE TRAILER SALES	9-P/C	20	606	14	2.827		2.827								
				370	720			0 SF								
32	AHCI LP % HOBART MORGAN	9-P/B	1	543	27	1.364		1.364								

TYPE	YEAR	PROJECT NO.	SHEET NO.
PRELIM.	2004	STP-NHE-115(26)	3D
-	-	-	-
-	-	-	-
-	-	-	-

R.O.W. ACQUISITION TABLE

TRACT NO.	PROPERTY OWNERS	COUNTY RECORDS				TOTAL AREA (ACRES)			AREA TO BE ACQUIRED (ACRES)			AREA REMAINING (ACRES)		EASEMENT (SQUARE FEET)		
		TAX MAP NO.	PARCEL NO.	DEED DOCUMENT REFERENCE		LEFT	RIGHT	TOTAL	LEFT	RIGHT	TOTAL	LEFT	RIGHT	PERM DRAINAGE	SLOPE	CONST.
				BK.	PAGE											
32	AHCI LP % HOBART MORGAN	9-P/B	1	489	3			0 SF								
				479	567			0 SF								
33	AHCI LP % HOBART MORGAN	9-P/B	6	543	27	2.642		2.642								
				489	3			0 SF								
				479	567			0 SF								
34	AHCI LP % HOBART MORGAN	9-P/B	7	543	27	2.642		2.642								
				489	3			0 SF								
				479	567			0 SF								
35	PARK, JUNA S	9-P/B	8	578	88		0.292	0.292								
				524	377			0 SF								
				512	220			0 SF								
36	J & S MANAGEMENT, INC.	9-P/B	9	618	464		4.757	4.757								
				164	752			0 SF								
37	J & S MANAGEMENT, INC.	9-P/B	10	618	464		0.354	0.354								
				440	275			0 SF								
38	SATTERFIELD, DENNIS R & JENENE	9-P/C	22	570	24	0.422		0.422								
				168	253			0 SF								
39	LAW, ALLEN G, ET AL	9-P/C	27	451	473	1.221		1.221								
40	LAW, ALLEN G, ET AL	9-P/C	25	202	485	0.502		0.502								
41	LAW, ALLEN G, ET AL	9-P/C	23	451	473	0 SF		0 SF								
42	THOMAS, TINA M	9-P/C	21	517	243	0.593		0.593								
				497	71			0 SF								
				372	108			0 SF								
43	SHIPLEY, HARRY A & KATIE JEAN	9-P/B	11	284	387		0.649	0.649								
44	SHIPLEY, HARRY A & KATIE JEAN	9-P/A	1	145	376		0.705	0.705								
45	LAW, ALLEN G, ET AL	9-P/A	12	451	473		0.655	0.655								
46	HENRY, NANCIE LAW	9-P/A	11	530	593		0.386	0.386								
				298	57			0 SF								
47	BIVENS, DELBERT & DORIS MAE	9-P/A	10	225	210			0.364								
48	SLOAN, CHARLES & HUGH	9	79	450	118	11.117		11.117								
49	SLOAN, CHARLES & HUGH	9	78	450	118	11.117		11.117								
50	WILLIAMS, HARRY	9	77.01	434	590	0.327		0.327								
51	SLOAN, CHARLES & HUGH	9	77.02	450	118	11.117		11.117								
52	LUALLEN, ADELBERT LEROY	9	76	170	465	0.409		0.409								
53	EATEN, ANN MARIE MAGEE & VERA JANE TECLICK	9	75	NO DEED	BY WILL	22.326		22.326								
54	EPPS, WANDA RUTH & MORRIS F	9	12	524	697	27.242		27.242								
				380	140			0 SF								
55	MCGINLEY, CHARLES JULIAN & ELIZABETH ROSE	9-P/A	13	147	400		1.389	1.389								
56	CLAYTON HOMES INC.	9-1/A	14	489	353		2.276	2.276								
				479	564			0 SF								
57	DOONEY, J W & G K SMITHER	9-1/A	15	442	80		1.66	1.660								
58	CLAYTON HOMES INC.	9-1/A	15.01	494	108		1.046	1.046								
59	HOWARD, K H JR & HELEN R	9-1/A	7.01	490	371		0.284	0.284								
				463	751			0 SF								
60	HOWARD, K H JR & HELEN R	9-1/A	7	490	323		0.286	0.286								
				475	859			0 SF								
61	HOWARD, K H JR & HELEN R	9-1/A	8	503	333		0.284	0.284								
				391	334			0 SF								
62	VANCE, MINARD A	9-1/A	9	59	894		0.483	0.483								
63	HOWARD, K H JR & HELEN R	9-1/A	10.01	502	624		0.275	0.275								

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TYPE	YEAR	PROJECT NO.	SHEET NO.
PRELIM.	2004	STP-NHE-115(26)	3E
-	-	-	-
-	-	-	-
-	-	-	-

R.O.W. ACQUISITION TABLE

TRACT NO.	PROPERTY OWNERS	COUNTY RECORDS				TOTAL AREA (ACRES)			AREA TO BE ACQUIRED (ACRES)			AREA REMAINING (ACRES)		EASEMENT (SQUARE FEET)		
		TAX MAP NO.	PARCEL NO.	DEED DOCUMENT REFERENCE		LEFT	RIGHT	TOTAL	LEFT	RIGHT	TOTAL	LEFT	RIGHT	PERM DRAINAGE	SLOPE	CONST.
				BK.	PAGE											
63	HOWARD, K H JR & HELEN R	9-1/A	10.01	486	611			0 SF								
				413	568			0 SF								
64	HOWARD, K H JR & HELEN R	9-1/A	10	569	756		0.93	0.930								
65	HOWARD, K H JR & HELEN R	9-1/A	11.02				1.113	1.113								
66	HOWARD, K H JR & HELEN R	9-1/A	11	508	35		1.543	1.543								
				4909	622			0 SF								
				426	455			0 SF								
67	MONDAY, JAMES STEPHENSON & RAY MORGAN	9-1/A	12				2.153	2.153								
68	MCGINLEY, CHARLES JULIAN & ELIZABETH ROSE	9-1/A	13	147	400		2.666	2.666								
69	GOIN, BOBBY G & SHARON A	9-1/A	15	527	284		0.793	0.793								
				485	913			0 SF								
				420	831			0 SF								
70	RENO, J ELMER & GENEVA	9-1/A	14	485	915		1.114	1.114								
				132	762			0 SF								
71	GIBSON, MYRTLE SUE ROSE	9	74	388	329		0.504	0.504								
72	GOIN, BOBBY G & SHARON A	9	73	535	488		0.862	0.862								
				426	560			0 SF								
73	PHILLIPS, WILLIAM R. TRUSTEE	9	72	457	76		0.852	0.852								
74	MOORES POTATO CHIP CO BORDEN INC - TAX DEPT	9	70.02	411	657		1	1.000								
75	UNITED AMERICAN LAND CORP & GREEN ACRES	9	71	MS109	52		13.36	13.360								
				340	486			0 SF								
76	OGLE, DEWAYNE KENNETH	9	14	610	551		0.316	0.316								
				481	810			0 SF								
76A	YUNKER, BARBARA LAMBERT	9	14.01	470	744		0.726	0.726								
77	LAKEMONT MARKET, LLC	9	13	627	293		4.438	4.438								
				566	892			0 SF								
				524	226			0 SF								
				522	741			0 SF								
78	LEXTEX & JERRY BARBER	9	15	517	806		0.795	0.795								
				488	111			0 SF								
				355	82			0 SF								
79	GARRETT, DONALD L	9	16	568	852		1.954	1.954								
				512	563			0 SF								
				509	668			0 SF								
80	TENNYEC INC.	9	69.08	521	31		0.981	0.981								
81	ARNHART, HOWARD E & NIKKI B	9	17	531	85		1.658	1.658								
				351	394			0 SF								
82	FOSHEE, LARRY	9	19	554	687		4.749	4.749								
				476	301			0 SF								
83	MCGINLEY, CHARLES JULIAN & ELIZABETH ROSE	9	18	326	362			0 SF								
84	MILLER, SAMUEL T & PAULINE S	9-H/A	28	318	80		0.414	0.414								
85	ORR, WILLIAM EDWARD	9-H/A	29	536	716		0.429	0.429								
				508	661			0 SF								
				496	36			0 SF								
86	MORGAN, PALPH K & ANN T	9-H/A	30	549	538			0 SF								
				508	793			0 SF								
				289	105			0 SF								
87	BLAIR, JAMES A JR.	9-H/A	31	569	268			0 SF								
				459	36			0 SF								

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TYPE	YEAR	PROJECT NO.	SHEET NO.
PRELIM.	2004	STP-NHE-115(26)	3F
-	-	-	-
-	-	-	-
-	-	-	-

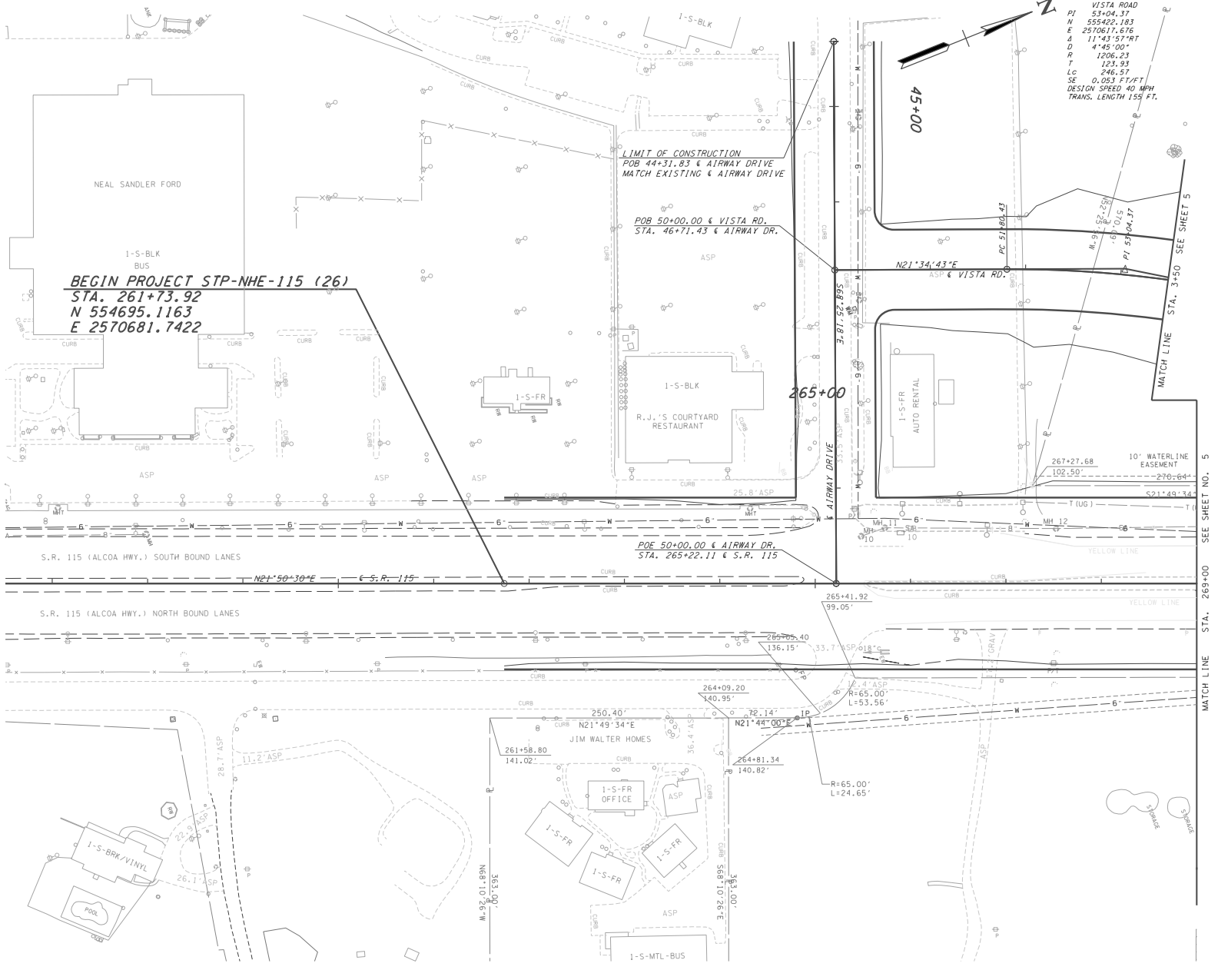
R.O.W. ACQUISITION TABLE

TRACT NO.	PROPERTY OWNERS	COUNTY RECORDS				TOTAL AREA (ACRES)			AREA TO BE ACQUIRED (ACRES)			AREA REMAINING (ACRES)		EASEMENT (SQUARE FEET)		
		TAX MAP NO.	PARCEL NO.	DEED DOCUMENT REFERENCE		LEFT	RIGHT	TOTAL	LEFT	RIGHT	TOTAL	LEFT	RIGHT	PERM DRAINAGE	SLOPE	CONST.
				BK.	PAGE											
88	MORGAN, RALPH K & ANN T	9-H/A	37	549	538			0 SF								
				508	793			0 SF								
				315	338			0 SF								
89	ANDERSON, LAWRENCE LYNN	9-H/A	22	557	595		0.356	0.356								
				452	406			0 SF								
90	MILLER, D E	9-H/A	23	315	224		0.35	0.350								
91	DAVIS, DEWEY BRYAN	9-H/A	24	531	486		0.354	0.354								
				319	505			0 SF								
92	DAVIS, DEWEY BRYAN & DEBORAH LYNN	9-H/A	25	557	383		0.353	0.353								
				331	228			0 SF								
93	HENRY, HUGH J & MILDRED G	9-H/A	26	215	55		0.354	0.354								
94	HENRY, HUGH J & MILDRED G	9-H/A	27	215	55		0.287	0.287								
95	CLAYTON HOMES INC. Attn: E. Rochat #308	9	10	463	334		22.869	22.869								
96	HUDDLESTON, HOWARD FRANKLIN & MARGARET T	9	11.01	214	474	3.74		3.740								
97	GREENE, ROBERT S	9	11	556	760	9.068		9.068								
				214	474			0 SF								
98	RICHARDSON, CHESTER L & VALERIE	9	10.01	388	519			0 SF								
99	NEEDHAM, AUDREY K JR. & MARY F	9	7.01	465	781	0.476		0.476								
100	NEEDHAM, AUDREY K JR. & MARY F	9	7	475	685	0.931		0.931								
				777	648			0 SF								
102	DITCHWITCH EQUIP. OF TN INC.	9	8	447	458	1.115		1.115								
103	NEEDHAM, AUDREY K JR.	9	9	316	168	1.154		1.154								
104	KAMPUS, VICTOR & REBECCA	9	5.01	542	610	1.135		1.135								
				498	288			0 SF								
				497	403			0 SF								
105		9	5			1.646		1.646								
106	DREW INDUSTRIES INC	9	4	237	495	8.793		8.793								
107	HOOKS, ROBERT D. & ESTHER C.	9	2	560	276	1.768		1.768								
				504	863			0 SF								
				469	681			0 SF								
108	QUINN, ROBERT L Z FIDELITY PRINTING CO.	9	1.01	373	166	2.507		2.507								
109	HOWARD, K H JR & HELEN R	9	1	439	139	21.708		21.708								
110	TVA	9	3					0 SF								
111	ALCOA WAY BAPTIST CHURCH	3	1	278	121	2.915		2.915								
112	THE PANTRY INC.	9-P/B	25	408	977	0.471		0.471								
113	BRIGHT FUTURES MARKETING	9-P/B	25.01	568	336	0.488		0.488								
114	TROUTMAN, DAVID	9-P/B	24	527	745	0.796		0.796								
115	MAXWELL, KENNEDY R.	9-P/B	26	440	414	0.901		0.901								

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TYPE	YEAR	PROJECT NO.	SHEET NO.
PRELIM	2004	STP-NHE-115(26)	4
-	-	-	-
-	-	-	-
-	-	-	-

VISTA ROAD
 PI 53+04.37
 N 555422.183
 E 2510617.676
 L 11'43.57'01"
 D 4'45'00"
 T 123.93
 R 246.57
 LC 0.053 FT/FT
 DESIGN SPEED 40 MPH
 TRANS. LENGTH 155 FT.



BEGIN PROJECT STP-NHE-115 (26)
 STA. 261+73.92
 N 554695.1163
 E 2570681.7422

LIMIT OF CONSTRUCTION
 POB 44+31.83 & AIRWAY DRIVE
 MATCH EXISTING & AIRWAY DRIVE

POB 50+00.00 & VISTA RD.
 STA. 46+71.43 & AIRWAY DR.

POE 50+00.00 & AIRWAY DR.
 STA. 265+22.11 & S.R. 115

COORDINATE VALUES ARE NAD/83(1995)
 AND ARE DATUM ADJUSTED BY THE
 FACTOR LOGO1 & TIED TO THE TGN.

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

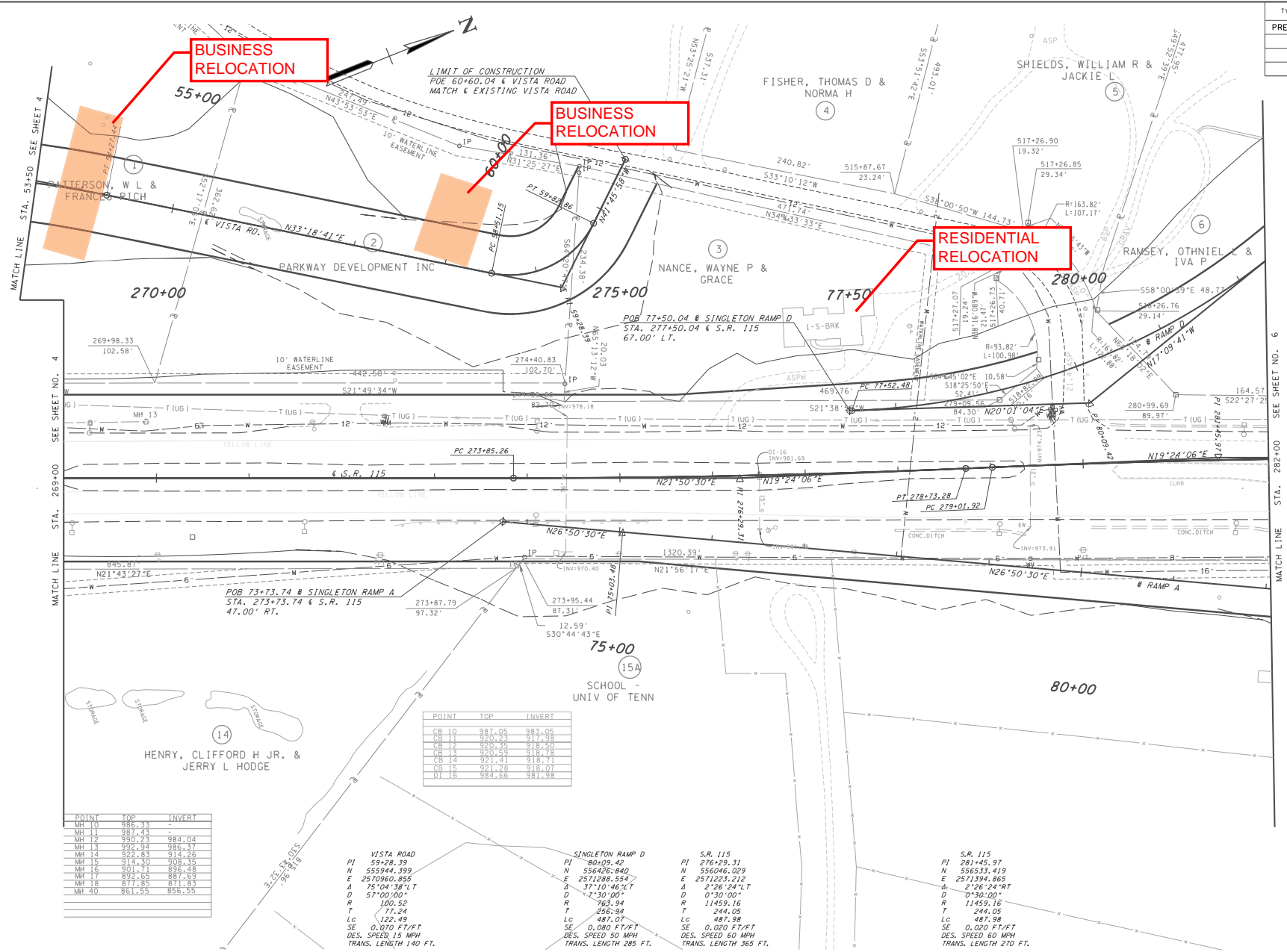
PRESENT LAYOUT

BEGIN TO STA. 269+00

SCALE: 1" = 50'

TYPE	YEAR	PROJECT
PRELIM	2004	STP-NHE-115(26)

TENNESSEE D.O.T.
DESIGN DIVISION
FILE NO.



POINT	TOP	INVERT
MH 10	886.33	-
MH 11	881.45	-
MH 12	890.23	884.04
MH 13	892.34	886.17
MH 14	895.83	814.26
MH 15	814.30	808.35
MH 16	801.71	836.48
MH 17	892.69	887.69
MH 18	874.85	871.83
MH 20	861.55	856.55

POINT	TOP	INVERT
CB 10	887.05	983.05
CB 11	820.25	911.98
CB 12	820.35	918.50
CB 14	820.59	918.78
CB 14	821.41	919.14
CB 15	821.28	918.07
DI 16	884.66	881.98

VISTA ROAD
PI 59+28.39
W 555944.399
E 2570960.855
A 75°04'38"LT
D 57°00'00"
R 100.52
T 77.24
LC 122.49
SE 0.070 FT/FT
DES. SPEED 15 MPH
TRANS. LENGTH 140 FT.

SINGLETON RAMP D
PI 80+09.42
W 556426.840
E 2571288.554
A 37°10'46"LT
D 7°30'00"
R 763.94
T 256.94
LC 487.07
SE 0.080 FT/FT
DES. SPEED 50 MPH
TRANS. LENGTH 285 FT.

S.R. 115
PI 276+29.31
W 556046.029
E 2571223.212
A 2°26'24"LT
D 0°30'00"
R 11459.16
T 244.05
LC 487.98
SE 0.020 FT/FT
DES. SPEED 60 MPH
TRANS. LENGTH 365 FT.

S.R. 115
PI 281+45.97
N 556531.419
E 2571394.865
A 2°26'24"RT
D 0°30'00"
R 11459.16
T 244.05
LC 487.98
SE 0.020 FT/FT
DES. SPEED 60 MPH
TRANS. LENGTH 270 FT.

COORDINATE VALUES ARE NAD/83(995) AND ARE DATUM ADJUSTED BY THE FACTOR 1.0001 & TIED TO THE TGRN.

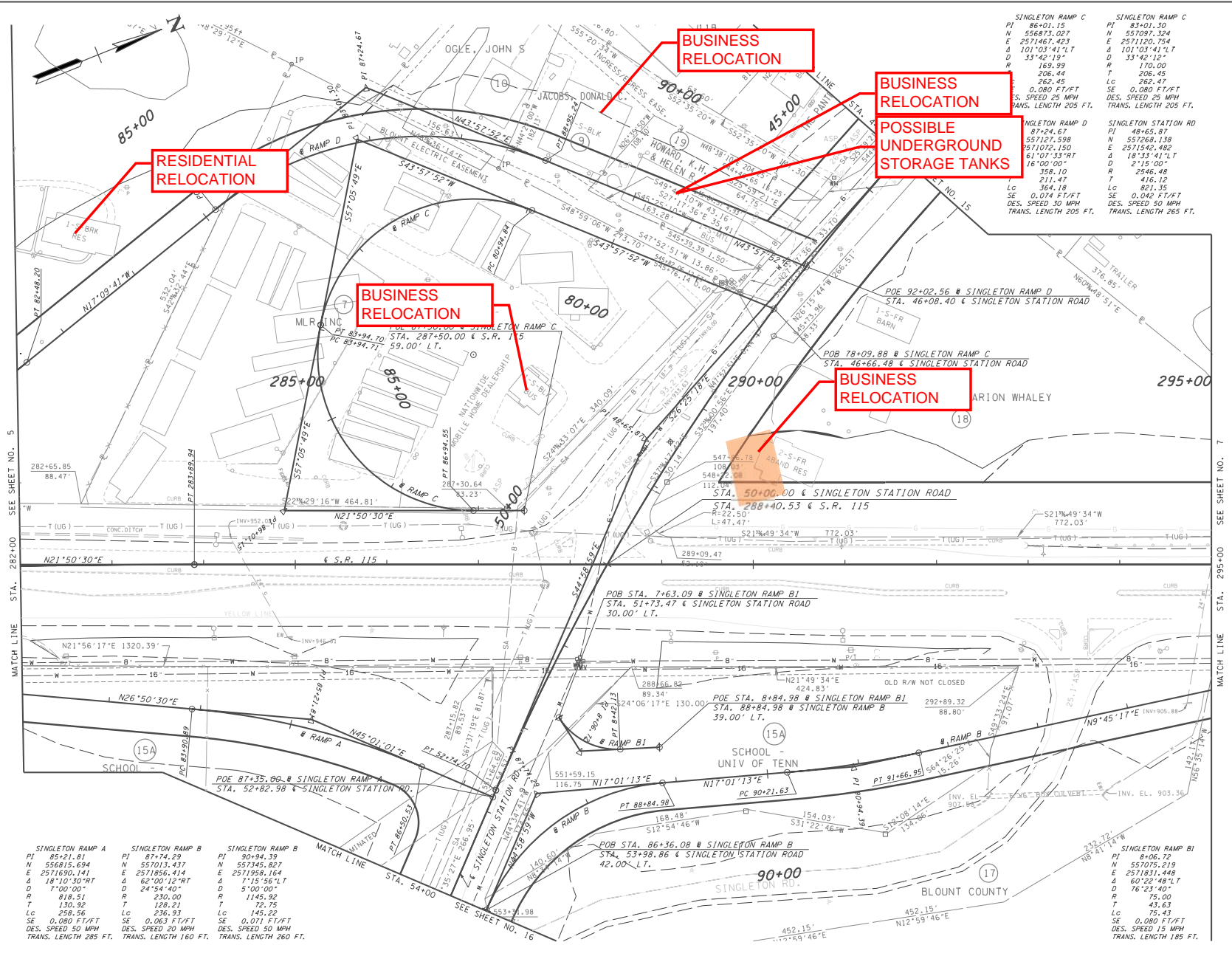
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PRESENT LAYOUT

STA. 269+00 TO STA. 282+00

SCALE: 1" = 50'

TYPE	YEAR	PROJECT NO.	SHEET NO.
PRELIM	2004	STP-NHE-115(26)	6



LINE	PI	PC	PT	STATIONING	DESIGN SPEED	TRANS. LENGTH
SINGLETON RAMP C	86+01.15	87+74.29	89+94.39	85+00.00	25 MPH	205 FT.
SINGLETON RAMP C	83+01.30	85+00.00	87+74.29	83+00.00	25 MPH	205 FT.
SINGLETON RAMP D	87+24.67	89+94.39	92+69.09	87+00.00	25 MPH	205 FT.
SINGLETON STATION RD	87+24.67	89+94.39	92+69.09	87+00.00	30 MPH	265 FT.

LINE	PI	PC	PT	STATIONING	DESIGN SPEED	TRANS. LENGTH
SINGLETON RAMP A	85+21.81	87+74.29	90+94.39	85+00.00	25 MPH	285 FT.
SINGLETON RAMP B	85+21.81	87+74.29	90+94.39	85+00.00	25 MPH	160 FT.
SINGLETON RAMP B	85+21.81	87+74.29	90+94.39	85+00.00	25 MPH	260 FT.

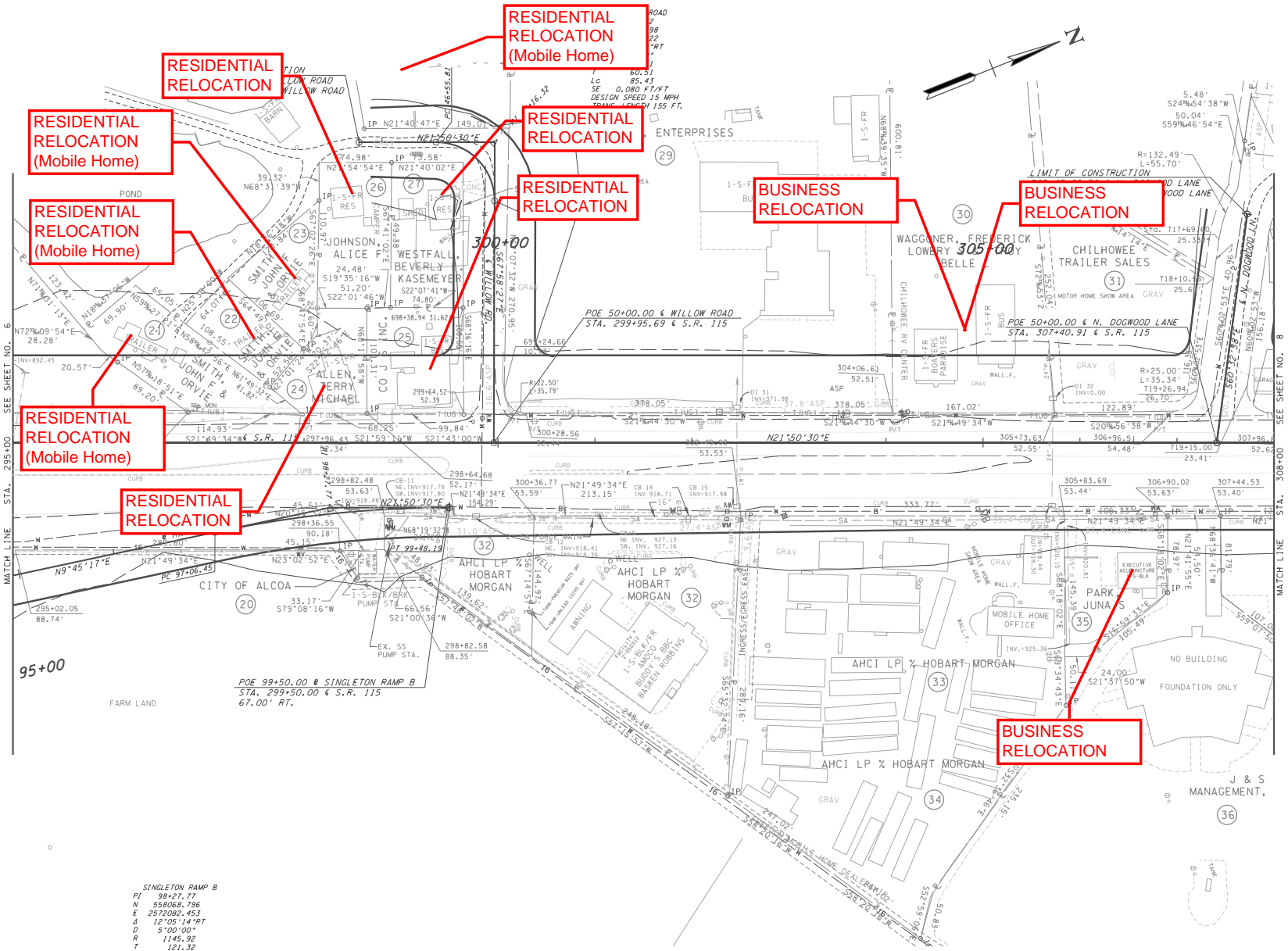
COORDINATE VALUES ARE NAD(83)/9951 AND ARE DATUM ADJUSTED BY THE FACTOR 1.0001 & TIED TO THE TGN.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PRESENT LAYOUT

STA. 282+00 TO STA. 295+00
SCALE: 1" = 50'

TYPE	YEAR	PROJECT NO.	SHEET NO.
PRELIM	2004	STP-NHE-115(26)	7
-	-	-	-
-	-	-	-
-	-	-	-

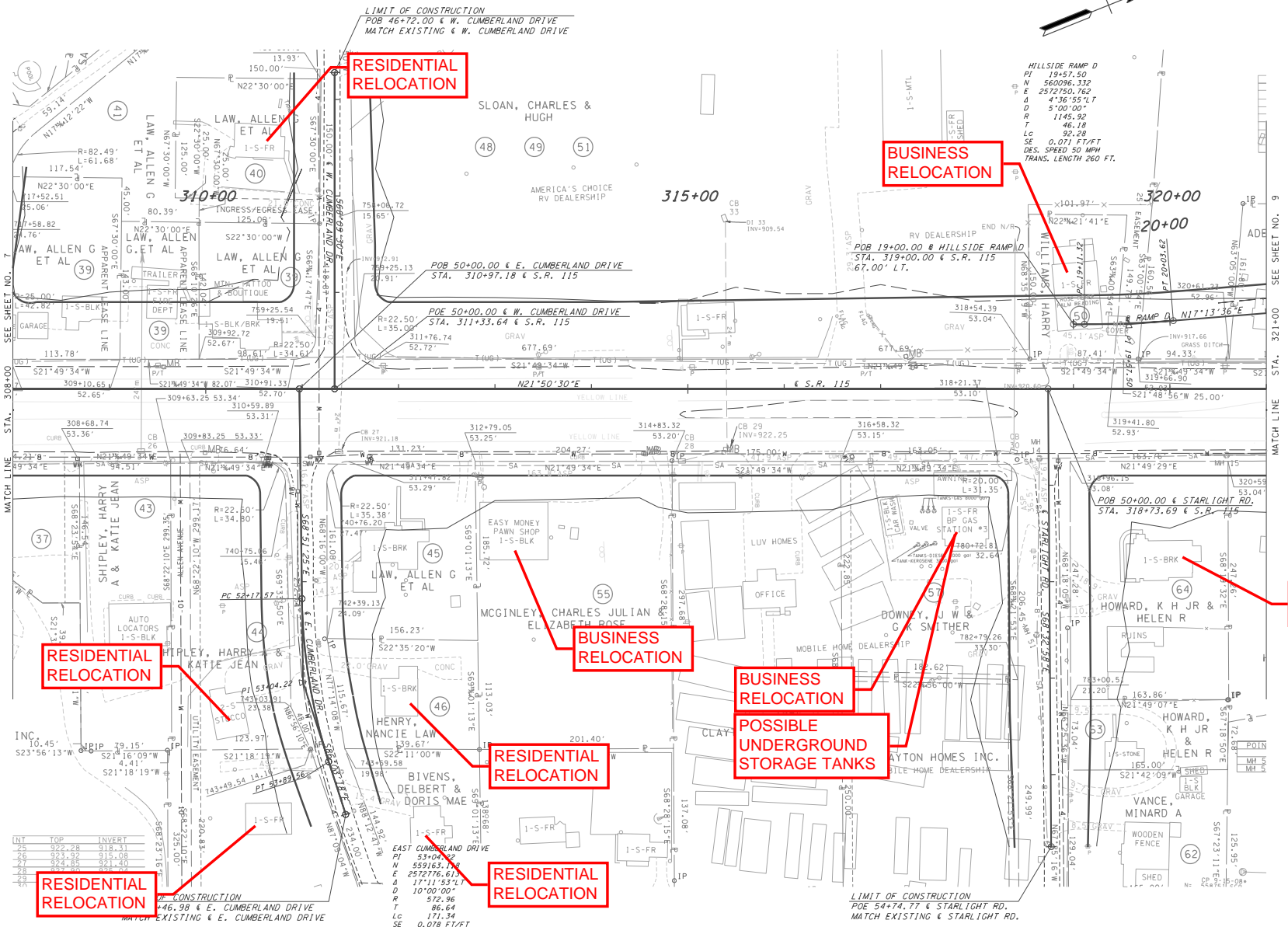
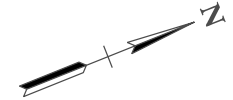


SINGLETON RAMP B
 PI 98+27.77
 N 550060.796
 E 2572082.453
 A 12°05'14"RT
 D 5'00'00"
 R 1145.92
 T 121.32
 LC 241.30
 SE 0.071 FT/FT
 DESIGN SPEED 50 MPH
 TRANS. LENGTH 260 FT.

COORDINATE VALUES ARE NAD/83(995)
 AND ARE DATUM ADJUSTED BY THE
 FACTOR 1.0001 & TIED TO THE TGRN.

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
PRESENT LAYOUT
 STA. 295+00 TO STA. 308+00
 SCALE: 1" = 50'

TYPE	YEAR	PROJECT NO.	SHEET NO.
PRELIM	2004	STP-NHE-115(26)	8



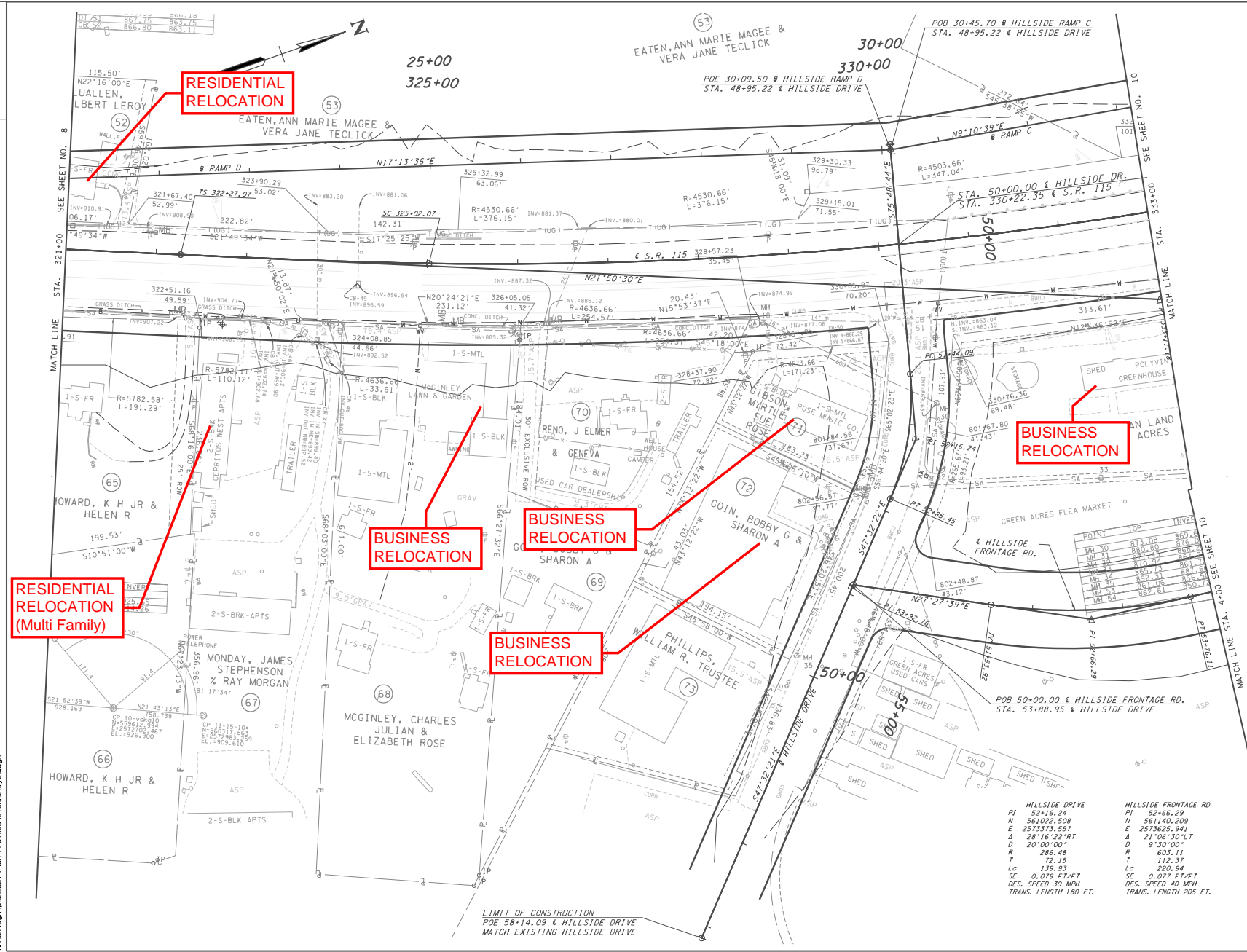
INT	TOP	INVERT
24	822.28	818.11
26	823.92	815.08
27	824.88	812.40
28	825.84	809.72
29	826.80	807.04

COORDINATE VALUES ARE NAD/83(95) AND ARE DATUM ADJUSTED BY THE FACTOR 1.0001 & TIED TO THE TGRN.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PRESENT LAYOUT
STA. 308+00 TO STA. 321+00
SCALE: 1" = 50'

TYPE	YEAR	PROJECT NO.	SHEET NO.
PRELIM	2004	STP-NH-115(26)	9
-	-	-	-
-	-	-	-
-	-	-	-



RESIDENTIAL RELOCATION
 (Multi Family)

RESIDENTIAL RELOCATION

BUSINESS RELOCATION

BUSINESS RELOCATION

BUSINESS RELOCATION

BUSINESS RELOCATION

	HILLSIDE DRIVE	HILLSIDE FRONTAGE RD
PI	52+16.24	52+66.29
N	561022.508	561140.209
E	2573373.557	2573625.941
Δ	20°16'02"RT	Δ 21°06'30"LT
D	20'00'00"	D 9'30'00"
R	286.48	R 603.11
T	72.15	T 112.33
Lc	139.93	Lc 220.94
SE	0.079 FT/FT	SE 0.077 FT/FT
DES. SPEED	30 MPH	DES. SPEED 40 MPH
TRANS. LENGTH	180 FT.	TRANS. LENGTH 205 FT.

POINT	TOP	INVER
MH 30	873.08	869.70
MH 31	860.80	860.70
MH 32	870.92	861.70
MH 33	869.41	861.70
MH 34	861.00	860.70
MH 35	862.00	860.70

LIMIT OF CONSTRUCTION
 POE 58+14.09 & HILLSIDE DRIVE
 MATCH EXISTING HILLSIDE DRIVE

COORDINATE VALUES ARE NAD(83)/9951
 AND ARE DATUM ADJUSTED BY THE
 FACTOR 1.0001 & TIED TO THE TGRN.

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

PRESENT LAYOUT

STA. 321+00 TO STA. 333+00

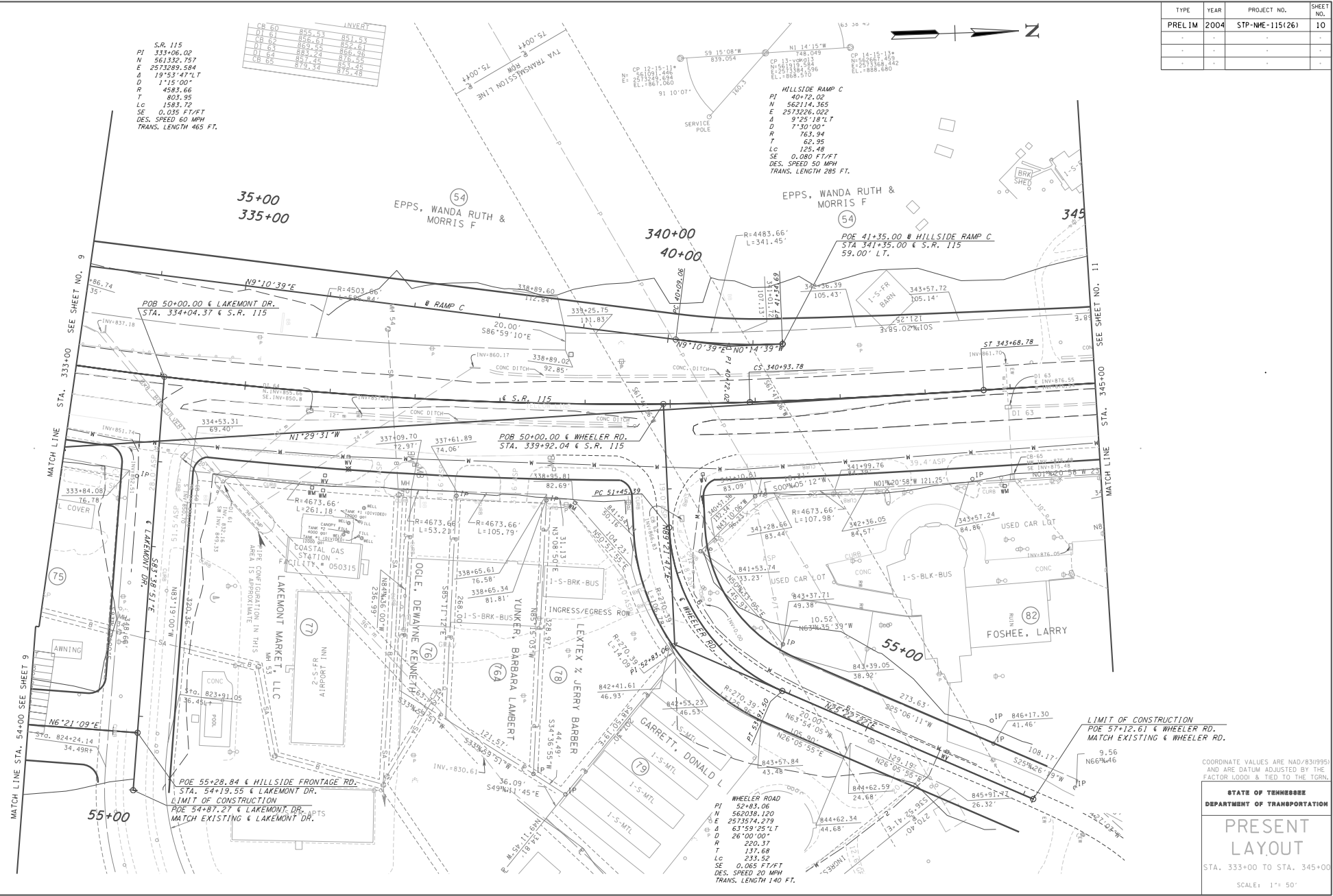
SCALE: 1" = 50'

TYPE	YEAR	PROJECT NO.	SHEET NO.
PRELIM	2004	STP-NHE-115(26)	10
-	-	-	-
-	-	-	-
-	-	-	-

S.R. 115
PI 333+06.00
N 561332.757
E 2573289.584
A 19°55'47"LT
D 1'15'00"
R 4583.66
T 803.95
LC 1583.72
SE 0.035 FT/FT
DES. SPEED 60 MPH
TRANS. LENGTH 465 FT.

CB	60	65	70	75	80	85	90	95	100
INVERT	856.53	856.53	851.53	851.53	852.61	852.61	858.29	858.29	858.29

HILLSIDE RAMP C
PI 40+72.02
N 562114.365
E 2573226.022
A 9°25'18"LT
D 7'30'00"
R 763.94
T 62.95
LC 125.48
SE 0.080 FT/FT
DES. SPEED 50 MPH
TRANS. LENGTH 285 FT.

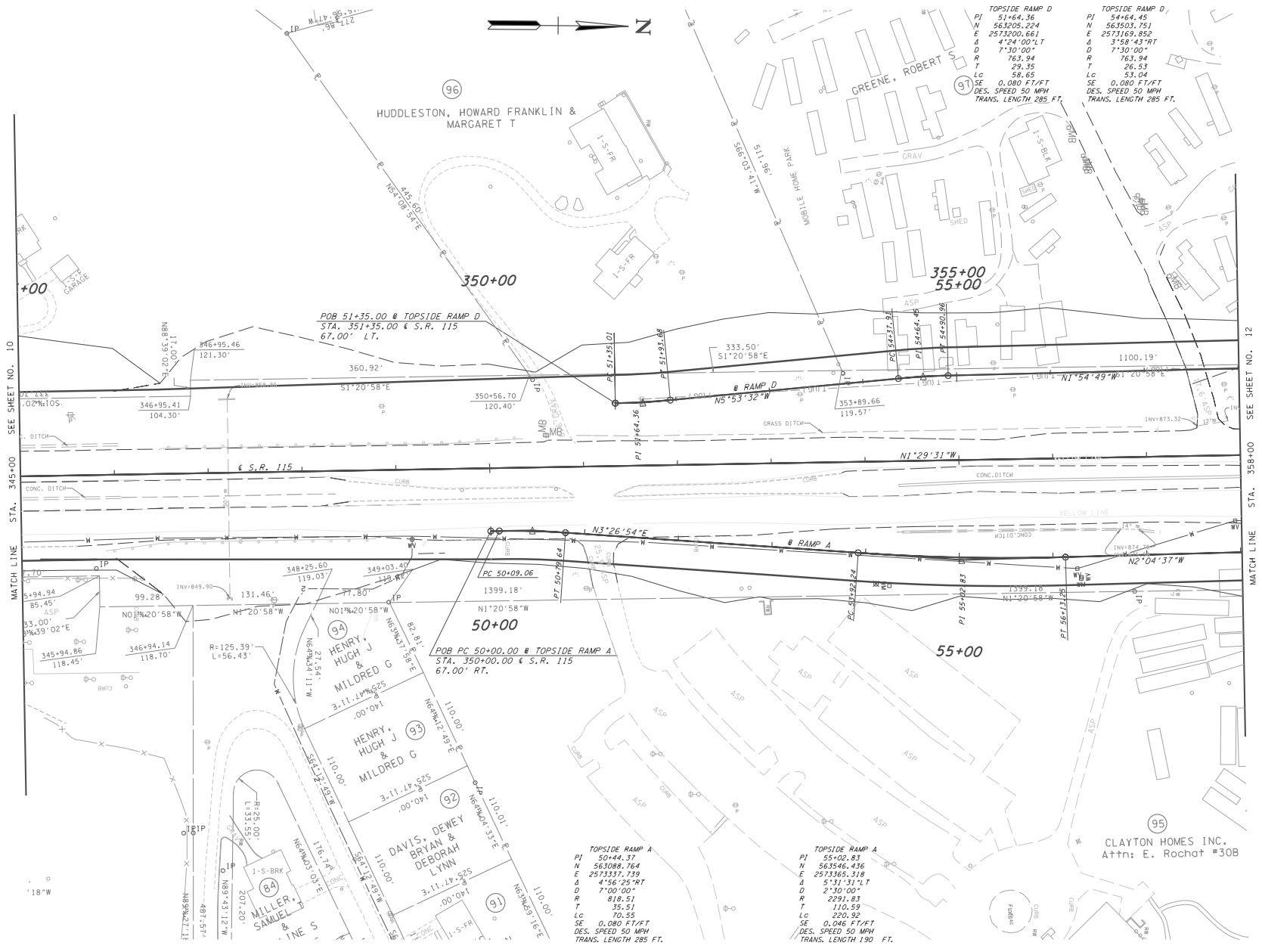


COORDINATE VALUES ARE NAD(83)9951 AND ARE DATUM ADJUSTED BY THE FACTOR 1.0001 & TIED TO THE TGRN.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PRESENT LAYOUT
STA. 333+00 TO STA. 345+00
SCALE: 1" = 50'

TYPE	YEAR	PROJECT NO.	SHEET NO.
PRELIM	2004	STP-NHE-115(26)	11
-	-	-	-
-	-	-	-
-	-	-	-



TOPSIDE RAMP A
 PI 50+44.37
 N 563088.764
 E 2573365.118
 A 4°56'25"RT
 D 7°00'00"
 R 2291.81
 T 110.59
 LC 220.92
 SE 0.046 FT/FT
 DES. SPEED 50 MPH
 TRANS. LENGTH 190 FT.

TOPSIDE RAMP A
 PI 55+02.83
 N 563546.436
 E 2573365.118
 A 5°31'31"LT
 D 2°30'00"
 R 2291.81
 T 110.59
 LC 220.92
 SE 0.046 FT/FT
 DES. SPEED 50 MPH
 TRANS. LENGTH 190 FT.

TOPSIDE RAMP D
 PI 51+64.36
 N 563205.524
 E 2573200.661
 A 4°24'00"LT
 D 7°30'00"
 R 763.94
 T 29.35
 LC 58.65
 SE 0.080 FT/FT
 DES. SPEED 50 MPH
 TRANS. LENGTH 285 FT.

TOPSIDE RAMP D
 PI 54+64.45
 N 563503.751
 E 2573169.852
 A 3°58'43"RT
 D 7°30'00"
 R 763.94
 T 26.53
 LC 53.04
 SE 0.080 FT/FT
 DES. SPEED 50 MPH
 TRANS. LENGTH 285 FT.

POB 51+35.00 @ TOPSIDE RAMP D
 STA. 351+35.00 & S.R. 115
 67.00' LT.

POB PC 50+00.00 @ TOPSIDE RAMP A
 STA. 350+00.00 & S.R. 115
 67.00' RT.

CLAYTON HOMES INC.
 Attn: E. Rochat #30B

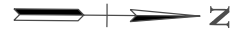
COORDINATE VALUES ARE NAD(83)9951
 AND ARE DATUM ADJUSTED BY THE
 FACTOR 1.0001 & TIED TO THE TGRN.

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

PRESENT LAYOUT

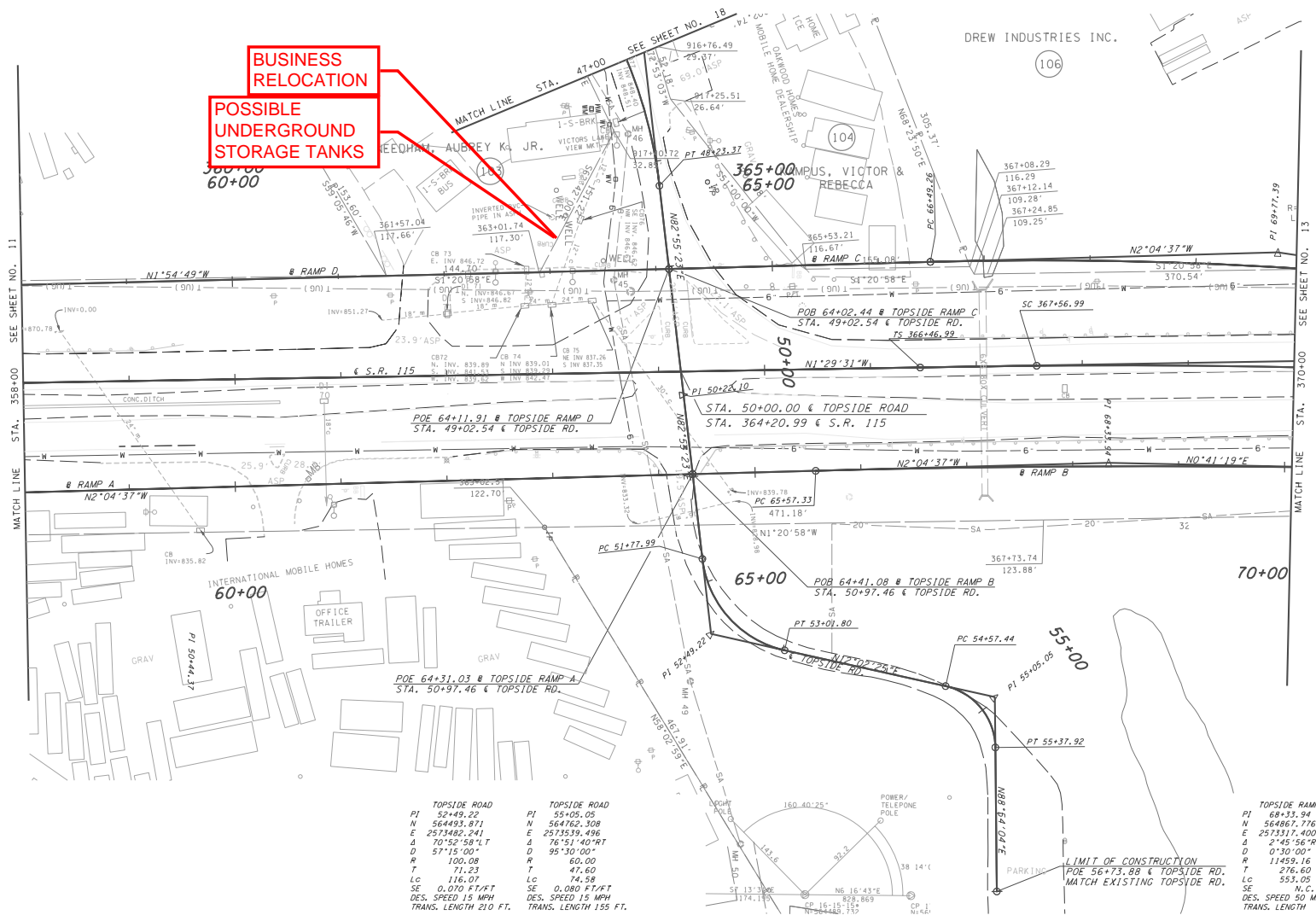
STA. 345+00 TO STA. 357+00
 SCALE: 1" = 50'

TYPE	YEAR	PROJECT NO.	SHEET NO.
PRELIM	2004	STP-NHE-115(26)	12
-	-	-	-
-	-	-	-
-	-	-	-



TOPSIDE RAMP C
 PI 65+77.39
 N 565025.739
 E 257317.361
 Δ 9°49'12"RT
 D 1°30'00"
 R 3819.72
 T 328.13
 LC 653.86
 SE 0.030 FT/FT
 DES. SPEED 50 MPH
 TRANS. LENGTH 145 FT.

**BUSINESS
 RELOCATION**
**POSSIBLE
 UNDERGROUND
 STORAGE TANKS**



TOPSIDE ROAD	PI 52+49.22	TOPSIDE ROAD	PI 55+05.05
N 564493.871	N 564762.308	N 564762.308	N 564762.308
E 257348.241	E 257359.496	E 257359.496	E 257359.496
Δ 70°52'58"LT	Δ 76°51'40"RT	Δ 76°51'40"RT	Δ 76°51'40"RT
D 57°15'00"	D 95°30'00"	D 95°30'00"	D 95°30'00"
R 100.08	R 60.00	R 60.00	R 60.00
T 71.23	T 47.60	T 47.60	T 47.60
LC 116.07	LC 74.58	LC 74.58	LC 74.58
SE 0.070 FT/FT	SE 0.080 FT/FT	SE 0.080 FT/FT	SE 0.080 FT/FT
DES. SPEED 15 MPH	DES. SPEED 15 MPH	DES. SPEED 15 MPH	DES. SPEED 15 MPH
TRANS. LENGTH 210 FT.	TRANS. LENGTH 155 FT.	TRANS. LENGTH 155 FT.	TRANS. LENGTH 155 FT.

TOPSIDE RAMP B
 PI 68+33.94
 N 564867.776
 E 257351.400
 Δ 2°45'56"RT
 D 0°30'00"
 R 11459.16
 T 276.60
 LC 553.05
 SE 0.030 FT/FT
 DES. SPEED 50 MPH
 TRANS. LENGTH 0 FT.

LIMIT OF CONSTRUCTION
 POE 56+73.88 & TOPSIDE RD.
 MATCH EXISTING TOPSIDE RD.

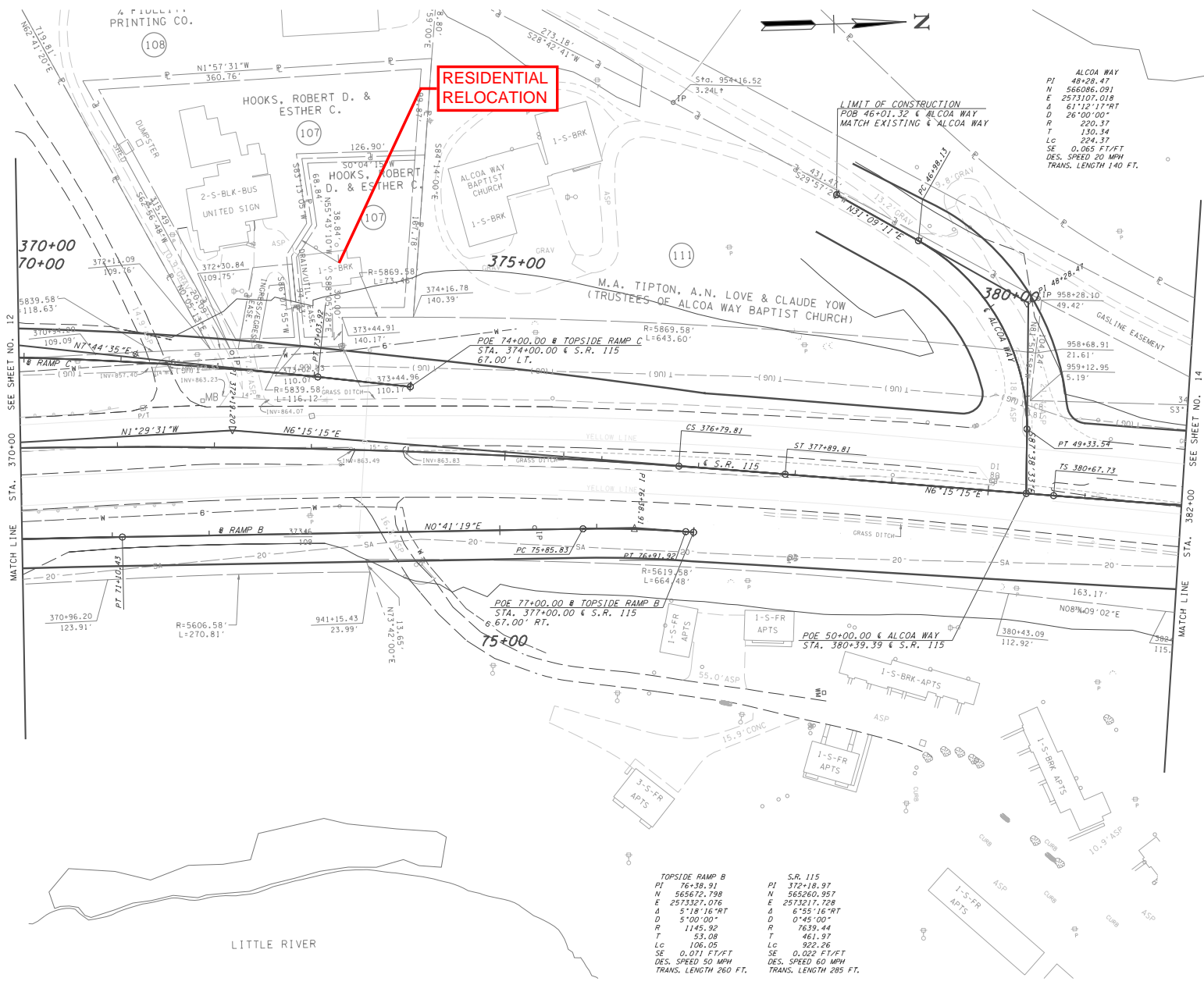
COORDINATE VALUES ARE NAD(83)9951 AND ARE DATUM ADJUSTED BY THE FACTOR 1.0001 & TIED TO THE TORN.

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

PRESENT LAYOUT

STA. 357+00 TO STA. 370+00
 SCALE: 1" = 50'

TYPE	YEAR	PROJECT NO.	SHEET NO.
PRELIM	2004	STP-NHE-115(26)	13
-	-	-	-
-	-	-	-
-	-	-	-



COORDINATE VALUES ARE NAD(83)9951 AND ARE DATUM ADJUSTED BY THE FACTOR 1.0001 & TIED TO THE TORN.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

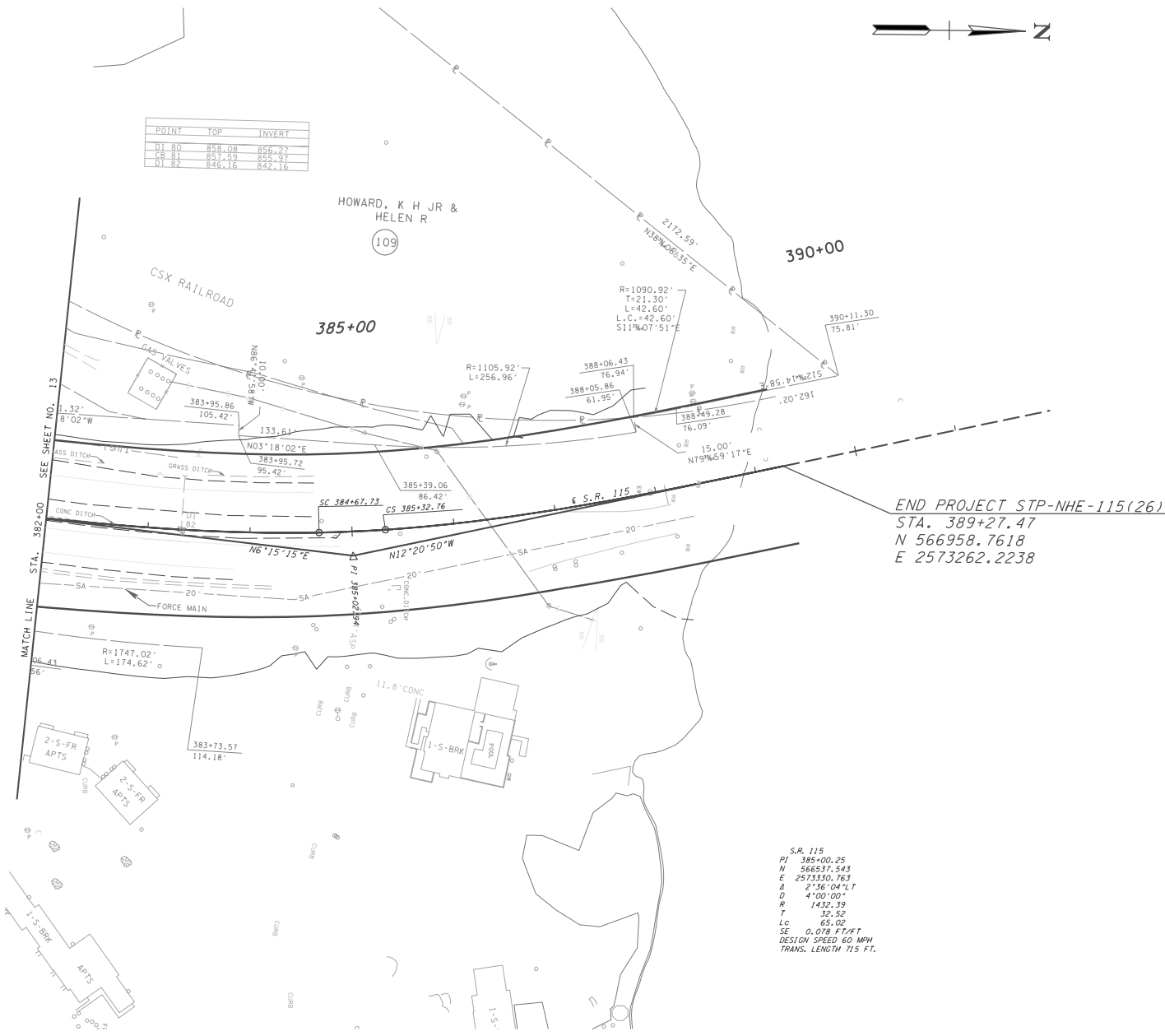
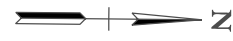
PRESENT LAYOUT

STA. 370+00 TO STA. 382+00

SCALE: 1" = 50'

TYPE	YEAR	PROJECT NO.	SHEET NO.
PRELIM	2004	STP-NHE-115(26)	14
-	-	-	-
-	-	-	-
-	-	-	-

POINT	TOP	INVERT
D1 80	858.08	856.23
CB 81	857.59	855.97
D1 82	846.16	842.16



S.R. 115
 PI 385+00.25
 N 566537.543
 E 2573330.763
 Δ 2'56.04" LT
 D 4'00'00"
 R 1432.39
 T 32.52
 LC 65.02
 SE 0.078 FT/FT
 DESIGN SPEED 60 MPH
 TRANS. LENGTH 715 FT.

COORDINATE VALUES ARE NAD(83)9951
 AND ARE DATUM ADJUSTED BY THE
 FACTOR 1.0001 & TIED TO THE TORN.

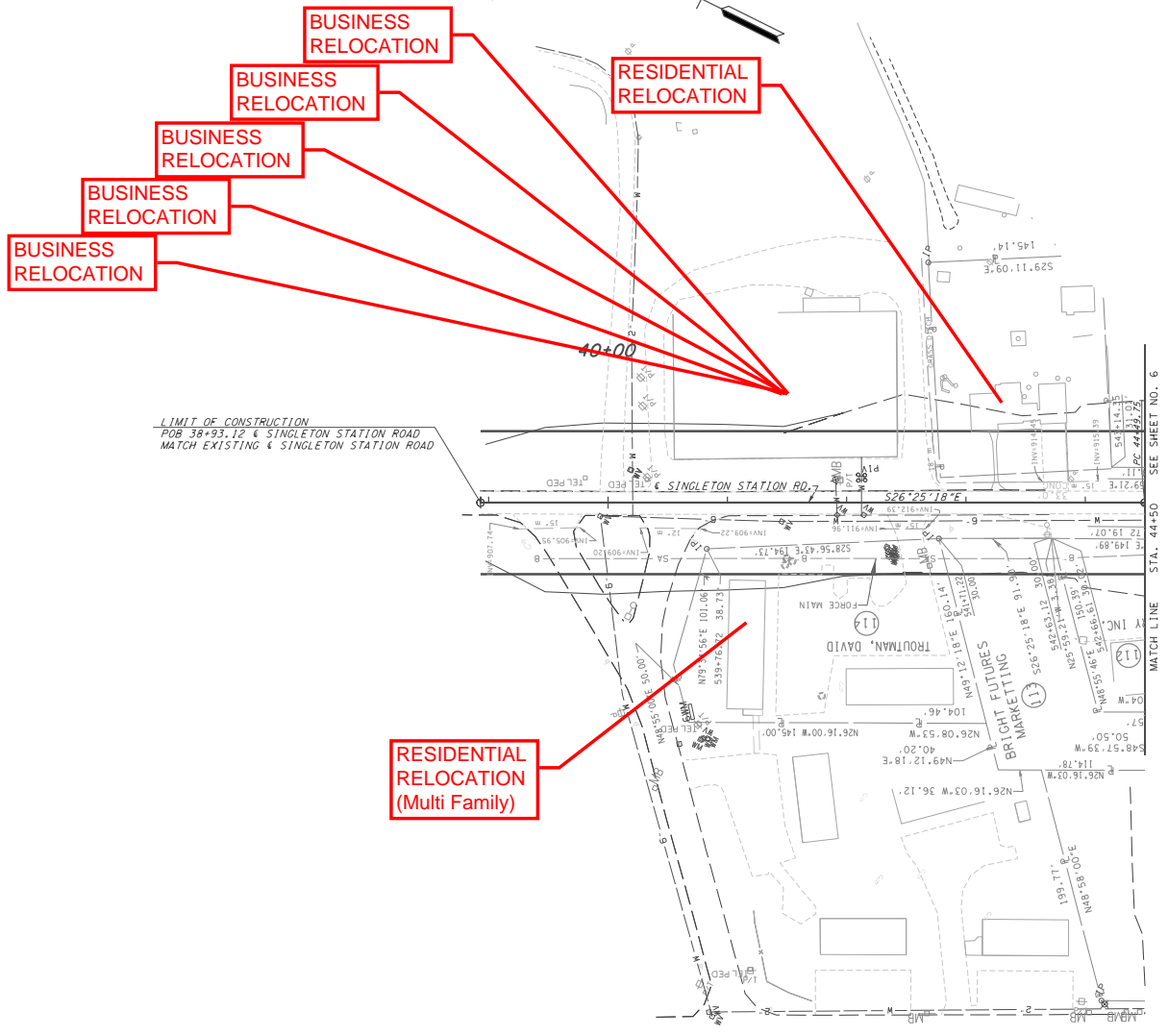
STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

PRESENT LAYOUT

STA. 382+00 TO END

SCALE: 1" = 50'

TYPE	YEAR	PROJECT NO.	SHEET NO.
PRELIM	2004	STP-NHE-115(26)	15
-	-	-	-
-	-	-	-
-	-	-	-



10/13/2011
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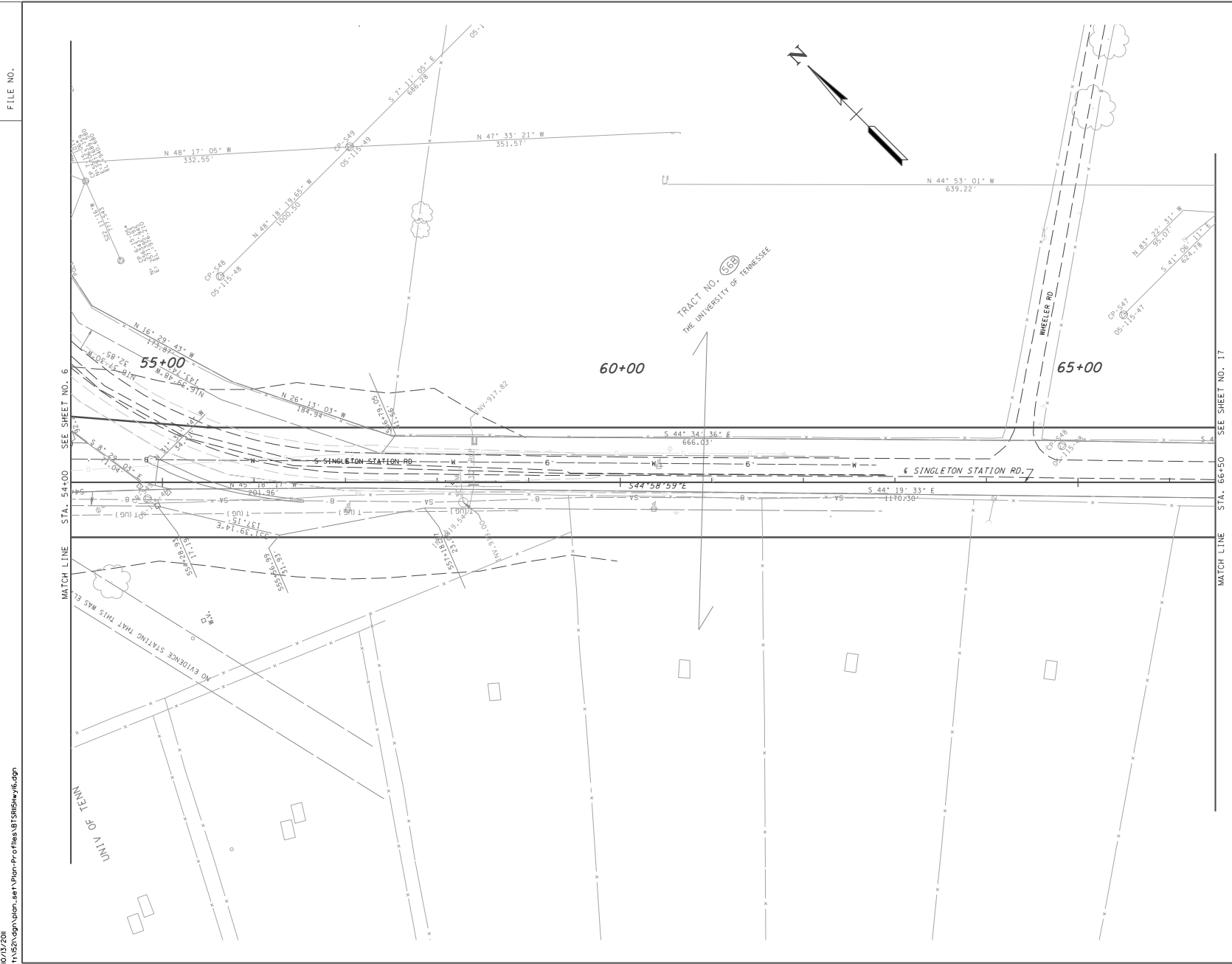
COORDINATE VALUES ARE NAD/83(995)
 AND ARE DATUM ADJUSTED BY THE
 FACTOR 1.0001 & TIED TO THE TORN.

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

PRESENT LAYOUT
 SINGLETON STATION ROAD
 BEGIN TO STA. 44+50
 SCALE: 1" = 50'

TENNESSEE D.O.T.
 DESIGN DIVISION
 FILE NO.

TYPE	YEAR	PROJECT NO.	SHEET NO.
PRELIM	2004	STP-NHE-115(26)	16
-	-	-	-
-	-	-	-
-	-	-	-



COORDINATE VALUES ARE NAD/83(1995)
 AND ARE DATUM ADJUSTED BY THE
 FACTOR 1.0001 & TIED TO THE TGRN.

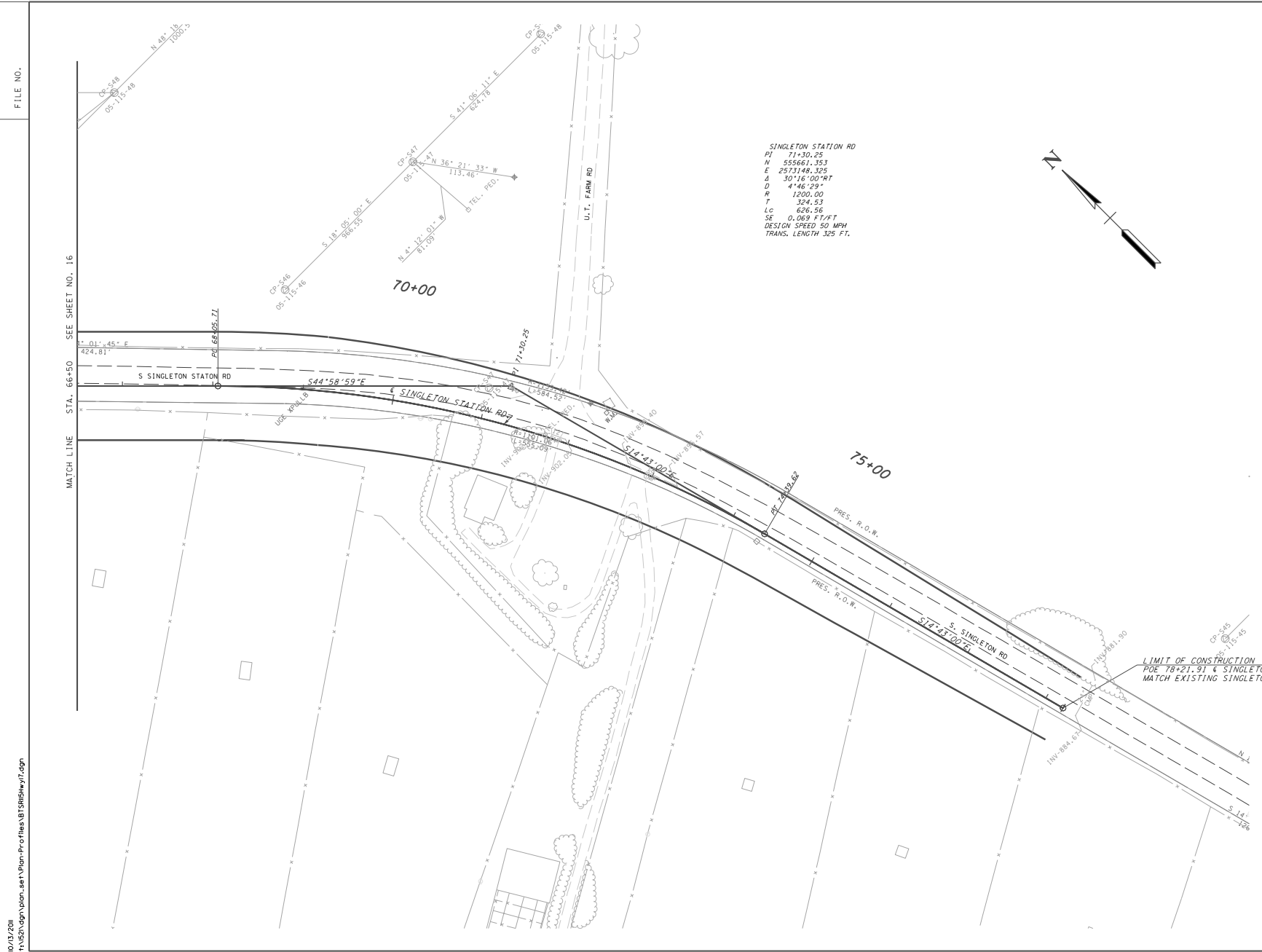
STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

PRESENT LAYOUT
 SINGLETON STATION ROAD
 STA. 54+00 TO STA. 66+50
 SCALE: 1" = 50'

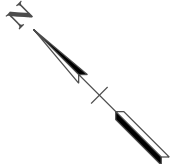
10/13/2011
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TENNESSEE D.O.T.
 DESIGN DIVISION
 FILE NO.

TYPE	YEAR	PROJECT NO.	SHEET NO.
PRELIM	2004	STP-NHE-115(26)	17
-	-	-	-
-	-	-	-
-	-	-	-



SINGLETON STATION RD
 PJ 71+30.25
 W 555661.353
 E 2573148.325
 Δ 30°16'00"RT
 D 4'46'29"
 R 19200.00
 T 324.53
 Lc 626.96
 SE 0.009 FTY/FT
 DESIGN SPEED 50 MPH
 TRANS. LENGTH 325 FT.



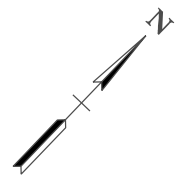
LIMIT OF CONSTRUCTION
 POE 78+21.91 & SINGLETON STATION ROAD
 MATCH EXISTING SINGLETON STATION ROAD

COORDINATE VALUES ARE NAD(83)995
 AND ARE DATUM ADJUSTED BY THE
 FACTOR 1.0001 & TIED TO THE TORN.

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

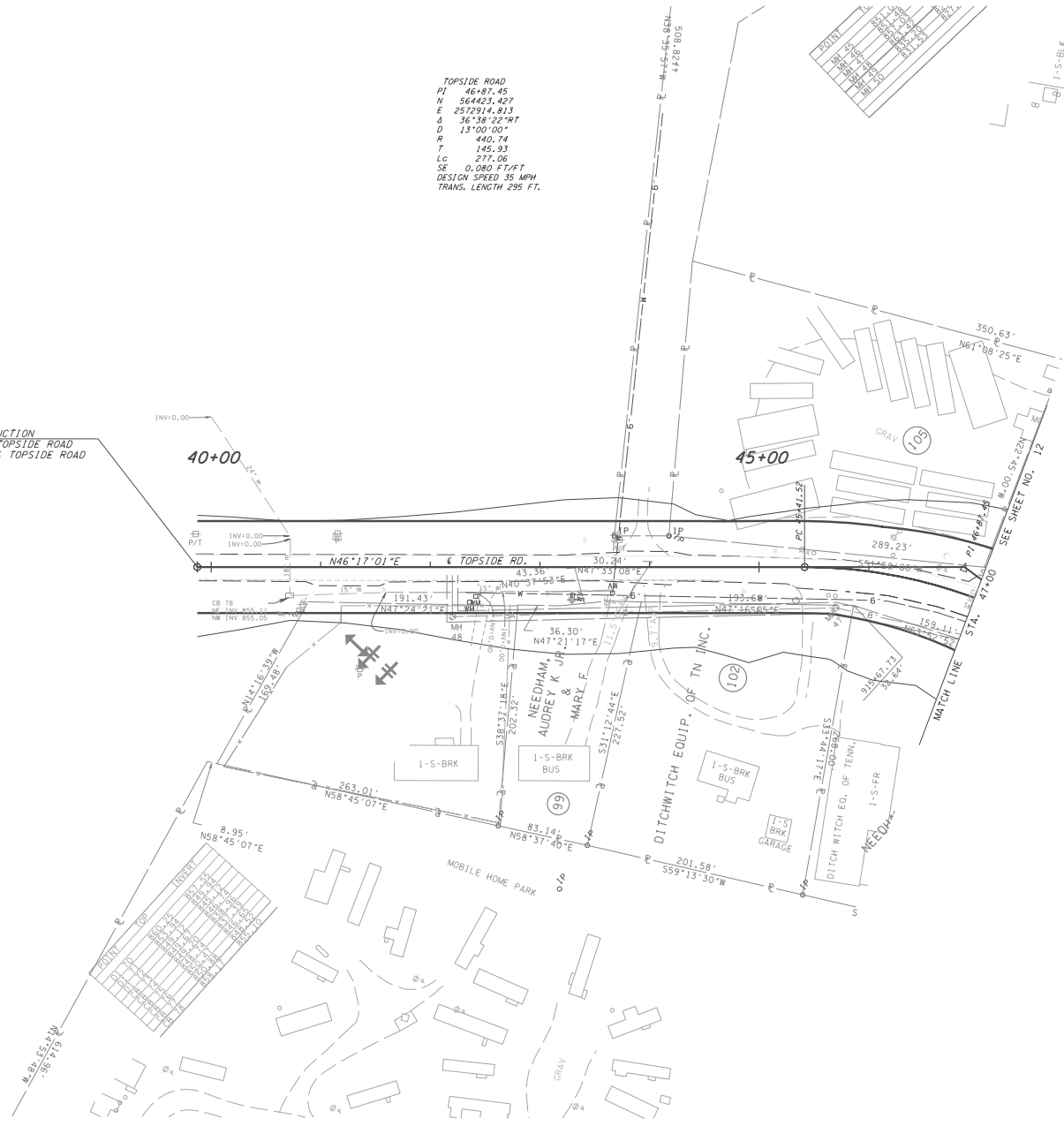
PRESENT LAYOUT
 SINGLETON STATION ROAD
 STA. 66+50 TO END
 SCALE: 1" = 50'

10/13/2011
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TOPSIDE ROAD
PI 46+87.45
N 564423.427
E 2572914.813
Δ 36°38'22"RT
D 13°00'00"
R 440.34
T 145.93
LC 277.06
SE 0.080 77.4 FT
DESIGN SPEED 35 MPH
TRANS. LENGTH 295 FT.

LIMIT OF CONSTRUCTION
POB 39+87.37 & TOPSIDE ROAD
MATCH EXISTING & TOPSIDE ROAD



TYPE	YEAR	PROJECT NO.	SHEET NO.
PRELIM	2004	STP-NHE-115(26)	18
-	-	-	-
-	-	-	-
-	-	-	-

COORDINATE VALUES ARE NAD/83(1995)
AND ARE DATUM ADJUSTED BY THE
FACTOR 1.0001 & TIED TO THE TGN.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PRESENT LAYOUT
TOPSIDE ROAD
BEGIN TO STA. 47+00
SCALE: 1" = 50'

Index Of Sheets

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2-20	TYPICAL SECTIONS
3-3A	PROPERTY MAP
3B	R.O.W. ACQUISITION TABLE
4-14	PRESENT LAYOUT
4A-15A	PROPOSED LAYOUT
4B-15B	PROFILES
4C-15C	SIDEROAD PROFILES
4D-15D	PRIVATE DRIVE PROFILES
4E-15E	DRAINAGE MAP
4F-15F	CULVERT CROSS-SECTIONS
4G-15G	ROADWAY CROSS-SECTIONS

TENN.	YEAR	1
FED. AID PROJ. NO.	STP-NHE-115(27)	
STATE PROJ. NO.		

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF PLANNING AND DEVELOPMENT

KNOX COUNTY

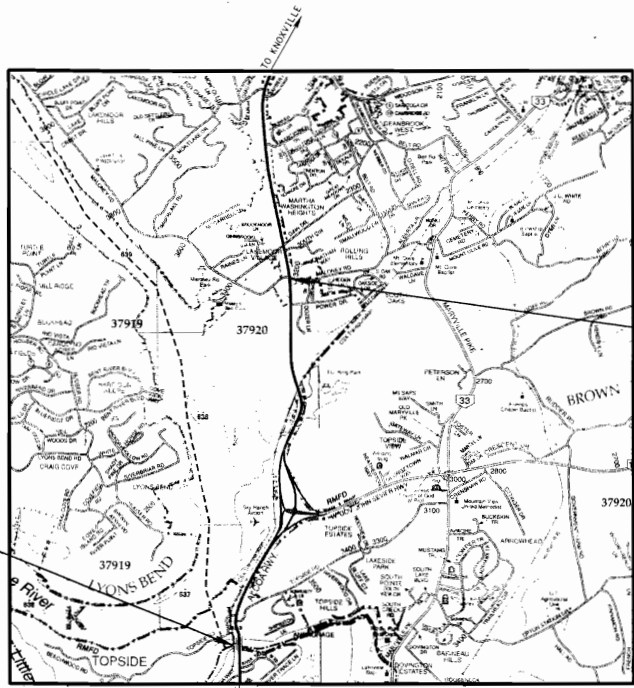
S.R. 115 (US-129) ALCOA HIGHWAY
FROM: NORTH OF LITTLE RIVER
TO: MALONEY ROAD

R.O.W.

STATE HIGHWAY NO. 115 F.A.H.S. NO. N/A



FOR PRELIMINARY FIELD REVIEW ONLY



NO EXCLUSIONS
NO EQUATIONS

END PROJECT STP-NHE-115(27)
STA. 205+56.46 (R.O.W.)

BEGIN PROJECT STP-NHE-115(27)
STA. 99+12.26 (R.O.W.)

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED MARCH 1, 1995 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT

SCALE: 1" = 2700'

R.O.W. LENGTH = 2.016 MILES

TRAFFIC DATA	
ADT (2005)	59,530
ADT (2025)	89,620
DHV (2025)	8,982
D	70 - 30
T (ADT)	6 %
T (DHV)	4 %
V	60 MPH

APPROVED: *John C. Jones*
DIRECTOR, DESIGN DIVISION

DATE: _____

APPROVED: *Donna F. Kelly*
COMMISSIONER

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: _____
DIVISION ADMINISTRATOR DATE

PIN 100241.02

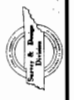
TDOT TRANS. MGR. 1 CLIFFORD STEWART

DESIGNED BY: ROBERT G. CAMPBELL & ASSOCIATES L.P.

DESIGNER: JAMES R. GARRETT, P.E.

P.E. NO.: 47039-1269-34

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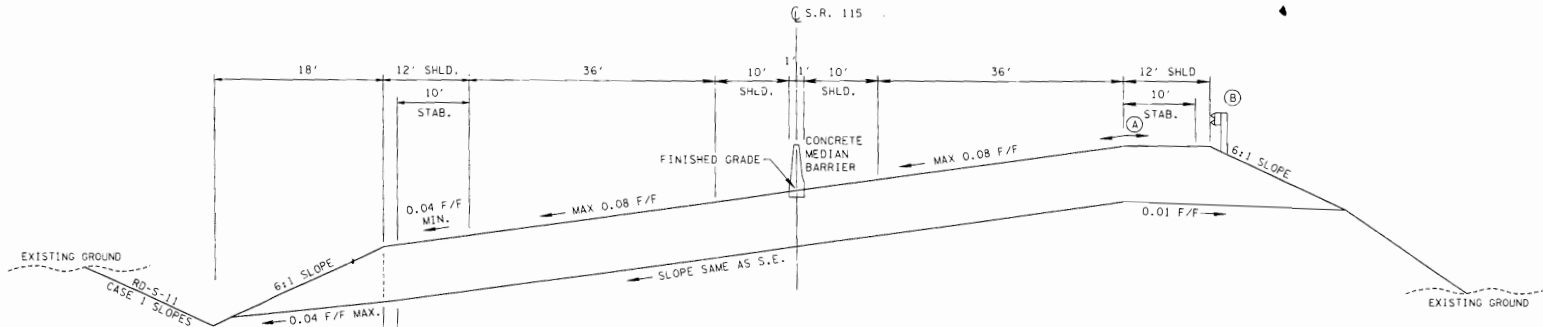


DESIGN DIVISION

FILE NO.

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2003	STP-NHE-115(27)	2

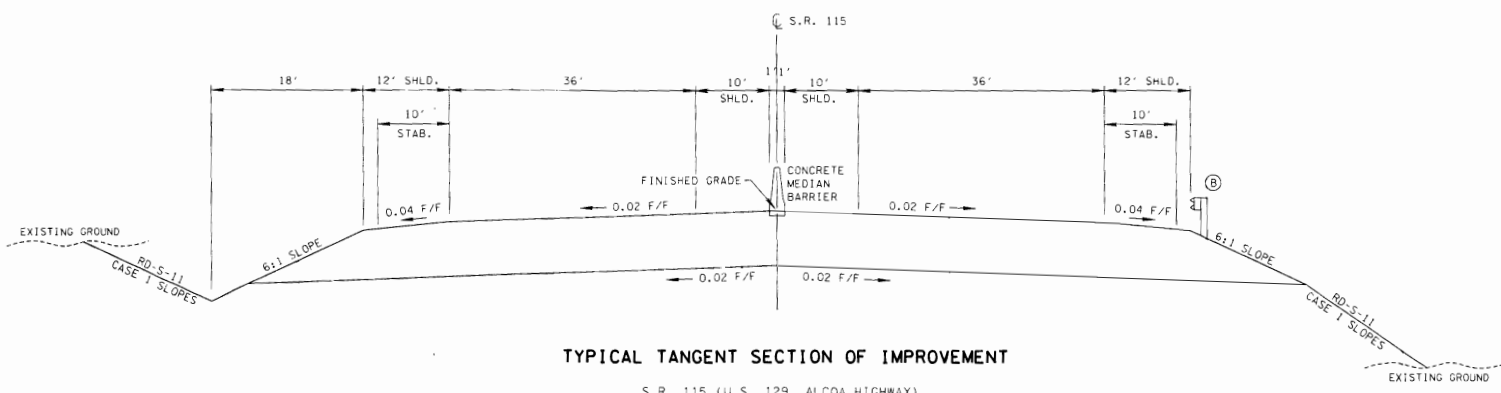
KNOX CO. S.R. 115 (ALCOA HWY)



TYPICAL SUPERELEVATED SECTION OF IMPROVEMENT

S.R. 115 (U.S. 129, ALCOA HIGHWAY)

- (A) THE SLOPES OF SHOULDER AND ROADWAY SHALL NOT EXCEED AN ALGEBRAIC DIFFERENCE OF 7%
- (B) GUARDRAILS AS INDICATED ON PROPOSED LAYOUT



TYPICAL TANGENT SECTION OF IMPROVEMENT

S.R. 115 (U.S. 129, ALCOA HIGHWAY)

SEE RD-S-11 FOR OTHER DETAILS NOT SHOWN

PROPOSED PAVEMENT SCHEDULE

--	--	--	--

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF PLANNING & DEVELOPMENT

TYPICAL SECTIONS

FOR PRELIMINARY FIELD REVIEW ONLY

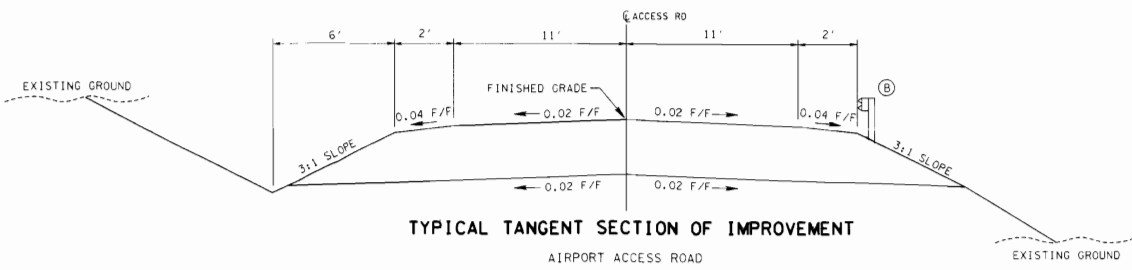
RD-S-11
CASE 1 SLOPES



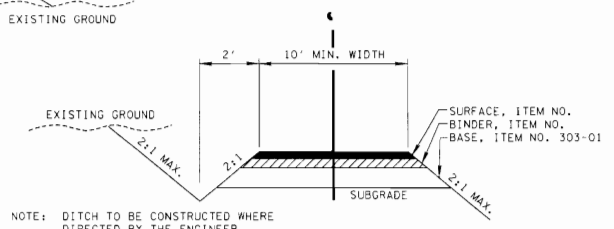
TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2003	STP-NHE-115(27)	2A

KNOX CO. S.R. 115 (ALCOA HWY)

- (A) THE SLOPES OF SHOULDER AND ROADWAY SHALL NOT EXCEED AN ALGEBRAIC DIFFERENCE OF 7%
- (B) GUARDRAILS AS INDICATED ON PROPOSED LAYOUT



TYPICAL TANGENT SECTION OF IMPROVEMENT
AIRPORT ACCESS ROAD

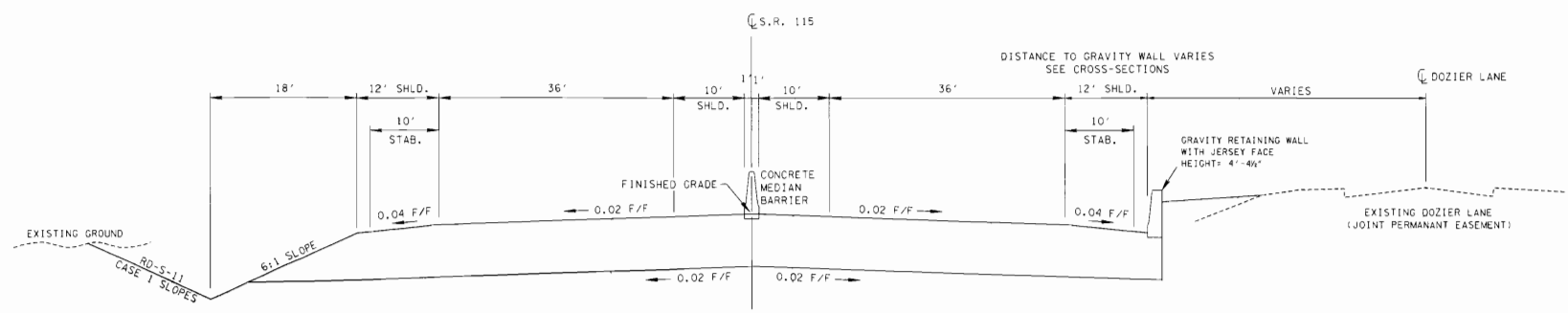


NOTE: DITCH TO BE CONSTRUCTED WHERE DIRECTED BY THE ENGINEER

TYPICAL SECTION
PRIVATE DRIVE TO BUSINESS, FIELD, OR RESIDENTIAL PROPERTY

BUSINESS	FIELD OR RESIDENTIAL
SURFACE - 1 1/4" ±	SURFACE - 1 1/4" ±
BINDER - 1 1/4" ±	BINDER - NONE
BASE - 4" ±	BASE - 4" ±

WHERE EXIST. DRIVE IS CONCRETE
SUBSTITUTE 4" CLASS "A" CONCRETE
FOR SURFACE & BINDER



TYPICAL TANGENT SECTION OF IMPROVEMENT
S.R. 115 @ DOZIER LANE (STA 185+50 TO STA 195+75)

SEE STD DWG RD-S-11
FOR OTHER DETAILS
NOT SHOWN

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF PLANNING & DEVELOPMENT

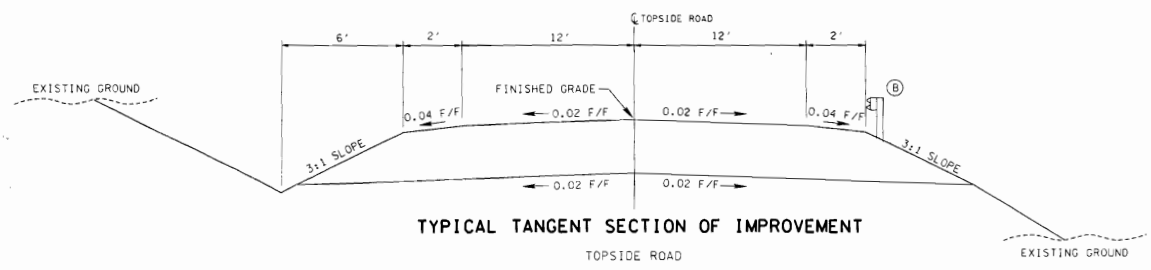
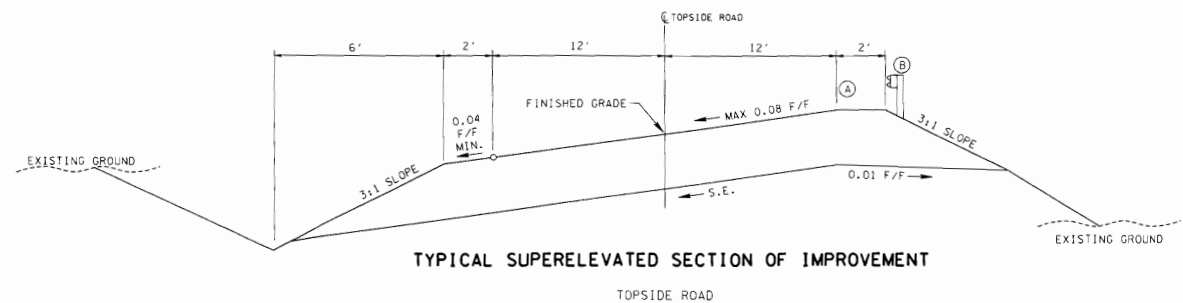
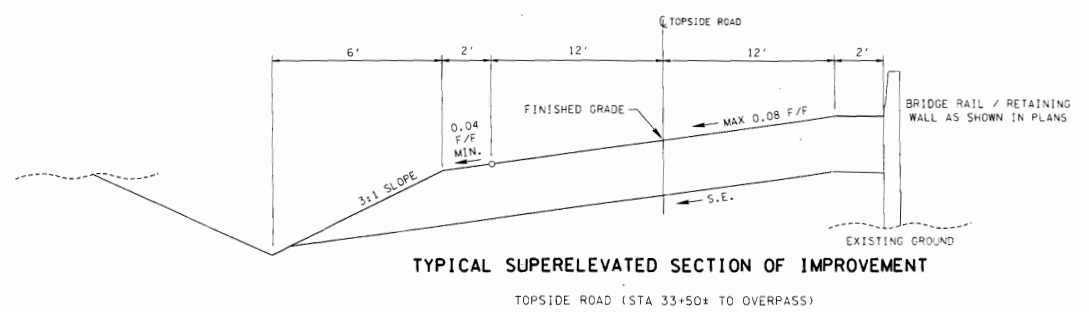
TYPICAL SECTIONS

FOR PRELIMINARY FIELD REVIEW ONLY

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2003	STP-NHE-115(27)	2B

KNOX CO. S.R. 115 (ALCOA HWY)

- (A) THE SLOPES OF SHOULDER AND ROADWAY SHALL NOT EXCEED AN ALGEBRAIC DIFFERENCE OF 7%
- (B) GUARDRAILS AS INDICATED ON PROPOSED LAYOUT



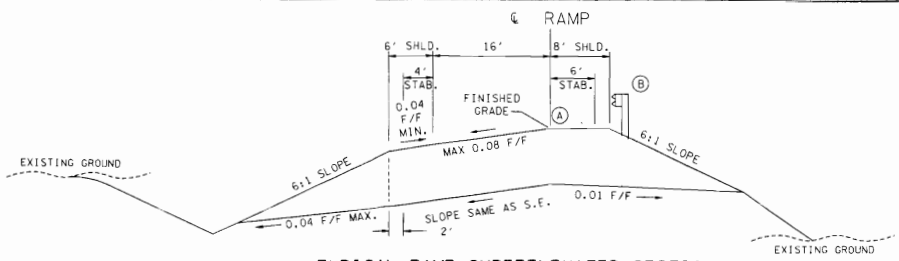
FOR PRELIMINARY FIELD REVIEW ONLY

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2003	STP-NHE-115(27)	2C

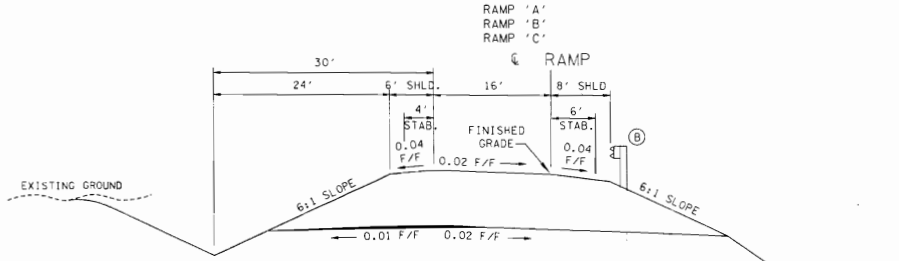
KNOX CO. S.R. 115 (ALCOA HWY)

(A) THE SLOPES OF SHOULDER AND ROADWAY SHALL NOT EXCEED AN ALGEBRAIC DIFFERENCE OF 7%.

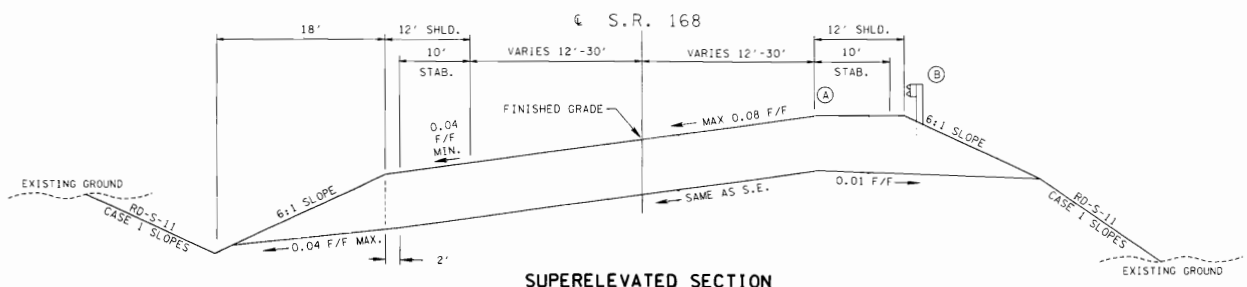
(B) GUARDRAILS AS INDICATED ON PROPOSED LAYOUT



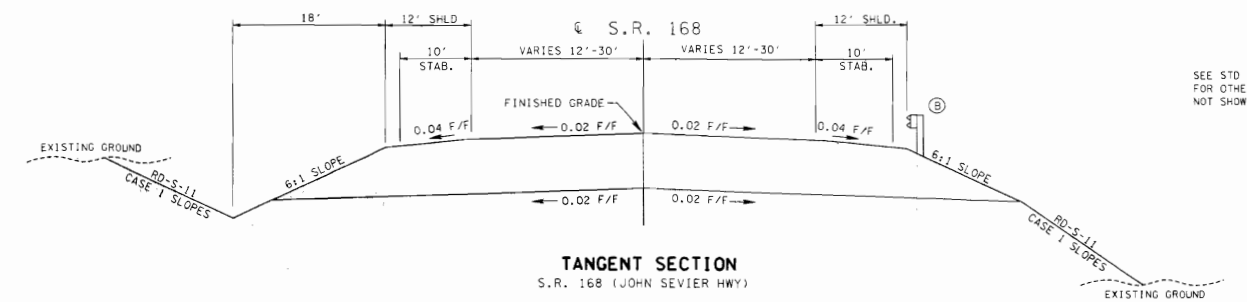
TYPICAL RAMP SUPERELEVATED SECTION



TYPICAL RAMP TANGENT SECTION



SUPERELEVATED SECTION
S.R. 168 (JOHN SEVIER HWY)



TANGENT SECTION
S.R. 168 (JOHN SEVIER HWY)

SEE STD DWG RD-S-11 FOR OTHER DETAILS NOT SHOWN

FOR PRELIMINARY FIELD REVIEW ONLY

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF PLANNING & DEVELOPMENT

TYPICAL SECTIONS

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TENNESSEE D.O.T.
DESIGN DIVISION
FILE NO.

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2003	STP-NHE-115(27)	3B

S.R. 115 (ALCOA HWY) KNOX COUNTY

TRACT NO.	PROPERTY OWNER	COUNTY RECORDS				TOTAL AREA (AC.)			AREA TO BE ACQUIRED (AC.)			AREA REMAINING			EASEMENT (S.F.)		
		TAX MAP NO.	PARCEL	DEED BOOK	PAGE	LEFT	RIGHT	TOTAL	LEFT	RIGHT	TOTAL	LEFT	RIGHT	PERM. DRAINAGE	SLOPE	CONST.*	
1	THE ANCHORAGE HOMEOWNERS ASSOCIATION, INC.	1471B	66	2044	135		2,204			0.634	0.634		1,571				
1A	JUBRAN RAJA TRUSTEE	1471B	5	2044	135		0,428			0,197	0,197		0,231				
1B	J. MADE & NANNA B. GILLEY	1471B	3 & 4	2094	135		0,766			0,308	0,308		0,458				
1C	PAUL S. JR. & JARET L. HOGAN	1471B	2	2094	135		0,357						0,357				
2	ACBL, INC.	147	50.01	2172	1040		0,219			0,219							
3	WRB & ASSOCIATES, INC.	147	16	1851	725	3,500			0,840	0,840	2,660						
4	J. RANDAL GREAVES & NOLA JAM FOSTER GREAVES	147	15	2161	206	5,255							4,359	1,174			
5	TIMOTHY M. & KIMBERLY F. MCLEMORE	147	18	1919	562	3,767							3,767				
6	EAST TENNESSEE NATURAL GAS COMPANY	147	19	913	35	3,942			1,672	1,672			2,270				
7	JAMES S. TIPTON, JR.	147	18.01	1919	559	6,890					6,890						
8	MARK J. & VICKIE A. MARGETTS	147	21	2241	135	30,260			1,793	1,793			28,467				
9	EAST TENNESSEE PILOTS CLUB	147	22	2086	90	74,958					74,958						
10	JERRY B. & PHILLIP W. & MARY B. LIND	147	30	2089	822	35,081			0,365	0,365			34,717				
11	LEE VERN & HELEN B. LAMBERT	147	31	1956	401	2,197			0,701	0,701			1,496				
12	LEE VERN & HELEN B. LAMBERT	147	32	1956	401	63,950			0,572	0,572			63,377				
12A	BRUCE & BRENDA G. MCLELLAN	147	33	1763	311	2,461							2,432				
13	FRED C. & MILDRED C. SCRUGGS AND PAUL LEE DOCKERY	147	28	1870	764	1,955							1,955				
14	ROBERT ROZAR, RICHARD MATTHEW TOOLE AND LAURA LEE TOOLE	147	23	2284	667	0,898			0,898	0,898							
15	KNOX COUNTY	135	15	905	229	21,854			3,019	3,019			18,835				
16	KNOX COUNTY	135	16	905	229	1,245					1,245			11,765	4,101		
16A	TENNESSEE VALLEY AUTHORITY	408												40880	0,401 AC.		
17	KNOX COUNTY	135	18	905	229	3,263							3,263	2,999	3,054		
18	UNIVERSITY OF TENNESSEE	135	11	1223	1082	212,817			2,313	2,313	210,504						
19	TIMOTHY F. NEAL, CLINTON A. NEAL, JR. AND SHERYL S. NEAL	135	10.25	2293	461	1,122			3,271 S.F.	3,271 S.F.			1,047	423	3,677		
20	SANDY STEPHS	135	10.22	2303	1079	0,777							0,777				
21	THREE PARTNERS, LTD.	135	10.21	2011	903	5,641							5,641				
22	DENNIS E. STARR	135	10.23	2210	450	4,214							4,214				
23	BRANDS PROPERTIES	135	10.17	2512	849	10,434							10,434				
24	THREE PARTNERS, LTD.	135	10.18	2011	903	0,822							0,822	5363	3,474		
25	SEVER HEIGHTS BAPTIST CHURCH	135	10.04	2137	300	33,851							33,851	5370	2,658		

* CONSTRUCTION EASEMENT FOR EROSION CONTROL STRUCTURES AND WORKING ROOM

RIGHT OF WAY NOTES

IT IS INTENDED THAT ALL BUILDINGS AND/OR PORTIONS OF BUILDINGS THAT ARE WITHIN THE PROPOSED RIGHT-OF-WAY AND/OR EASEMENT LINES FOR THE PROJECT, BE REMOVED THEREFROM IN THE PROCESS OF RIGHT-OF-WAY ACQUISITION. IF ANY SUCH BUILDINGS OR IMPROVEMENTS ARE NOT REMOVED IN THE COURSE OF RIGHT-OF-WAY ACQUISITION, THE CIVIL ENGINEERING MANAGER 2, DESIGN DIVISION IS TO BE NOTIFIED IN SUFFICIENT TIME TO PERMIT HAVING SUCH REMOVALS DESIGNATED AS A PART OF THE CONSTRUCTION CONTRACT.

ALL RAMP MUST CONFORM TO THE DEPARTMENT'S POLICY ON FINANCING CONSTRUCTION OF PUBLIC ROAD INTERSECTIONS AND DRIVEWAYS ON HIGHWAY RESURFACING, RECONSTRUCTION AND CONSTRUCTION PROJECTS ON NEW LOCATIONS. THE MANUAL ON RULES AND REGULATIONS FOR CONSTRUCTING DRIVEWAYS ON STATE HIGHWAY RIGHT-OF-WAY, STANDARD DRAWING RP-R-1, AND OTHER ACCEPTED DESIGN AND SAFETY STANDARDS.

EXISTING PAVED DRIVEWAY PER TRACT REMAINDER WILL BE REPLACED IN KIND TO A TOUCHDOWN POINT.

WHERE THE EXISTING DRIVEWAY IS UNPAVED AND THE PROPOSED DRIVEWAY EXCEEDS 7 PERCENT IN GRADE, EACH DRIVEWAY WILL BE PAVED TO A TOUCHDOWN POINT OR UNTIL THE GRADE IS LESS THAN 7 PERCENT.

WHERE THE EXISTING DRIVEWAY IS UNPAVED AND THE PROPOSED DRIVEWAY IS LESS THAN 7 PERCENT IN GRADE, EACH DRIVEWAY WILL BE PAVED A SHOULDER WIDTH FROM THE EDGE OF PAVEMENT AND THE REMAINDER OF THAT DRIVEWAY REPLACED IN KIND TO A TOUCHDOWN POINT.

ANY NECESSARY PAVING OF DRIVEWAYS WILL BE DONE DURING PAVING OPERATIONS ON THE MAIN ROADWAY.

TRACT REMAINDERS NOT HAVING AN EXISTING DRIVEWAY WILL BE PROVIDED ONE 50 FOOT OPENING IN THE ACCESS CONTROL FENCE AND A DRIVEWAY WILL BE CONSTRUCTED UNLESS PROVISION IS PROVIDED FROM AN INTERSECTING ROAD OR BASED ON PHYSICAL CONDITIONS AND/OR CONFLICTS WITH OTHER DESIGN CONSIDERATIONS WHICH PREVENT AN ACCESS DURING PAVING OF THESE NEW DRIVEWAYS WILL BE IN ACCORDANCE TO THE 7 PERCENT CRITERIA PREVIOUSLY MENTIONED FOR EXISTING DRIVEWAYS.

NEW DRIVEWAYS PROVIDED IN THE PLANS WILL BE PAVED BASED ON THE 7 PERCENT CRITERIA. THOSE 7 PERCENT OR STEEPER IN GRADE WILL BE PAVED AND THOSE FLATTER THAN 7 PERCENT WILL BE COVERED WITH BASE STONE.

ON PROJECTS WITHOUT CURB AND GUTTER THAT ARE ON STATE ROUTES, IT WILL BE THE RESPONSIBILITY OF THE OWNER TO SECURE A PERMIT AND TO CONSTRUCT ADDITIONAL DRIVEWAYS AND FIELD ENTRANCES OTHER THAN THOSE PROVIDED IN THE PLANS.

ON NON-STATE ROUTES, ADDITIONAL DRIVEWAYS AND FIELD ENTRANCES OTHER THAN THOSE PROVIDED IN THE PLANS SHALL REQUIRE A PERMIT UNLESS IF THE LOCAL AGENCY SPECIFIES THE NEED FOR THAT PERMIT.

UTILITY	OWNER	PHONE NO.	CONTACT	ADDRESS	CITY	STATE	ZIP CODE
TELEPHONE	BELLSOUTH	865-537-8644	JOHN WETNAM	5315 PARKWAY DR	KNOXVILLE	TN	37922
WATER	KNOXVILLE UTILITIES BOARD	865-538-2100	CAROL BOLAS	PO BOX 39017	KNOXVILLE	TN	37939
SEWER	KNOXVILLE UTILITIES BOARD	865-537-4937	JACK LINSLEY	PO BOX 39049	KNOXVILLE	TN	37940
	KNOXVILLE UTILITIES BOARD	865-538-2100	GABRIEL BOLAS	PO BOX 39017	KNOXVILLE	TN	37939
CABLE	KNOXVILLE UTILITIES BOARD	865-537-4937	JACK LINSLEY	PO BOX 39049	KNOXVILLE	TN	37940
	KNOXVILLE UTILITIES BOARD	865-538-2100	GABRIEL BOLAS	PO BOX 39017	KNOXVILLE	TN	37939
CABLE TV	COMCAST COMMUNICATIONS	865-862-5060	WESLEY SUFFRIDGE	5720 ASHWILLE HWY	KNOXVILLE	TN	37924

FOR PRELIMINARY FIELD REVIEW ONLY

COORDINATE VALUES ARE NAD83(1995) AND ARE DATUM ADJUSTED BY THE FACTOR 1.000009166 & TIED TO THE TCRN. ELEVATIONS ARE NAVD 1988

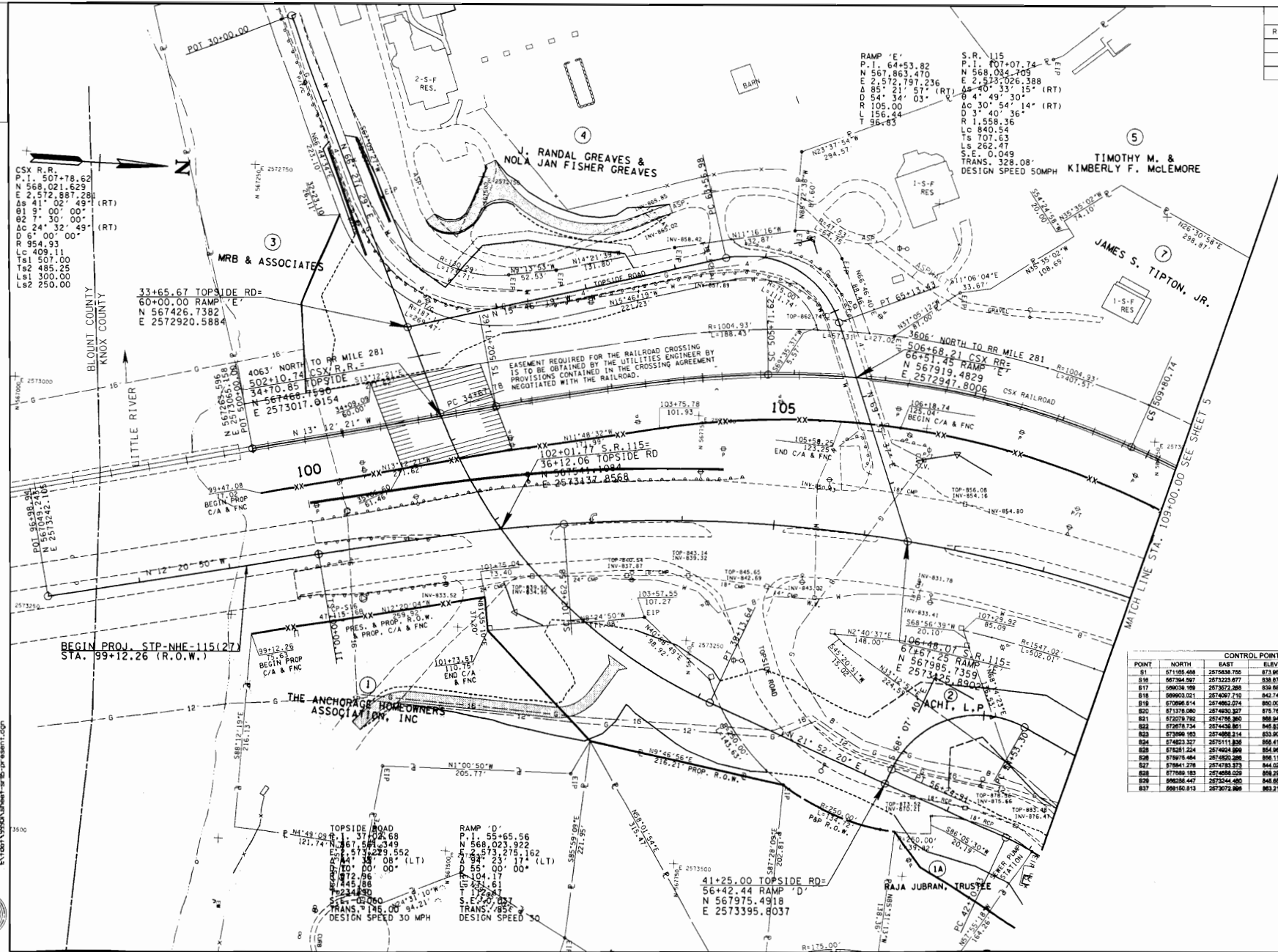
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF PLANNING & DEVELOPMENT

**R.O.W.
ACQUISITION
TABLE**

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TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2002	STP-NHE-115(27)	4



CONTROL POINTS INFORMATION						
POINT	NORTH	EAST	ELEV.	STATION	OFFSET	DESCRIPTION
91	57155.488	287688.765	873.98	89+00.00 S.R. 108	38.8	200.01
916	567384.507	287223.877	838.87	100+40.38	55.8	4611.168
917	569036.189	287262.388	839.88	118+01.16	6.3	4715.17
918	569033.021	287400.710	840.74	128+11.80	50.8	4848.18
919	570096.814	287486.074	800.00	137+78.42	140.8	4713.19
920	871378.080	287493.327	878.78	144+43.97	183.1	4715.20
921	572078.792	287478.860	888.84	161+08.01	120.0	4715.21
922	572678.734	287436.861	848.83	157+04.03	54.8	4715.22
923	573996.183	287488.214	833.80	170+46.22	1.2	4715.23
924	574833.327	287411.836	888.81	178+27.21	74.4	4715.24
925	575281.224	287494.809	854.88	184+41.14	2.4	4715.25
926	575979.484	287493.286	886.11	191+78.36	75.1	4715.26
927	576841.278	287473.373	844.02	202+06.48	89.7	4715.27
928	577688.183	287488.029	808.28	208+02.74	78.2	4715.28
929	586038.447	287504.480	848.88			SOUTH OF LITTLE RIVER
937	588150.818	2873672.388	883.21	107+88.90	-39.3	"XOXO"

COORDINATE VALUES ARE NAD83(93)SS1 AND ARE DATUM ADJUSTED BY THE FACTOR 0.000009166 & TIED TO THE TGRN. ELEVATIONS ARE NAVD 988

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF PLANNING & DEVELOPMENT

**PRESENT LAYOUT
&
PROPOSED R.O.W.**

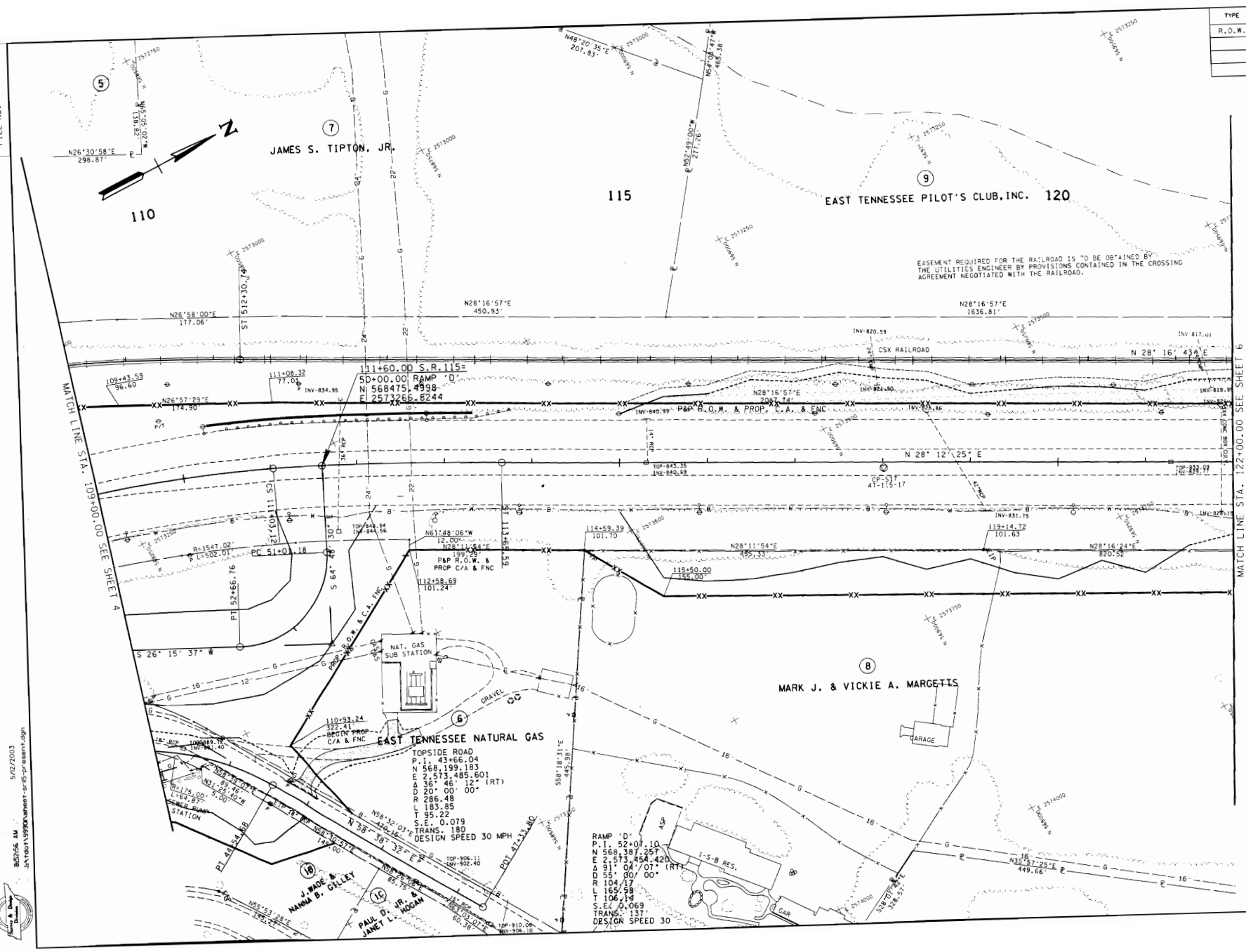
STA. 100+00.00 TO STA. 109+00.00

SCALE: 1"=50'

PRELIMINARY FIELD REVIEW ONLY

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2002	STP-NHE-115(27)	5

DESIGN DIVISION
FILE NO.



FOR PRELIMINARY FIELD REVIEW ONLY

COORDINATE VALUES ARE NAD83(1995) AND ARE DATUM ADJUSTED BY THE FACTOR 1.00000086 & TIED TO THE TORN. ELEVATIONS ARE NAVD 1988

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF PLANNING & DEVELOPMENT

**PRESENT LAYOUT
&
PROPOSED R.O.W.**

STA. 109+00.00 TO STA. 122+00.00

SCALE: 1"=50'

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110

115

120

JAMES S. TIPTON, JR.

EAST TENNESSEE PILOT'S CLUB, INC.

MARK J. & VICKIE A. MARGETIS

EAST TENNESSEE NATURAL GAS

J. WADE & WANDA B. GILLEY
PAUL D. JR. & JANET L. HOGAN

11+60.00 S.R. 115=50+00.00 RAMP 'D'
N 568475.4998
E 257326.8244

PAP R.O.W. & PROP. C/A & FNC
12+58.69
101.24

RAMP 'D'
P.I. 52+07.10
N 568,387.267
E 2,513,454.420
D 91.04/07 (RT)
R 104.17
L 185.98
T 106.14
S.E. 0.069
TRANS. 137
DESIGN SPEED 30

TOPSIDE ROAD
P.I. 43+66.04
N 568,199.183
E 2,513,485.601
D 55.00/00 (RT)
R 20.00/00
L 183.85
T 95.22
S.E. 0.079
TRANS. 180
DESIGN SPEED 30 MPH

EASEMENT REQUIRED FOR THE RAILROAD IS TO BE OBTAINED BY THE UTILITIES ENGINEER BY PROVISIONS CONTAINED IN THE CROSSING AGREEMENT NEGOTIATED WITH THE RAILROAD.

MATCH LINE STA. 109+00.00 SEE SHEET 4

MATCH LINE STA. 122+00.00 SEE SHEET 6

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2002	STP-NHE-115(2)1	7

S.R. 115
 P.I. 144+39.81
 N 571,339.637
 E 2,574,863.868
 Δ 9 47' 08" 41" (LT)
 θ 12' 42" 23"
 Δ C 21' 43" 55" (LT)
 D 4° 45' 06"
 R 1,206.23
 LC 457.52
 TS 191.66
 LS 536.00
 S.E. MAX 0.080
 TRANS. 144
 DESIGN SPEED 60 MPH

ACCESS ROAD
 P.I. 72+89.53
 N 571,288.335
 E 2,574,328.753
 Δ 16' 17' 38" (LT)
 θ 20' 00' 00"
 D 286.48
 L 81.47
 T 41.01
 S.E. N.C.
 TRANS. N/A
 DESIGN SPEED 20 MPH

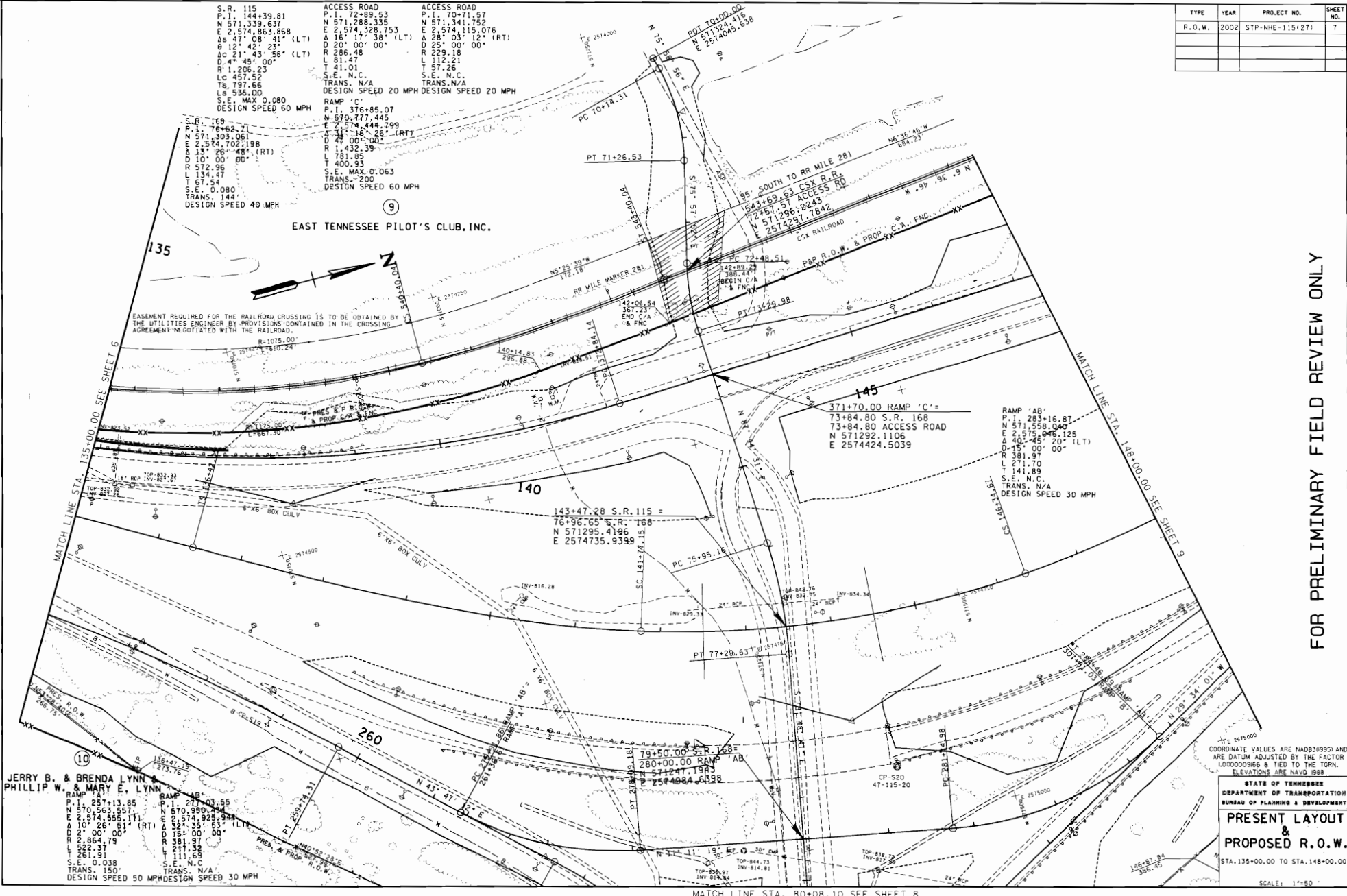
ACCESS ROAD
 P.I. 70+71.57
 N 571,341.752
 E 2,574,115.076
 Δ 28' 03' 12" (RT)
 θ 25' 00' 00"
 D 229.18
 L 112.21
 T 57.26
 S.E. N.C.
 TRANS. N/A
 DESIGN SPEED 20 MPH

RAMP 'C'
 P.I. 376+85.07
 N 570,771.945
 E 2,574,444.799
 Δ 51' 16' 26" (RT)
 D 44' 00' 00"
 R 1,432.39
 L 781.85
 T 400.93
 S.E. MAX 0.063
 TRANS. 200
 DESIGN SPEED 60 MPH

S.B. 168
 P.I. 76+62.71
 N 571,303.061
 E 2,574,702.198
 Δ 15' 26' 48" (RT)
 D 10' 00' 00"
 R 572.96
 L 134.47
 T 67.54
 S.E. 0.080
 TRANS. 144
 DESIGN SPEED 40 MPH

EAST TENNESSEE PILOT'S CLUB, INC.

BASEMENT REQUIRED FOR THE RAILROAD CROSSING IS TO BE OBTAINED BY THE UTILITIES ENGINEER BY PROVISIONS CONTAINED IN THE CROSSING AGREEMENT NEGOTIATED WITH THE RAILROAD.



FOR PRELIMINARY FIELD REVIEW ONLY

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JERRY B. & BRENDA LYNN &
 PHILLIP W. & MARY E. LYNN
 RAMP 'A'
 P.I. 257+13.85
 N 570,563.557
 E 2,574,555.111
 Δ 10' 26' 01" (RT)
 θ 26' 00' 00"
 D 186.479
 L 222.37
 T 111.93
 S.E. 0.038
 TRANS. 150
 DESIGN SPEED 50 MPH

RAMP 'AB'
 P.I. 277+03.55
 N 570,950.794
 E 2,574,925.944
 Δ 32' 35' 53" (LT)
 D 181.97
 R 381.97
 L 211.32
 T 111.63
 S.E. N.C.
 TRANS. N/A
 DESIGN SPEED 30 MPH

COORDINATE VALUES ARE NAD83(1995) AND ARE DATUM ADJUSTED BY THE FACTOR 1.00000864 & TIED TO THE TOWN ELEVATIONS ARE NAVD 1988

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF PLANNING & DEVELOPMENT

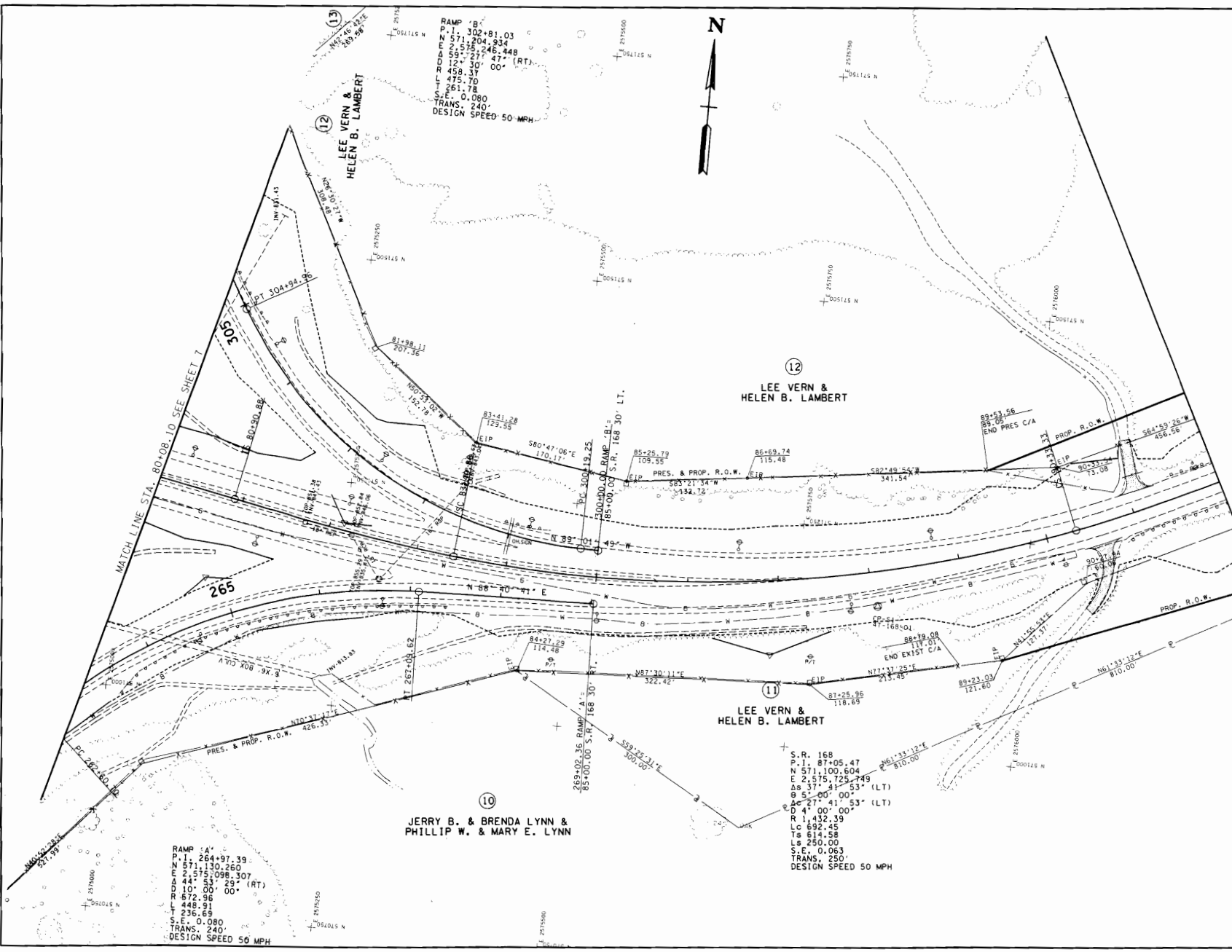
**PRESENT LAYOUT
&
PROPOSED R.O.W.**

STA. 135+00.00 TO STA. 148+00.00

SCALE: 1" = 50'

MATCH LINE STA. 80+08.10 SEE SHEET 8

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2002	STP-NHE-115(27)	8



RAMP "B"
P.I. 302+81.03
N 571.204.934
E 257.246.448
L 127.271.477 (RT)
T 127.30' 00"
S 458.37
W 475.70
T 261.78
S.E. 0.080
TRANS. 240'
DESIGN SPEED 50 MPH

RAMP "A"
P.I. 264+97.39
N 571.130.260
E 447.098.307
L 10' 00' 00" (RT)
T 872.96
W 448.51
T 236.69
S.E. 0.080
TRANS. 240'
DESIGN SPEED 50 MPH

S.R. 168
P.I. 87+05.47
N 571.100.604
E 257.575.749
L 37' 41' 53" (LT)
T 5' 00' 00"
D 4' 00' 00"
R 1,432.39
Lc 632.45
Ts 614.58
Ls 250.00
S.E. 0.063
TRANS. 250'
DESIGN SPEED 50 MPH

COORDINATE VALUES ARE NAD83(995) AND ARE DATUM ADJUSTED BY THE FACTOR 1.000000966 & TIED TO THE TGN. ELEVATIONS ARE NAVD83

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF PLANNING & DEVELOPMENT
**PRESENT LAYOUT
&
PROPOSED R.O.W.**
JOHN SEVIER HWT
SCALE: 1"=50'

FOR PRELIMINARY FIELD REVIEW ONLY

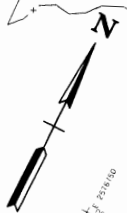


5/17/2003
STP-NHE-115(27) SHEET 8 OF 8

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2002	STP-NHE-115(27)	9

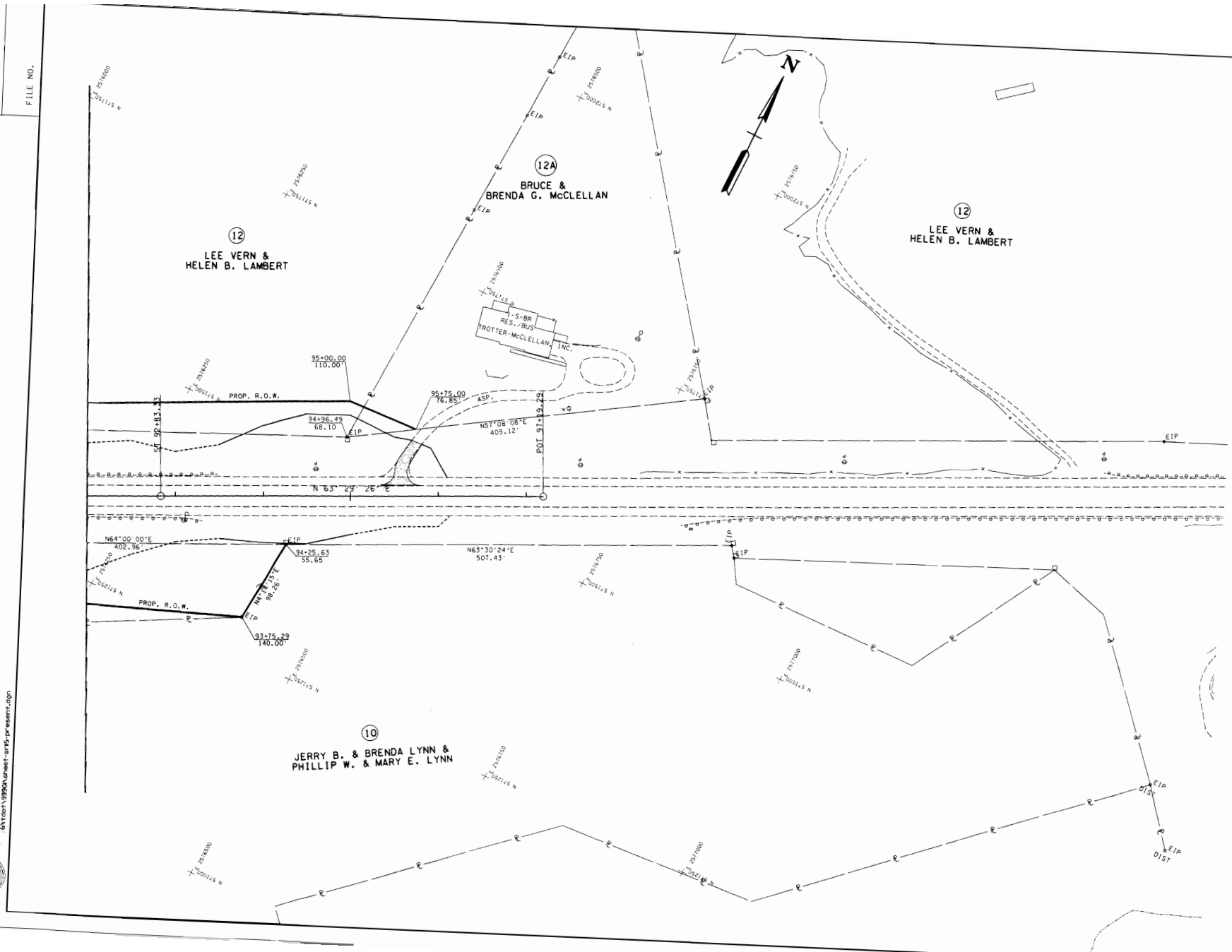
FOR PRELIMINARY FIELD REVIEW ONLY

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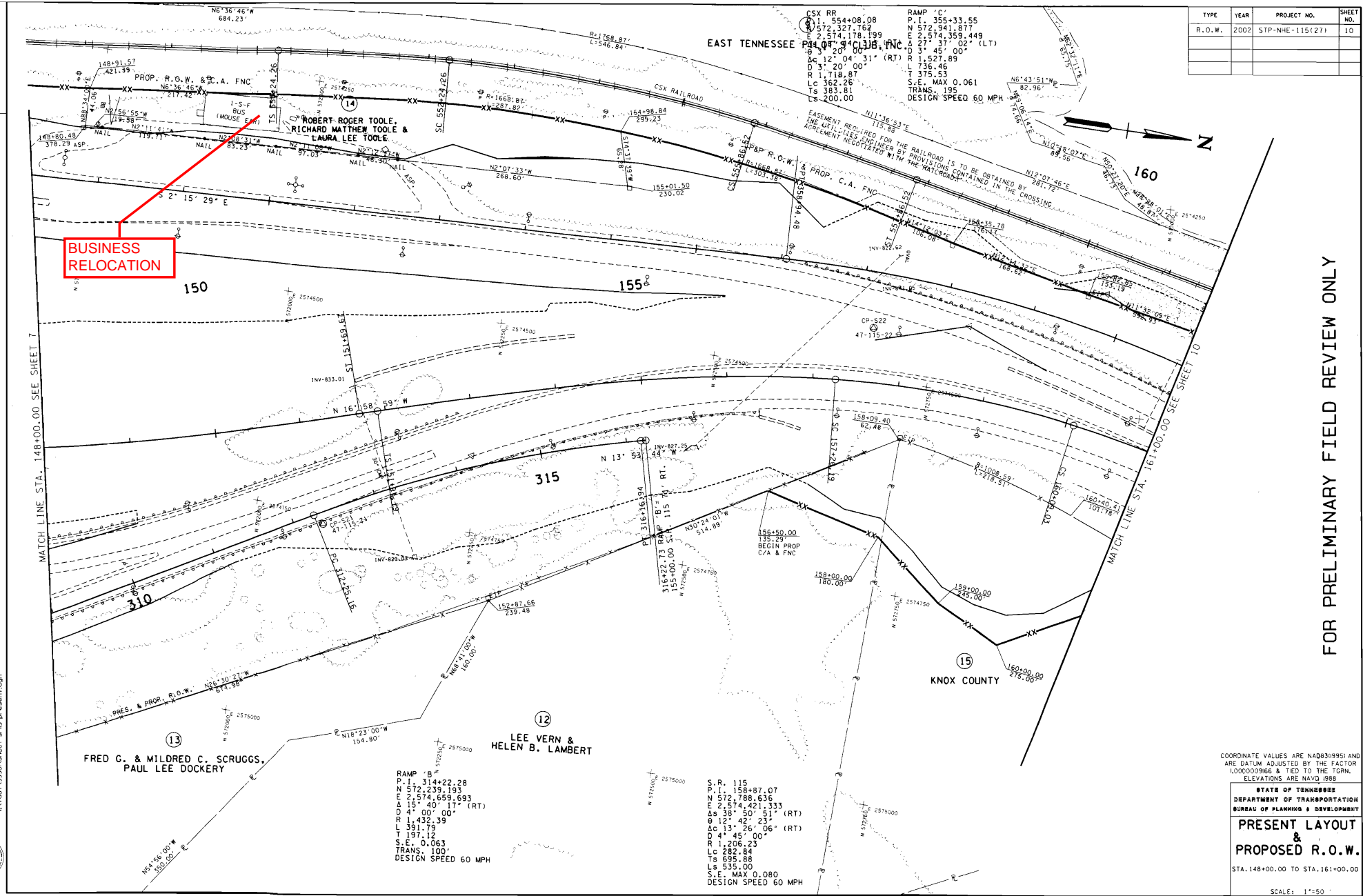
COORDINATE VALUES ARE NAD83(1983) AND
 ARE DATUM ADJUSTED BY THE FACTOR
 1.00000086 & TIED TO THE TGNL.
 ELEVATIONS ARE NAVD 1988.

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF PLANNING & DEVELOPMENT
**PRESENT LAYOUT
 &
 PROPOSED R.O.W.**
 JOHN SEVIER HWY
 SCALE: 1"=50'



TENNESSEE, D.O.T.
 DESIGN DIVISION
 FILE NO.

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2002	STP-NHE-115(127)	10



**BUSINESS
 RELOCATION**

EAST TENNESSEE RAILROAD
 CSX RAILROAD

FRED G. & MILDRED C. SCRUGGS,
 PAUL LEE DOCKERY

RAMP 'B'
 P.I. 314+22.28
 N 57° 23' 19"
 E 2,574.659, 693
 Δ 15° 40' 17" (RT)
 D 47' 00' 00"
 R 1,432.39
 L 391.79
 T 197.12
 S.E. 0.063
 TRANS. 100'
 DESIGN SPEED 60 MPH

LEE VERN &
 HELEN B. LAMBERT

S. R. 115
 P.I. 158+87.07
 N 57° 18' 636
 E 2,574.421, 333
 Δ 38° 50' 51" (RT)
 D 12' 42' 23"
 R 1,262.23
 L 262.84
 T 695.88
 S.E. 0.080
 TRANS. 100'
 DESIGN SPEED 60 MPH

RAMP 'C'
 P.I. 355+33.55
 N 57° 341, 877
 E 2,574.355, 449
 Δ 27° 37' 02" (LT)
 D 3' 45' 00"
 R 1,527.89
 L 736.46
 T 375.53
 S.E. MAX 0.061
 TRANS. 195
 DESIGN SPEED 60 MPH

COORDINATE VALUES ARE NAD83(1983) AND
 ARE DATUM ADJUSTED BY THE FACTOR
 0.00000966 & TIED TO THE TGRN.
 ELEVATIONS ARE NAVD 1988

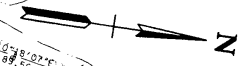
STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF PLANNING & DEVELOPMENT

**PRESENT LAYOUT
 &
 PROPOSED R.O.W.**

STA. 148+00.00 TO STA. 161+00.00

SCALE: 1"=50'

FOR PRELIMINARY FIELD REVIEW ONLY



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MATCH LINE STA. 148+00.00 SEE SHEET 7

MATCH LINE STA. 161+00.00 SEE SHEET 10

TENNESSEE D.O.T.
DESIGN DIVISION
FILE NO.

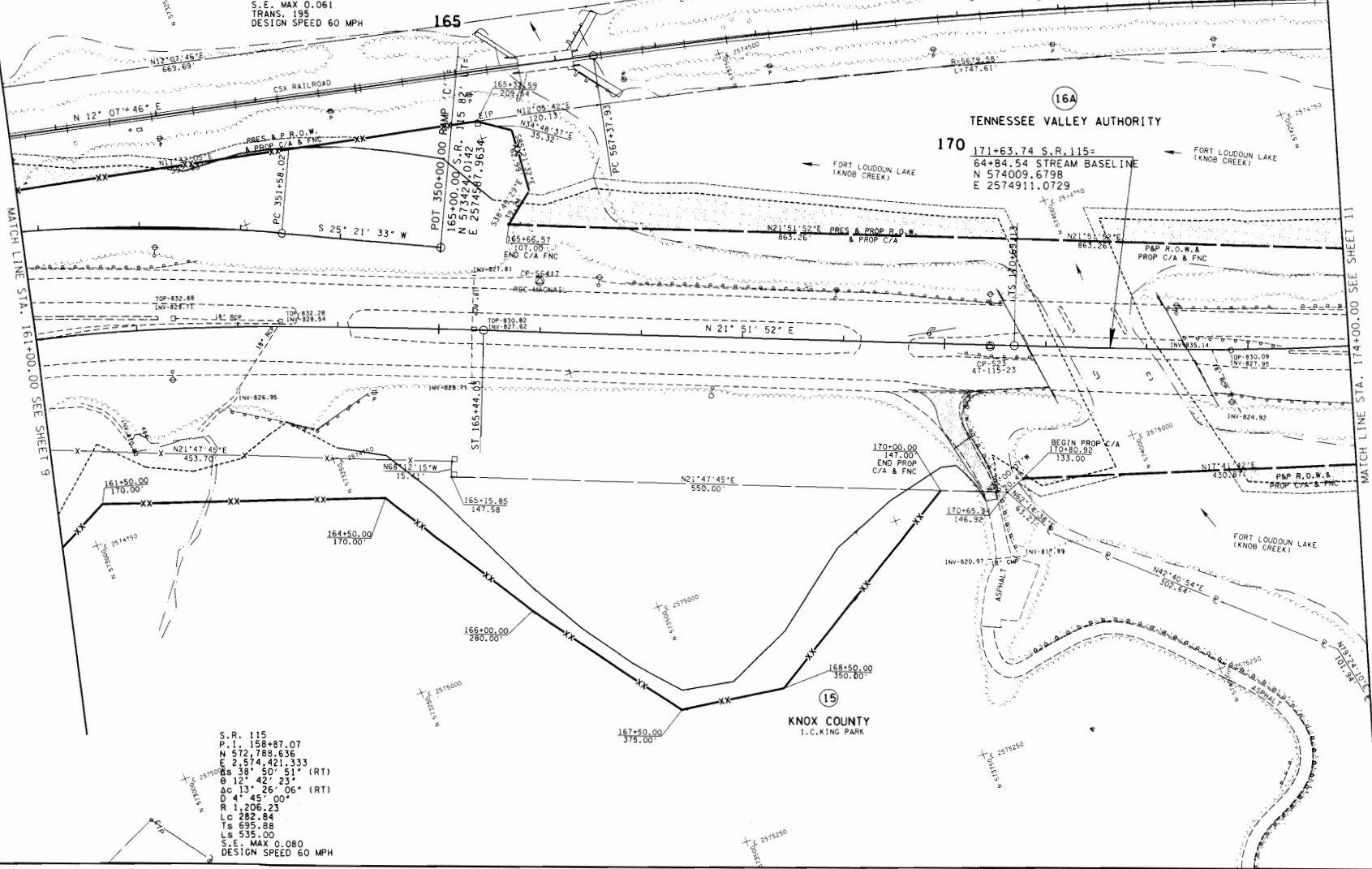
EASEMENT REQUIRED FOR THE RAILROAD CROSSING IS TO BE OBTAINED BY THE UTILITIES ENGINEER BY PROVISIONS CONTAINED IN THE CROSSING AGREEMENT NEGOTIATED WITH THE RAILROAD.



RAMP 'C'
P.I. 355+33.55
N 572.341.877
E 2574.359.449
D 27° 37' 02" (LT)
O 4' 45" 00"
R 1,527.89
L 736.46
T 375.53
S.E. MAX 0.061
TRANS. 195
DESIGN SPEED 60 MPH

S.R. 115
P.I. 177+74.33
N 574.576.260
E 2,575.138.664
D 32° 37' 35" (LT)
O 12' 42' 23"
Ac 14' 12' 50" (LT)
O 4' 45" 00"
R 1,206.23
L 299.24
T 705.20
L 535.00
S.E. MAX 0.080
DESIGN SPEED 60 MPH

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2002	STP-NHE-115(27)	11



S.R. 115
P.I. 158+87.07
N 572.788.636
E 2,574.421.333
D 38° 50' 51" (RT)
O 12' 42' 23"
Ac 13' 26' 06" (RT)
O 4' 45" 00"
R 1,206.23
L 282.84
T 695.86
L 535.00
S.E. MAX 0.080
DESIGN SPEED 60 MPH

COORDINATE VALUES ARE NAD83(995) AND ARE DATUM ADJUSTED BY THE FACTOR 0.0000085 & TIED TO THE TORN. ELEVATIONS ARE NAVD 1988

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF PLANNING & DEVELOPMENT

**PRESENT LAYOUT
&
PROPOSED R.O.W.**

STA. 161+00.00 TO STA. 174+00.00

SCALE: 1"=50'

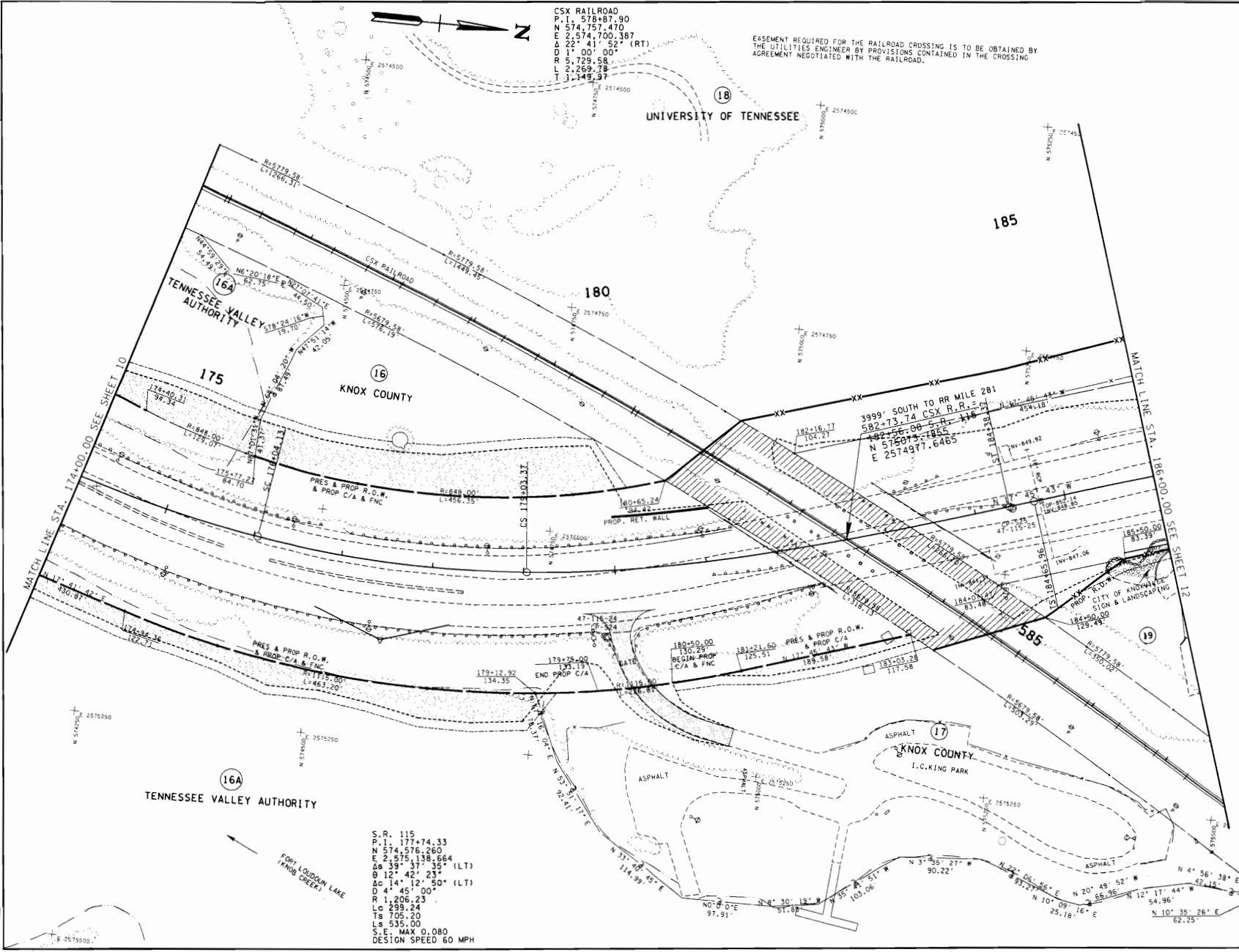
FOR PRELIMINARY FIELD REVIEW ONLY

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TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2002	STP-NHE-115(27)	12

CSX RAILROAD
 P.I. 578+87.90
 N 574,757.470
 E 2,574,700.387
 Δ 22° 41' 52" (RT)
 D 1' 00" 00"
 R 2,268.78
 L 149.37

EASEMENT REQUIRED FOR THE RAILROAD CROSSING IS TO BE OBTAINED BY THE UTILITIES ENGINEER BY PROVISIONS CONTAINED IN THE CROSSING AGREEMENT NEGOTIATED WITH THE RAILROAD.



FOR PRELIMINARY FIELD REVIEW ONLY

S.R. 115
 P.I. 177+74.33
 N 574,576.260
 E 2,575,138.664
 Δ 8° 39' 37" 35" (LT)
 B 12' 42' 23"
 Δ C 14' 12' 50" (LT)
 D 4' 45" 00"
 R 1,206.23
 L 299.24
 T_S 705.20
 L_S 535.00
 S.E. MAX 0.080
 DESIGN SPEED 60 MPH

COORDINATE VALUES ARE NAD83(99S) AND ARE DATUM ADJUSTED BY THE FACTOR 1.00000966 & TIED TO THE TGN. ELEVATIONS ARE NAVD 1988

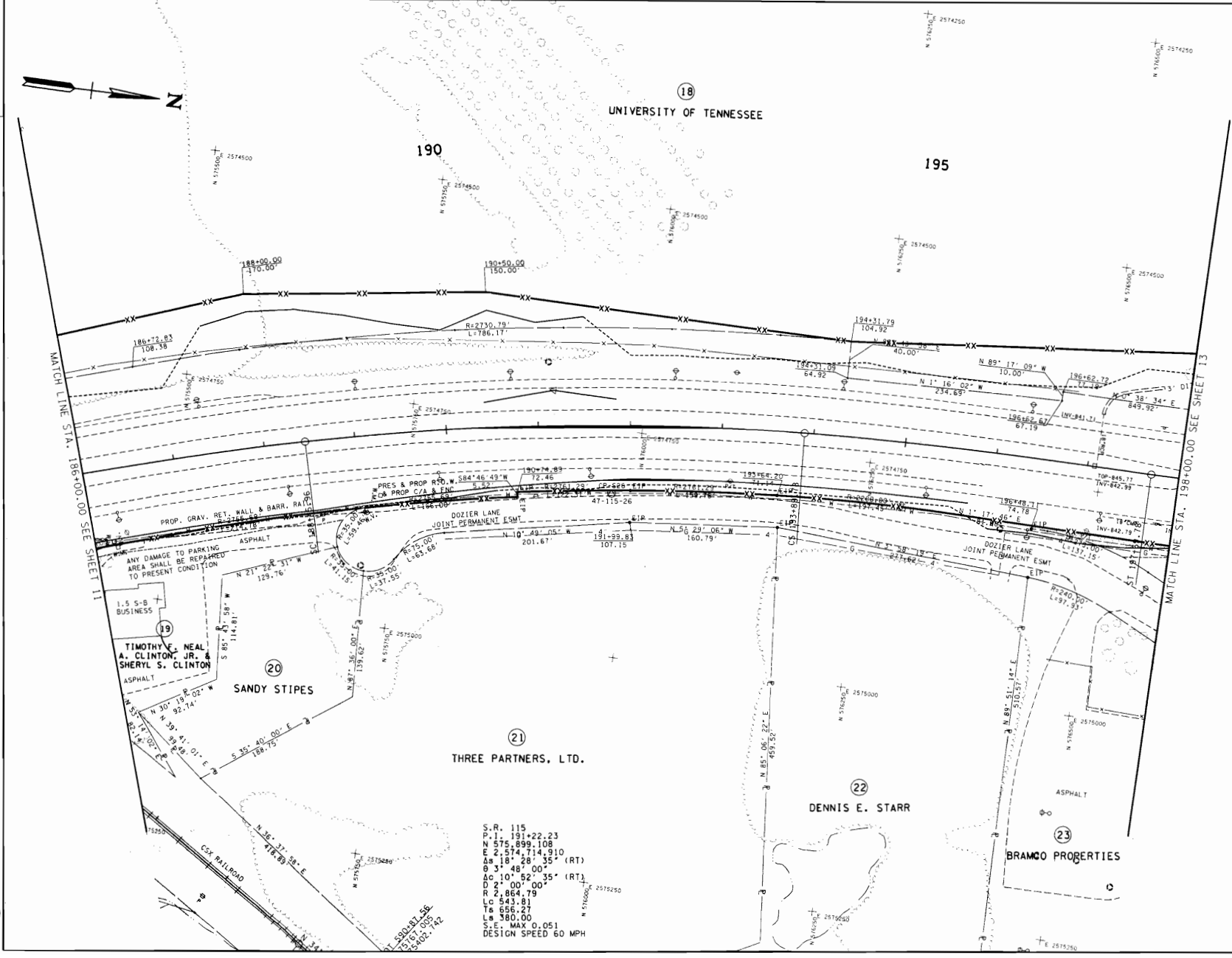
STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF PLANNING & DEVELOPMENT

**PRESENT LAYOUT
 &
 PROPOSED R.O.W.**

STA. 174+00.00 TO STA. 186+00.00

SCALE: 1" = 50'

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2002	STP-NHE-115(27)	13



COORDINATE VALUES ARE NAD83/9951 AND ARE DATUM ADJUSTED BY THE FACTOR 1.00000966 & TIED TO THE TGRN. ELEVATIONS ARE NAVD 1988

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF PLANNING & DEVELOPMENT

**PRESENT LAYOUT
&
PROPOSED R.O.W.**

STA. 186+00.00 TO STA. 198+00.00

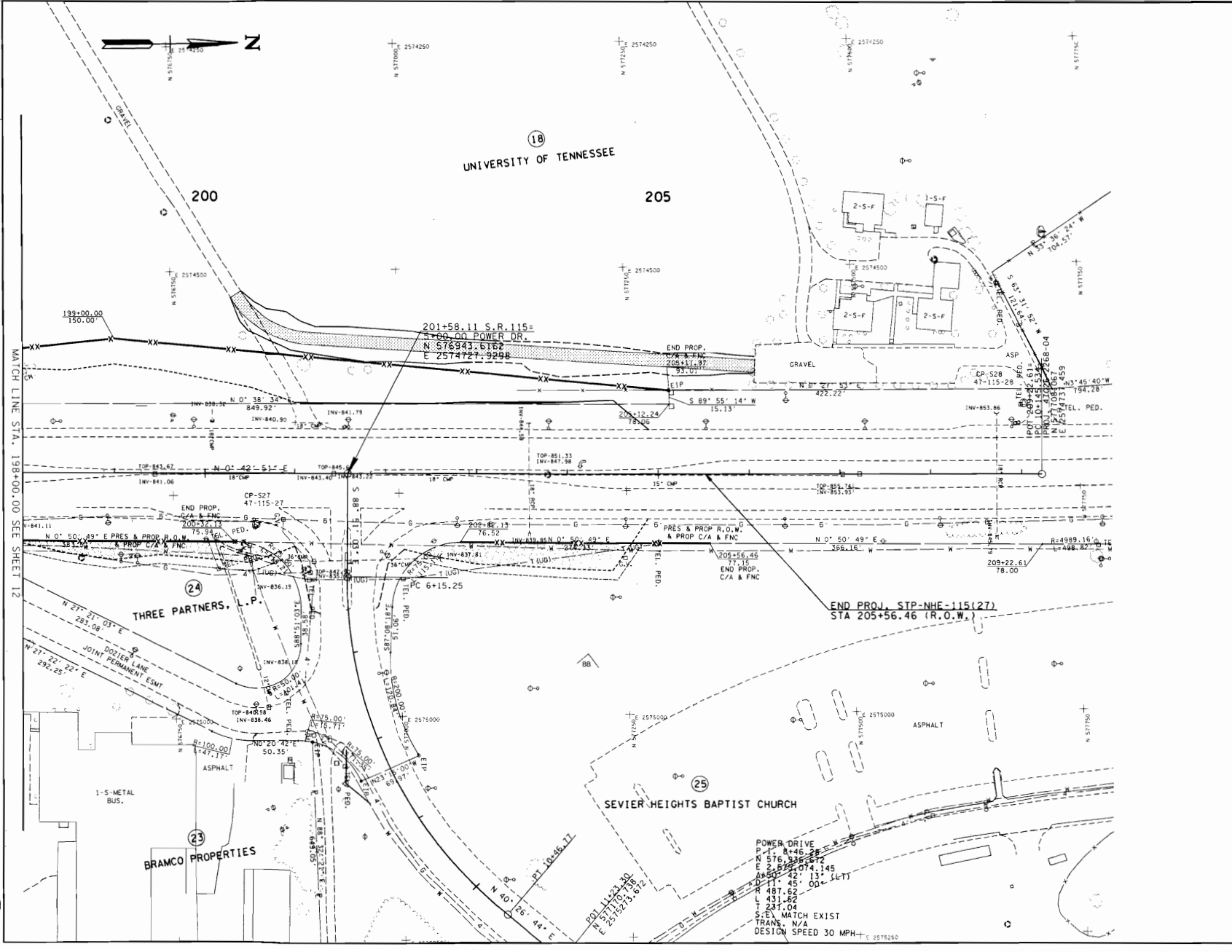
SCALE: 1" = 50'

S.R. 115
P.I. 191+22.23
N 575.899.108
E 2,574,714.910
Δ 18° 28' 35" (RT)
θ 31' 48" 00"
Δ 0 10' 52' 35" (RT)
D 2' 00" 00"
R 2,864.79
L 0 543.81
T 8 656.27
L 8 580.00
S.E. MAX 0.051
DESIGN SPEED 60 MPH

FOR PRELIMINARY FIELD REVIEW ONLY



TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2002	STP-NHE-115(27)	14



FOR PRELIMINARY FIELD REVIEW ONLY

COORDINATE VALUES ARE NAD83(1995) AND ARE DATUM ADJUSTED BY THE FACTOR 1.00000066 & TIED TO THE TGN. ELEVATIONS ARE NAVD 1988

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF PLANNING & DEVELOPMENT

**PRESENT LAYOUT
&
PROPOSED R.O.W.**

STA. 198+00.00 TO STA. 204+75.00

SCALE: 1"=50'

5/12/2003
8:03:06 AM
I:\vtd\1999\shelton-ar\rip\present.dgn

100241.00

Index of Sheets (R.O.W.)

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2, 2A-2E	TYPICAL SECTIONS
3, 3A-3F	PROPERTY MAP
3G	TRAFFIC MOVEMENTS
4-16	PRESENT LAYOUTS
4A-16A	PROPOSED LAYOUTS
4B-16B	PROPOSED PROFILES
4C-16C	PUBLIC SIDE ROAD PROFILES
5D, 8D-11D	PUBLIC SIDE ROAD PROFILES
8E, 9E, 11E, 8F, 11F	PUBLIC SIDE ROAD PROFILES
17-25	PRIVATE DRIVEWAYS AND FIELD RAMP PROFILES
23, 24	DRAINAGE MAPS
25-42, 42A-42C	CULVERT SECTIONS
43-45	BOX CULVERT SECTIONS
46-174	CROSS SECTIONS

STATE OF TENNESSEE DEPARTMENT OF TRANSPORTATION BUREAU OF PLANNING AND DEVELOPMENT

KNOX COUNTY

S.R. 115 (U.S. 129, ALCOA HIGHWAY)
FROM 400m (1,312') SOUTH OF MALONEY ROAD
TO WOODSON DRIVE

R.O.W.

STATE HIGHWAY NO. 115 F.A.H.S. NO. 115

TENN.	YEAR	SHEET NO.
	2000	1
FED. AID PROJ. NO.		
STATE PROJ. NO.	47026-2268-04	

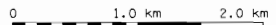
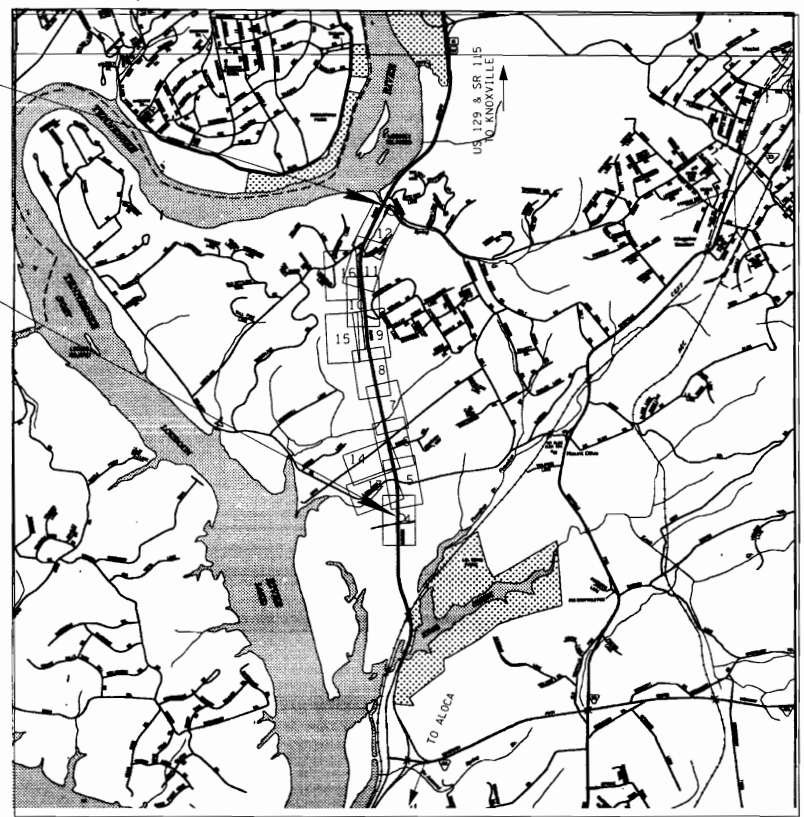
REV. 04-13-2001: REVISED INDEX SHEET NUMBERS
REV. 28-FEB-2003: REVISED INDEX OF SHEETS.



KNOX COUNTY
R.O.W. 47026-2268-04

END R.O.W PROJECT
47026-2268-04
STA. 12+690.338

BEGIN R.O.W PROJECT
47026-2268-04
STA. 10+000.000



1:20,000



NO EQUATIONS
NO EXCLUSIONS



APPROVED: *JHC Jones* DESIGN DIVISION

DATE:

APPROVED: *Bruce S. Little* COMMISSIONER

DATE OF ORIGINAL SURVEY FALL 96
DATE OF SURVEY UPDATE MAY 99

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED MARCH 1, 1995 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT

TDOT TRANSPORTATION MANAGER CLIFFORD F. STEWART

DESIGNED BY VAUGHN & MELTON

DESIGNER STEPHEN D. ROBBINS

P.E. NO. 47026-1263-04

PIN 100241.04

RIGHT-OF-WAY PROJECT LENGTH 2.690/1.672 KILOMETERS/MILES

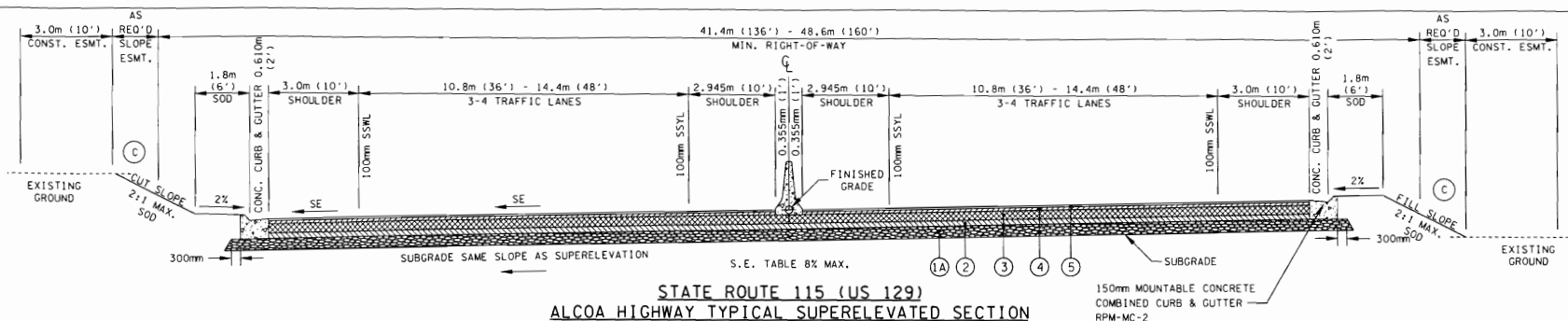
TRAFFIC DATA	
ADT (2000)	57,000
ADT (2020)	79,870
DHV (2020)	7,990
D	70 - 30
T (ADT)	5%
T (DHV)	3%
V	100 km/h (60mph)

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: _____
DIVISION ADMINISTRATOR DATE

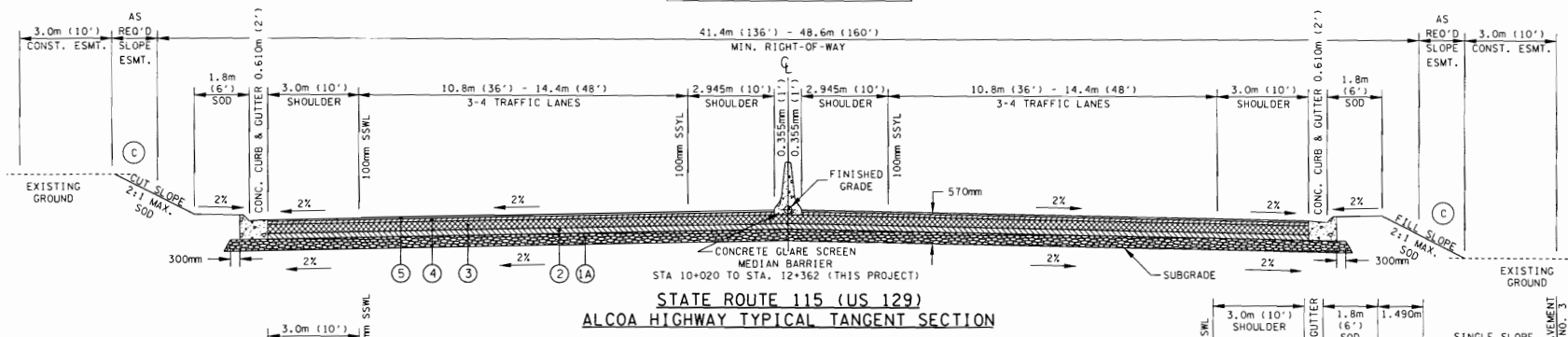


DESIGN DIVISION
FILE NO.



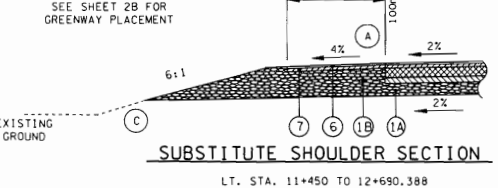
**STATE ROUTE 115 (US 129)
ALCOA HIGHWAY TYPICAL SUPERELEVATED SECTION**

STA. 10+094.534 TO 10+528.032
STA. 11+311.486 TO 11+615.963
STA. 11+917.995 TO 12+427.467



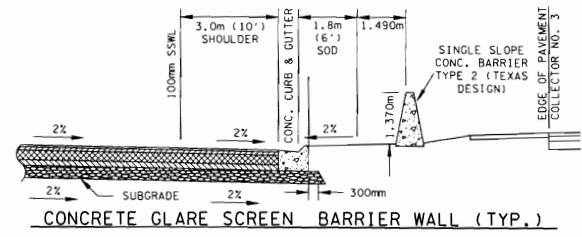
**STATE ROUTE 115 (US 129)
ALCOA HIGHWAY TYPICAL TANGENT SECTION**

STA. 10+000.000 TO 10+094.534
STA. 10+528.032 TO 11+311.486
STA. 11+615.963 TO 11+917.995
STA. 12+427.467 TO 12+690.388



SUBSTITUTE SHOULDER SECTION

LT. STA. 11+450 TO 12+690.388



CONCRETE GLARE SCREEN BARRIER WALL (TYP.)

STA. 12+340 TO STA. 12+660 LT.

PAVING SCHEDULE SR115

<p>① AGGREGATE BASE MATERIAL (0.6025 Tonnes/m²) (250mm DEPTH) Item 303M01 Mineral Aggregate, Type "A" Base, Grading "D".</p>	<p>④ GRADING "B-M2" MIXTURE (BINDER) (0.1200 Tonnes/m²) (50mm DEPTH) Item 307M08.06 Performance Grade (PG 76-22): (BPMB-HM) Grade B-M2</p>	<p>SHOULDER</p>		<p>Ⓐ THE SLOPE OF THE SHOULDER AND ROADWAY PAVEMENT SHALL NOT EXCEED AN ALGEBRAIC DIFFERENCE OF 7%.</p>
<p>② GRADING "A-S" MIXTURE (BLACK BASE) (0.1728 Tonnes/m²) (90mm DEPTH) 3.25% Item 307M08.02 Performance Grade Asphalt Cement (PG 76-22) : (BPMB-HM) Grade A-S 96.75% Item 307M01.03 Aggregate (BPMB-HM) Grading A-S Mix</p>	<p>⑤ GRADING "D" SURFACE (0.0684 Tonnes/m²) (30mm DEPTH) 6% Item 411M08.01 Performance Grade Asphalt Cement (PG 76-22) Grade "D" 94% Item 411M01.01 Mineral Aggregate (BPMS-DM) Includes Choker</p>	<p>Ⓑ AGGREGATE BASE MATERIAL (1.1809 Tonnes/m²) (490mm DEPTH) Item 303M01 Mineral Aggregate, Type "A" Base, Grading "D".</p>	<p>Ⓑ SHOULDER SLOPE ON LOW SIDE EQUAL TO S.E. WHEN S.E. IS GREATER THAN NORMAL SHOULDER SLOPE.</p>	
<p>③ GRADING "A" MIXTURE (BLACK BASE) (0.3660 Tonnes/m²) (150mm DEPTH) Item 307M08.01 Performance Grade (PG 76-22): (BPMB-HM) Grade "A"</p>	<p>Ⓣ TACK COAT (0.0009336 Tonnes/m²) Item 403M01 Bituminous Material for Tack Coat (TC)</p>	<p>⑥ SURFACE GRADING "E" (STAB. SHOULDER) (0.0660 Tonnes/m²) (30mm DEPTH) Item 411M03.10 Asphaltic Concrete Surfaces (Hot Mix) Grading "E" (Shoulders)</p>	<p>Ⓒ SEE STD. DRAWING RDM-S-11 FOR FILL AND CUT SLOPE TABLE (CASE 1) UNLESS NOTED OTHERWISE ON CROSS SECTIONS</p>	
	<p>Ⓟ PRIME COAT Item 402M01 Bituminous Material for Prime Coat (PC) @ 1.47 L/m Item 402M02 Aggregate for Cover Material (PC) @ 5.4 kg/m</p>	<p>⑦ BITUMINOUS BINDER GRADING "B-M2D2" (0.1200 Tonnes/m²) (50mm DEPTH) Item 307M03.16 Bituminous Plant Mix Base (Hot Mix) Grading "B-M2".</p>	<p>Ⓓ SEE STD. DRAWING RDM-S-11A FOR ROUNDING OF ROADSIDE DITCH SLOPE</p>	

TYPE	YEAR	PROJECTS NO.	SHEET NO.
R.O.W.	2000	47026-2268-04	2

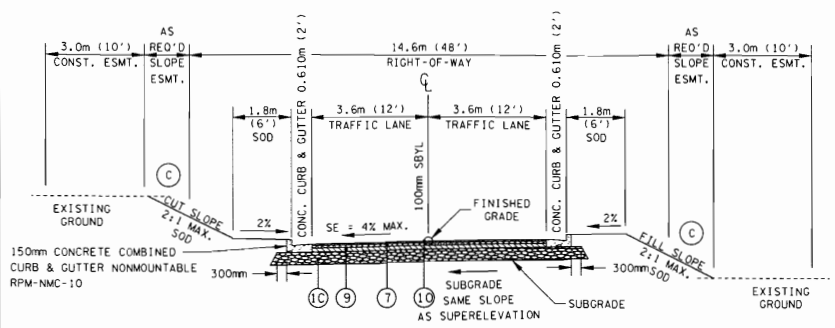
REV. 28-FEB-2003: REVISED STATION LOCATIONS FOR HORIZONTAL CURVES. REVISED GREENWAY LOCATION.



NOTE: ENGLISH UNITS ARE FOR REFERENCE ONLY.

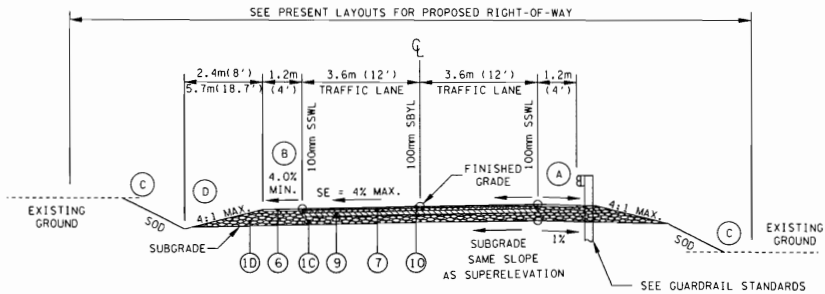
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS



TYPICAL SUPERELEVATED SECTION
(WITH CURB & GUTTER)

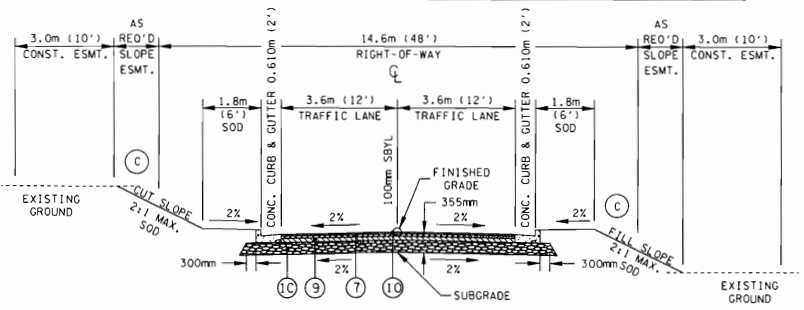
ROAD	WITH CURB & GUTTER	WITHOUT CURB & GUTTER	CURB ONLY ON LEFT
COLLECTOR NO. 1			STA. 1+512.857 TO 1+697.360 STA. 1+724.113 TO 1+764.829
SOUTH CIRCLE	STA. 3+020.700 TO 3+150.000		
DRESSER RD.	STA. 2+000.000 TO 2+050.000	STA. 2+157.000 TO 2+159.936	STA. 2+050.000 TO 2+157.000
GINN RD. RT.	STA. 2+800.000 TO 2+835.378 STA. 2+840.259 TO 2+909.054		
GINN RD. LT.	STA. 2+605.882 TO 2+680.000		
BARBER HILL RD.	STA. 7+500.000 TO 7+597.301		



TYPICAL SUPERELEVATED SECTION
(WITHOUT CURB & GUTTER)

ROAD	WITH CURB & GUTTER	WITHOUT CURB & GUTTER
MALONEY RD.		STA. 1+040.000 TO 1+086.043 STA. 1+095.013 TO 1+256.301 STA. 1+326.358 TO 1+448.573

SEE SHEET 2B FOR PAVING SCHEDULE
SEE SHEET 2D FOR OVERLAY PAVING SCHEDULE

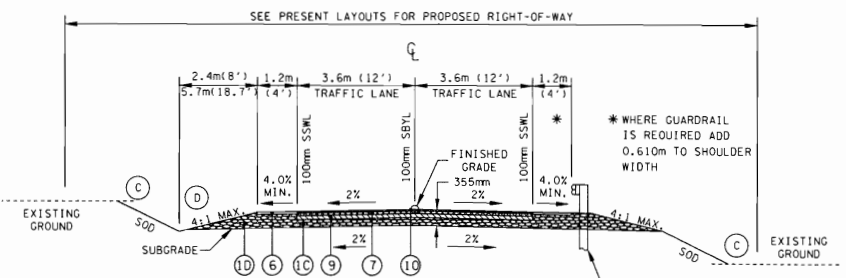


TYPICAL TANGENT SECTION
(WITH CURB & GUTTER)

ROAD	WITH CURB & GUTTER	WITHOUT CURB & GUTTER	CURB ONLY ON LEFT
COLLECTOR NO. 1			STA. 1+697.360 TO 1+724.113
DRESSER RD.	STA. 2+200.000 TO 2+368.000	STA. 2+154.936 TO 2+200.000	
GINN RD. RT.	STA. 2+835.378 TO 2+840.259 STA. 2+909.054 TO 2+919.000		
GINN RD. LT.	STA. 2+540.000 TO 2+605.882		

* TRAFFIC LANES STA. 2+200.000 TO 2+368.000 ARE 4.0m(13')

- (A) THE SLOPE OF THE SHOULDER AND ROADWAY PAVEMENT SHALL NOT EXCEED AN ALGEBRAIC DIFFERENCE OF 7%.
- (B) SHOULDER SLOPE ON LOW SIDE EQUAL TO S.E. WHEN S.E. IS GREATER THAN NORMAL SHOULDER SLOPE.
- (C) SEE STD. DRAWING RDM-S-11 FOR FILL AND CUT SLOPE TABLE (CASE 1) UNLESS NOTED OTHERWISE ON CROSS SECTIONS
- (D) SEE STD. DRAWING RDM-S-11A FOR ROUNDING OF ROADSIDE DITCH SLOPE



TYPICAL TANGENT SECTION
(WITHOUT CURB & GUTTER)

ROAD	WITH CURB & GUTTER	WITHOUT CURB & GUTTER
MT. VERNON RD.		STA. 9+000.000 TO 9+090.000
BUNKER HILL		STA. 9+300.000 TO 9+351.000
MALONEY RD.		STA. 1+086.043 TO 1+095.013 STA. 1+256.301 TO 1+326.358 STA. 1+448.573 TO 1+503.000

* WHERE GUARDRAIL IS REQUIRED ADD 0.610m TO SHOULDER WIDTH

SEE GUARDRAIL STANDARDS FOR TYPICAL PLACEMENT OF GUARDRAIL

TYPE	YEAR	PROJECTS NO.	SHEET NO.
R.O.W.	2000	47026-2268-04	2A

REV. 28-FEB-2003: ADDED STATION LOCATION FOR BUNKER HILL. REVISED STA. LOCATION FOR SOUTH CIRCLE, DRESSER RD., MALONEY RD., BUNKER HILL, MT. VERNON RD. & GINN RD. RT.



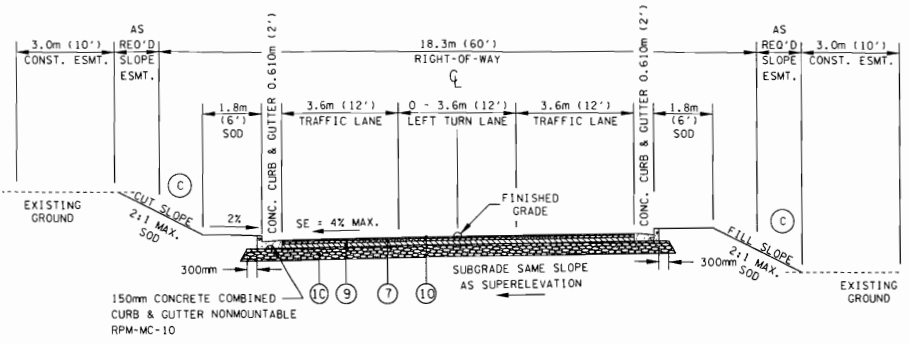
NOTE: ENGLISH UNITS ARE FOR REFERENCE ONLY.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

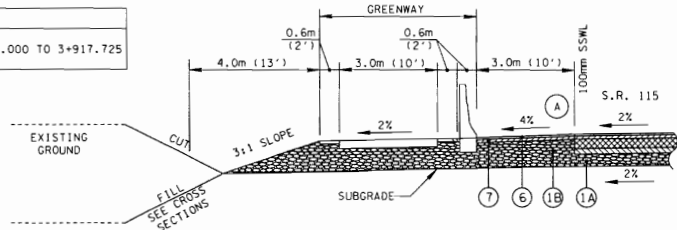
TYPE	YEAR	PROJECTS NO.	SHEET NO.
R.O.W.	2000	47026-2268-04	2B

REV. 04-13-2001: REVISED STATION TABLES
REV. 28-FEB-2003: REVISED STATION LOCATION FOR COLLECTOR NO. 2
REMOVED TYPICAL 3 LANE SECTIONS WITHOUT CURB & GUTTER. ADDED DETAIL FOR GREENWAY.

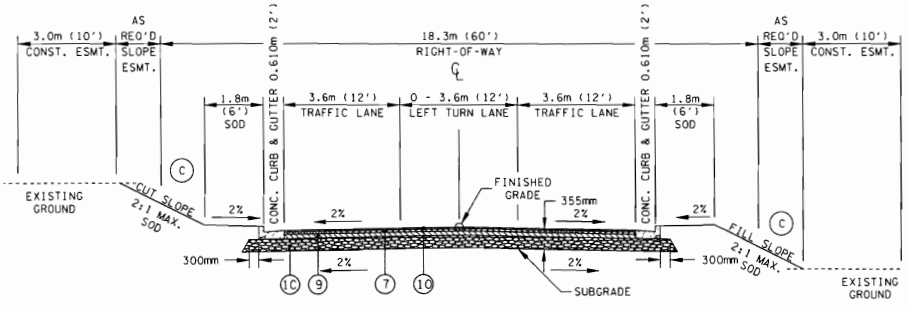


**TYPICAL SUPERELEVATED 3 LANE SECTION
COLLECTOR NO. 2 (WITH CURB & GUTTER)**

ROAD	STATIONING
COLLECTOR NO. 2	STA. 3+867.000 TO 3+917.725



RETAINING WALL & GREENWAY SECTION
STA. 12+360 TO 12+690.388



**TYPICAL TANGENT 3 LANE SECTION
COLLECTOR # 2 (WITH CURB & GUTTER)**

ROAD	STATIONING
COLLECTOR NO. 2	STA. 3+830 TO 3+867.000

PAVING SCHEDULE INTERSECTING ROADS	
<p>(1C) AGGREGATE BASE MATERIAL (0.4820 Tonnes/m²) (200mm DEPTH) Item 303M01 Mineral Aggregate, Type "A" Base, Grading "D".</p>	<p>(T) TACK COAT (0.00009336 Tonnes/m²) Item 403M01 Bituminous Material for Tack Coat (TC)</p>
<p>(9) BLACK BASE GRADING "A" (0.1830 Tonnes/m²) (75mm DEPTH) Item 307M03.09 Bituminous Plant Mix Base (Hot Mix) Grading "A" Mixture Black Base</p>	<p>(P) PRIME COAT (0.001525 Tonnes/m²) Item 402M01 Bituminous Material for Prime Coat (PC) @ 1.47 L/m² Item 402M02 Aggregate for Cover Material (PC) @ 5.4 Kg/m² Aggregate (0.9054 Tonnes/m²)</p>
<p>(7) BITUMINOUS BINDER GRADING "B-MOD2" (0.1200 Tonnes/m²) (50mm DEPTH) Item 307M03.16 Bituminous Plant Mix Base (Hot Mix) Grading "B-M2".</p>	<p>SHOULDERS</p>
<p>(1D) GRADING "D" SURFACE (0.0684 Tonnes/m²) (30mm DEPTH) 94% Item 411M01.01 Mineral Aggregate (ACS) Grading D 6% Item 411M01.02 Asphalt Cement (ACS) Grading D</p>	<p>(1D) AGGREGATE BASE MATERIAL (0.7590 Tonnes/m²) (315mm DEPTH) Item 303M01 Mineral Aggregate, Type "A" Base, Grading "D".</p>
	<p>(6) SURFACE GRADING "E" (STAB. SHOULDER) (0.0880 Tonnes/m²) (40mm DEPTH) Item 411M03.10 Asphaltic Concrete Surfaces (Hot Mix) Grading "E" (Shoulders)</p>

- (A) THE SLOPE OF THE SHOULDER AND ROADWAY PAVEMENT SHALL NOT EXCEED AN ALGEBRAIC DIFFERENCE OF 7%.
- (B) SHOULDER SLOPE ON LOW SIDE EQUAL TO S.E. WHEN S.E. IS GREATER THAN NORMAL SHOULDER SLOPE.
- (C) SEE STD. DRAWING RDM-S-11 FOR FILL AND CUT SLOPE TABLE (CASE 1) UNLESS NOTED OTHERWISE ON CROSS SECTIONS
- (D) SEE STD. DRAWING RDM-S-11A FOR ROUNDING OF ROADSIDE DITCH SLOPE



NOTE: ENGLISH UNITS ARE FOR REFERENCE ONLY.

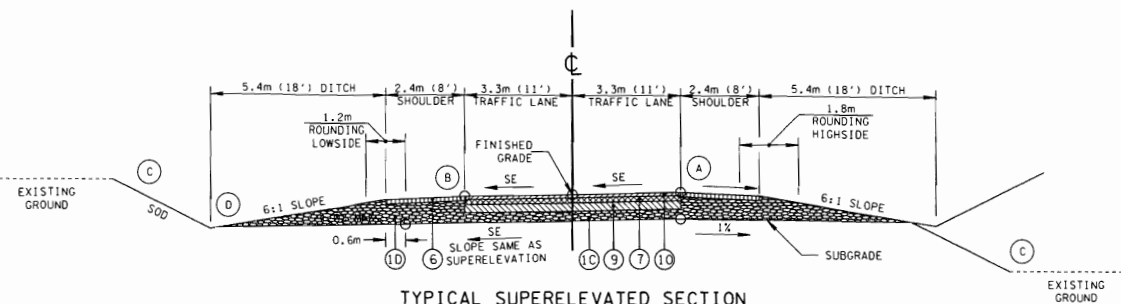
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2000	47026-2268-04	2C

REV. 28-FEB-2003; ADDED TYPICALS FOR GREENWAY & MOVED DRIVEWAY DETAIL TO SHEET 2E. REVISED STA. LOCATION FOR MONTLAKE DR. & COLLECTOR NO. 3. ADDED STA. LOCATION FOR COLLECTOR NO. 5, NO. 6 & MONTLAKE COURT.

- (A) THE SLOPE OF THE SHOULDER AND ROADWAY PAVEMENT SHALL NOT EXCEED AN ALGEBRAIC DIFFERENCE OF 7%.
- (B) SHOULDER SLOPE ON LOW SIDE EQUAL TO S.E. WHEN S.E. IS GREATER THAN NORMAL SHOULDER SLOPE.
- (C) SEE STD. DRAWING RDM-S-11 FOR FILL AND CUT SLOPE TABLE (CASE 1) UNLESS NOTED OTHERWISE ON CROSS SECTIONS
- (D) SEE STD. DRAWING RDM-S-11A FOR ROUNDING OF ROADSIDE DITCH SLOPE

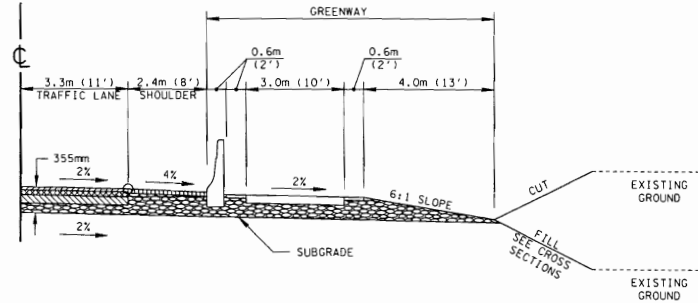


TYPICAL SUPERELEVATED SECTION
COLLECTOR NO. 3, NO. 5, NO. 6 & MONTLAKE DR.

(BASED ON STD. DWG. RDM-TS-2)

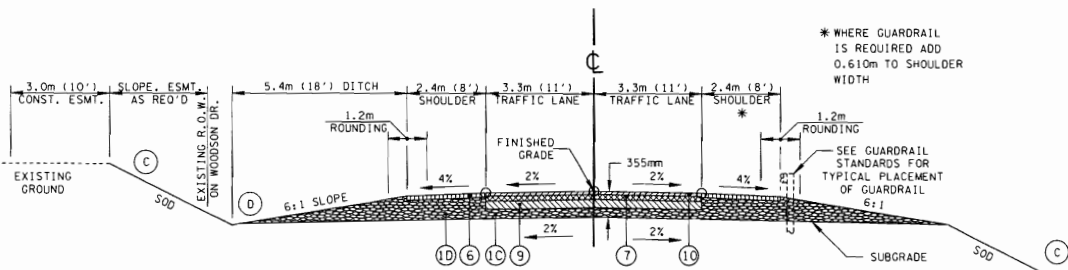
ROAD	WITHOUT CURB & GUTTER
MONTLAKE DR.	STA. 8+043.000 TO 8+128.826
COLLECTOR NO. 3	STA. 112+086.330 TO 112+383.474
COLLECTOR NO. 5	STA. 5+170.000 TO 5+181.854 STA. 5+186.593 TO 5+313.010 STA. 5+352.916 TO 5+399.092
COLLECTOR NO. 6	STA. 6+309.969 TO STA. 6+389.458
MONTLAKE CT.	STA. 9+500.000 TO STA. 9+550.420

SEE SHEET 2B FOR PAVING SCHEDULE



GREENWAY COLLECTOR NO. 6

STA. 6+000.000 TO STA. 6+393.231



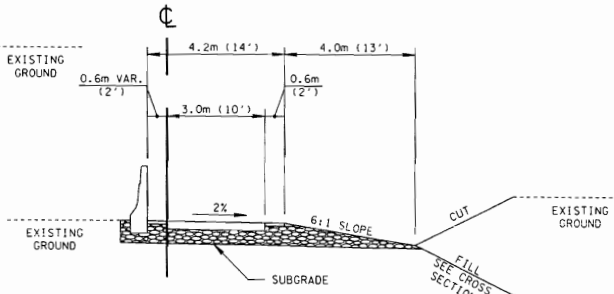
TYPICAL TANGENT SECTION
COLLECTOR NO. 3, NO. 5, NO. 6 & MONTLAKE DR.

(BASED ON STD. DWG. RDM-TS-2)

ROAD	WITHOUT CURB & GUTTER
MONTLAKE DR.	STA. 8+000.000 TO 8+043.000 STA. 8+128.826 TO 8+179.986
COLLECTOR NO. 3	STA. 112+004.931 TO 112+086.330 STA. 112+383.474 TO 112+640.000
COLLECTOR NO. 5	STA. 5+181.854 TO 5+186.593 STA. 5+313.010 TO 5+352.916 SEE ALSO SHEET 2E
COLLECTOR NO. 6	STA. 6+000.000 TO 6+309.969
MONTLAKE CT.	STA. 9+500.420 TO STA. 9+620.000

* WHERE GUARDRAIL IS REQUIRED ADD 0.610m TO SHOULDER WIDTH

SEE GUARDRAIL STANDARDS FOR TYPICAL PLACEMENT OF GUARDRAIL



TYPICAL GREENWAY

STA. 6+663.000 TO STA. 7+020.000
SEE RAMP B



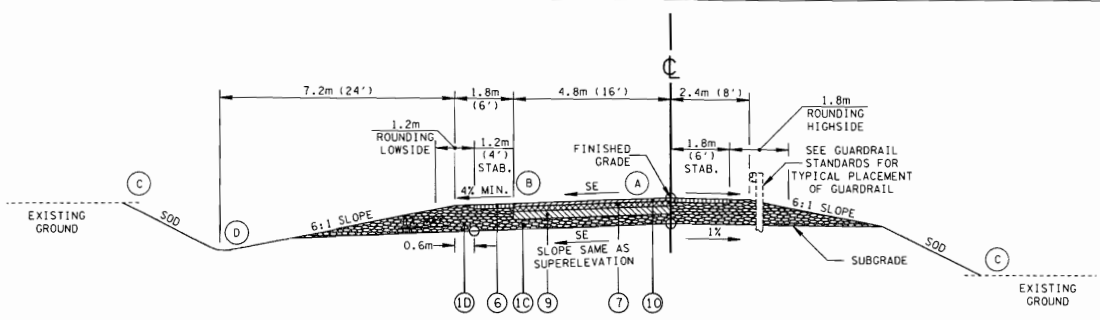
NOTE: ENGLISH UNITS ARE FOR REFERENCE ONLY.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TYPICALS SECTIONS

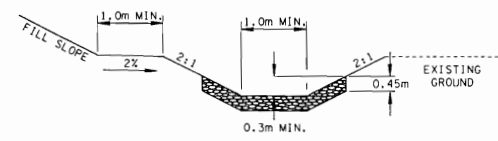
TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2000	47026-2268-04	2D

REV. 04-13-2001: OMITTED SECTION WITHIN WALLS AT BRIDGE
 REV. 28-FEB-2003: ADDED GREENWAY DETAIL FOR RAMPS. REVISED STA. LOCATIONS FOR RAMPS. REVISED TITLES FOR TYPICALS.



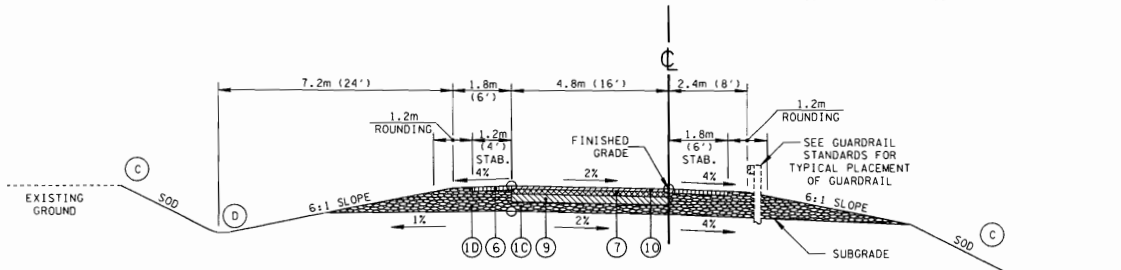
TYPICAL SUPERELEVATED SECTION RAMPS A & B
 (BASED ON STD. DWG. RDM-TS-4)

RAMP B	RAMP A
STA. 6+503.000 TO STA. 6+598.317	STA. 122+545.168 TO STA. 122+686.241
STA. 6+636.778 TO STA. 6+779.737	STA. 122+815.446 TO STA. 122+921.854
	STA. 111+580.000 TO STA. 111+691.000



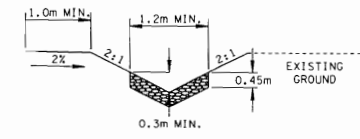
TYPICAL RIP RAP OR SOD "TRAPEZOIDAL" DITCH

SEE TABLE FOR DITCH LOCATION
 (SAME WIDTH DIMENSIONS FOR SEED AND SOD DITCHES)



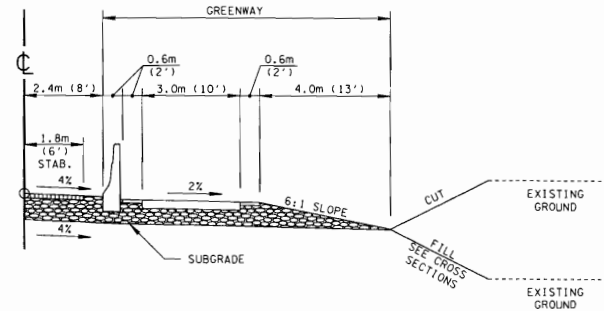
TYPICAL TANGENT SECTION RAMPS A & B
 (BASED ON STD. DWG. RDM-TS-4)

RAMP B	RAMP A
STA. 6+500.000 TO STA. 6+503.000	STA. 122+360.000 TO STA. 122+545.168
STA. 6+598.317 TO STA. 6+636.778	STA. 122+686.241 TO STA. 122+815.446
STA. 6+779.737 TO STA. 6+805.710	STA. 122+921.854 TO STA. 122+987.335



TYPICAL RIP RAP OR SOD "V" DITCH

SEE TABLE FOR DITCH LOCATION



GREENWAY RAMPS A & B

STA. 122+360.000 TO STA. 122+609.219 RAMP A (12+360 ML)
 SEE COLLECTOR NO. 6
 STA. 6+500.000 TO STA. 6+663.000 RAMP B

- (A) THE SLOPE OF THE SHOULDER AND ROADWAY PAVEMENT SHALL NOT EXCEED AN ALGEBRAIC DIFFERENCE OF 7%.
- (B) SHOULDER SLOPE ON LOW SIDE EQUAL TO S.E. WHEN S.E. IS GREATER THAN NORMAL SHOULDER SLOPE.
- (C) SEE STD. DRAWING RDM-S-11 FOR FILL AND CUT SLOPE TABLE (CASE 1) UNLESS NOTED OTHERWISE ON CROSS SECTIONS
- (D) SEE STD. DRAWING RDM-S-11A FOR ROUNDING OF ROADSIDE DITCH SLOPE

SEE SHEET 2B FOR PAVING SCHEDULE	
OVERLAY PAVING SCHEDULE	
③	GRADING "A" MIXTURE (BLACK BASE) (0.1830 Tonnes/m ²) (75mm DEPTH) Item 307M08.01 Performance Grade (PG 76-22); (BPMB-HM) Grade "A"
④	GRADING "B-M2" MIXTURE (BINDER) (0.1200 Tonnes/m ²) (50mm DEPTH) Item 307M08.06 Performance Grade (PG 76-22); (BPMB-HM) Grade B-M2
⑤	GRADING "D" SURFACE (0.0684 Tonnes/m ²) (30mm DEPTH) Item 411M08.01 Performance Grade Asphalt Cement (PG 76-22) Grade "D" Item 411M01.01 Mineral Aggregate (BPMS-CM) Includes Choker
⑪	COLD PLANING EXISTING BASE AND PAVE Item 415M01.01 Cold Planing Bituminous Pavement
①	TACK COAT (0.00009336 Tonnes/m ²) Item 403M01 Bituminous Material for Tack Coat (TC)



NOTE: ENGLISH UNITS ARE FOR REFERENCE ONLY.

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

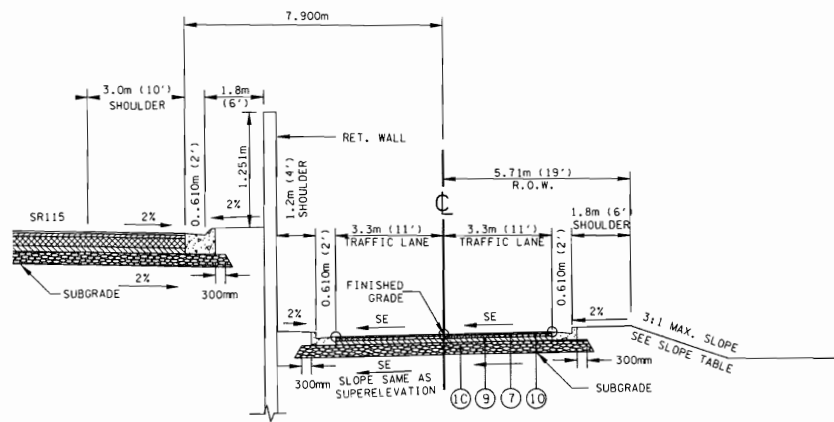
TYPICALS SECTIONS

DESIGN DIVISION

FILE NO. ---

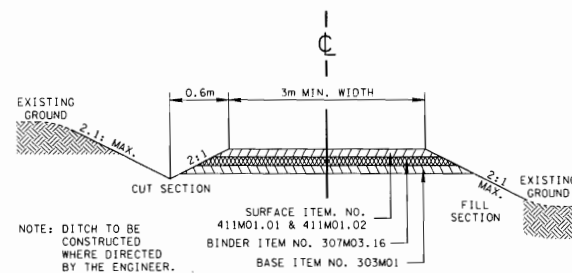
TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2000	47026-2268-04	2E

REV. 28-FEB-2003; REVISED STA. LOCATION FOR COLLECTOR NO.5 ADDED DRIVEWAY DETAIL. REMOVED NOTE BLOCKS.



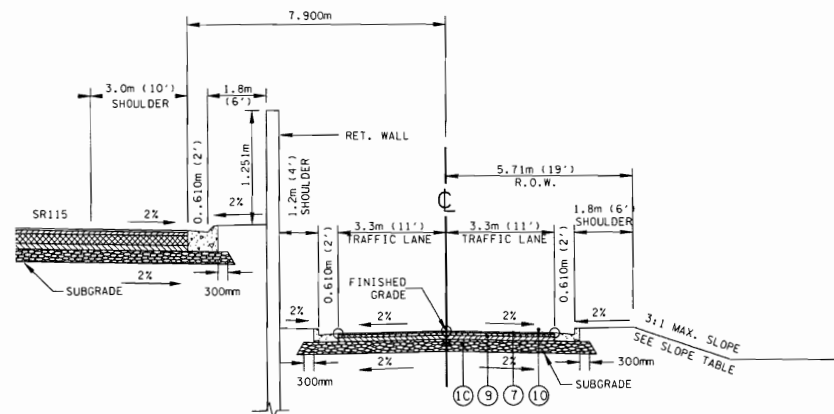
**TYPICAL SUPERELEVATED SECTION
COLLECTOR NO. 5**

STA. 4+448.429 - STA. 4+546.686
 STA. 4+561.814 - STA. 4+644.898
 STA. 4+676.457 - STA. 4+751.433
 STA. 5+074.147 - STA. 5+170.000



**TYPICAL SECTION
PRIVATE DRIVE TO BUSINESS, FIELD
OR RESIDENTIAL PROPERTY**

BUSINESS	FIELD OR RESIDENTIAL
SURFACE - 30 mm	SURFACE - 40 mm
BINDER - 45 mm	BINDER - NONE
BASE - 100 mm	BASE - 100 mm



**TYPICAL TANGENT SECTION
COLLECTOR NO. 5**

STA. 4+420.000 - STA. 4+448.429
 STA. 4+546.686 - STA. 4+561.814
 STA. 4+644.898 - STA. 4+676.457
 STA. 4+751.433 - STA. 5+074.147

- NOTES:**
- DRIVEWAY DESIGN SHALL BE IN ACCORDANCE WITH THE STATE OF TENNESSEE RULES AND REGULATIONS FOR CONSTRUCTION OF DRIVEWAYS ON STATE HIGHWAY RIGHT-OF-WAY.
 - WHERE SURFACE OF EXISTING DRIVE IS CONCRETE, SUBSTITUTE 150 mm CONCRETE FOR BASE AND SURFACE.
 - FOR CURB AND GUTTER SECTIONS, THIS TYPICAL SECTION IS TO BEGIN AT THE BACK OF THE PROPOSED OR FUTURE SIDEWALK. SEE STANDARD DRAWING RPM-D-14 FOR DESIGN OF DRIVES FROM CURB LINE TO BACK OF SIDEWALK.
 - IF AN EXISTING DRIVE IS GREATER THAN 3m, THE WIDTH OF THE PROPOSED DRIVE SHALL BE EQUAL TO THE EXISTING WIDTH; BUT NOT GREATER THAN THE MAXIMUM ALLOWABLE WIDTH AS SPECIFIED IN THE RULES AND REGULATIONS.
 - IF EXISTING DRIVE IS GRAVEL, SURFACING WILL BE FOR ONE SHOULDER WIDTH. THE REMAINDER OF THE DRIVE WILL BE REPLACED WITH GRAVEL TO THE TOUCHDOWN POINT.
 - WHEN REPLACING A DRIVE INTO A PARKING AREA, THE SIDE SLOPES SHALL BE 12:1.
 - SEE SHEET 3 FOR RIGHT-OF-WAY NOTES ON CONSTRUCTION DRIVEWAYS ON STATE HIGHWAY R.O.W.



NOTE: ENGLISH UNITS ARE FOR REFERENCE ONLY.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

**TYPICAL
SECTIONS**

R.O.W. ACQUISITION TABLE

TRACT NO	PROPERTY OWNERS	COUNTY RECORDS				AREA TO BE ACQUIRED HECTARES (ACRES)			AREA REMAINING HECTARES (ACRES)		EASEMENT SO. M (SQ. FT.)			TOTAL AREA HECTARES (ACRES)			O R G I N
		TAX MAP NO.	PARCEL NO.	DEED BK.	PAGE	LEFT	RIGHT	TOTAL	LEFT	RIGHT	PERM. DRAINAGE	SLOPE ②	CONST. ③	LEFT	RIGHT	TOTAL	
1	SEVIER HEIGHTS BAPTIST CHURCH	135-135-G	10.01-04 10.06-11	2135 2205	922 507		0.1679 (0.415)	0.1679 (0.415)		19.1827 (47.401)	221.0 (2,379)	0.4722 HA. (1.167 AC.)	0.1657 HA. (0.409 AC.)	19.3505 (47.816)	19.3505 (47.816)		C
2	NOT USED	-	-	-	-												-
3	BEATRICE H. CARPENTER	135	10	1997	689		0.1397 (0.345)	0.1397 (0.345)	2.3318 (5.762)		0.1058 HA. (0.261 AC.)	165.9 (4,363)	405.3 (6,107)	2.4715 (6.107)	2.4715 (6.107)		C
4	BETTY K. ZUMSTEIN	135	10.06	2177	739			0.4456 (1.101)					6.0 (65)	0.4456 (1.101)	0.4456 (1.101)		D
4S	BETTY K. ZUMSTEIN	135	10.06	2177	739			0.4456 (1.101)			17.9 (193)			0.4456 (1.101)	0.5099 (1.260)		D
5	DIXIE BEARING, INC.	135	10.05	2012	904			0.5099 (1.260)			19.4 (209)	3.0 (32)	85.6 (921)		0.5099 (1.260)		D
6	AMEX LEASING, INC.	A. 135-G	9.7	2035 2226	658 257 276		0.2426 (0.599)	0.2296 (0.567)	0.4722 (1.167)	2.7469 (6.788)	---			2.9895 (7.387)	0.2296 (0.567)	3.2191 (7.955)	D
7	NOT USED	-	-	-	-												-
8	CARGO OIL COMPANY, INC.	A. 135-G	9.01	1826	857		0.3177 (0.785)	0.3177 (0.785)	---	0.3177 (0 SF) ④				0.3177 (0.785)	0.3177 (0.785)		C
9	RICHARD H. HARB, JULIET HARB HOWELL, HARRIET HARB CABAGE AND GEORGIA HARB	A. 135-G	10.11	1918	151		0.1944 (0.480)	0.1944 (0.480)		1.5834 (3.913)		167.2 (1,800)	141.9 (1,527)		1.7778 (4.393)	1.7778 (4.393)	C
10	FLORENCE E. HILLIS	B.135-H	8	1817	309		0.3133 (0.774)	0.3133 (0.774)	---	0.3133 (0 SF) ④				0.3133 (0.774)	0.3133 (0.774)		C
10S	FLORENCE E. HILLIS	B.135-H	8	1817	309		154.0 M2 (1,658 SF)	0.5 M2 (5 SF)	154.5 M2 (1,663 SF)	---	0.0000 (0 SF)			154.0 M2 (1,658 SF)	0.5 M2 (5 SF)	154.5 M2 (1,663 SF)	C
11	RONALD A. WATKINS & ROBERT F. SLACK	B.135-H	8.02	1828	736		0.3213 (0.794)	0.3213 (0.794)	---	0.3213 (0 SF) ④				0.3213 (0.794)	0.3213 (0.794)		D
11A	RONALD A. WATKINS	B.135-H	8.03	2281 1827	1127 477		76.3 M2 (821 SF)	200.0 M2 (2,153 SF)	276.3 M2 (2,974 SF)	---	0.0000 (0 SF)			76.3 M2 (821 SF)	200.0 M2 (2,153 SF)	276.3 M2 (2,974 SF)	C
12	KINH NGUYEN	D.122-P	18.02	2325	421		12.8 M2 (138 SF)	12.8 M2 (138 SF)	0.4075 (1.007)			128.8 (1,386)	171.9 (1,850)	0.4088 (1.010)	0.4088 (1.010)		C
13	NOT USED	-	-	-	-												-
14	REX A. & DEBORAH S. WILSON	B.135-H	6	1614	317		80.7 M2 (869 SF)	80.7 M2 (869 SF)	0.2711 (0.685)			132.5 (1,426)	136.5 (1,469)		0.2852 (0.705)	0.2852 (0.705)	C
15	ALFORD - ALCOA, LTD.	B.135-H	7	1863	748		362.8 M2 (3,905 SF)	362.8 M2 (3,905 SF)	1.0127 (2.502)			158.1 (1,702)	408.0 (4,392) ⑦	1.0490 (2.592)	1.0490 (2.592)		C
16	WILLIAM L. IRWIN	B.135-H	5.01	1693	613-624		4.5 M2 (48 SF)	4.5 M2 (48 SF)	0.7928 (1.959)			60.5 (651)	69.9 (752)	0.7932 (1.960)	0.7932 (1.960)		D
17	JAMES E. & JEANNE S. GEIGER	B.135-H	8.01, 9	1634	560		0.9082 (2.244)	0.9755 (2.411)	0.2725 (0.673) ⑤	0.0000 (0 SF)				1.1807 (2.918)	1.2480 (3.084)		C
18	LAKEMOOR LAND CO.	A.135-G	12.01	1929	24				0.2533 (0.626)			63.2 (680)	82.2 (885)	0.2533 (0.626)	0.2533 (0.626)		D
18S	LAKEMOOR LAND CO.	A.135-G	12.01	1929	24		13.3 M2 (143 SF)	13.3 M2 (143 SF)	0.2520 (0.623)			14.3 (154)	84.1 (905)	0.2533 (0.626)	0.2533 (0.626)		D
19	BETTY HARB AND RAMSEY J. & MARGARET ANN HARB	A.135-G	12	1196	1		0.2876 (0.711)	0.2876 (0.711)	0.0000 (0 SF) ④					0.2876 (0.711)	0.2876 (0.711)		D
20	GUS & MARY KAMPAS, VICTOR AND REBECCA KAMPAS	B.135-H	7.01	2085	11		303.2 M2 (3,264 SF)	303.2 M2 (3,264 SF)	0.3291 (0.813)			78.7 (847)	189.3 (2,038) ③	0.3594 (0.888)	0.3594 (0.888)		C
21	THOMA J. BITH, JR. AND SUSAN BITH	D.122-9	17	2241	270		216.6 M2 (2,331 SF)	216.6 M2 (2,331 SF)	0.2838 (0.701)			71.6 (771)	164.0 (1,765) ⑦	0.3055 (0.755)	0.3055 (0.755)		C
22	JAMES L. KENNEDY	A.135-G	35	1933	934		26.2 M2 (282 SF)	26.2 M2 (282 SF)	0.5420 (1.339)		17.4 (187)	323.1 (3,478)	219.6 (2,364)		0.5446 (1.346)	0.5446 (1.346)	C
23	EUGENE BOLINGER AND LORINE BOLINGER	A.135-G	36	2021	466		26.0 M2 (280 SF)	26 M2 (280 SF)	0.2085 (0.515)				21.3 (229)	0.2111 (0.522)	0.2111 (0.522)		C

ORIGIN LEGEND

- D = DEED
- TM = TAX MAP
- C = CALCULATED
- ① FOR CONSTRUCTION OF DRAINAGE DITCH
- ② FOR CONSTRUCTION OF SLOPE
- ③ FOR CONSTRUCTION OF EROSION CONTROL AND FOR WORKING ROOM
- ④ TOTAL TAKE
- ⑤ LOSS OF ACCESS
- ⑥ CONSTRUCTION EASEMENT FOR REMOVAL OF STRUCTURES
- ⑦ CONSTRUCTION EASEMENT FOR BUILDING THE RETAINING WALL IS TO BE LIMITED TO ONE YEAR.

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2000	47026-2268-04	3A

REV. 04-13-2001: REVISED AREAS FOR TRACTS 31 & 35
 REV. 03-01-2002: REVISED AREAS OF TRACT 17 AND LABELED REMAINDER AS LOSS OF ACCESS.
 REV. 28-FEB-2003: REVISED TABLE CONTENTS FROM TRACTS 1-36 TO 1-25; ADDED TRACT 105, 11A, 185, AND 25; REVISED R.O.W. SLOPE, AND CONST. EASEMENTS TRACTS 1, 19 AND 22; REVISED SLOPE, CONST. AND DRAINAGE EASEMENT TRACTS 3 AND 5; REVISED CONST. AND DRAINAGE EASEMENT TRACT 4; REVISED SLOPE AND CONST. EASEMENT, TRACTS 15 AND 20; REVISED R.O.W. TRACT 17; RENAMED TRACT 24 TO TRACT 8024
 REV. 15-MAR-2004: ADDED NOTES 6, 7 AND 8; RESTORED TRACT 24; ADDED TRACT 45; MOVED TRACT 24, 8024 AND 25 DATA TO SHEET 3C.
 REV. 24-SEP-2004: REMOVED NOTE * 8; REVISED CONSTRUCTION EASEMENTS ON TRACT 15.



R.O.W. ACQUISITION TABLE

TRACT NO	PROPERTY OWNERS	COUNTY RECORDS				AREA TO BE ACQUIRED HECTARES (ACRES)			AREA REMAINING HECTARES (ACRES)		EASEMENT SO. M. (SO. FT.)			TOTAL AREA HECTARES (ACRES)			ORIGIN	
		TAX MAP NO.	PARCEL NO.	DEED BK.	PAGE	LEFT	RIGHT	TOTAL	LEFT	RIGHT	PERM. DRAINAGE	SLOPE ②	CONST. ③	LEFT	RIGHT	TOTAL		
24	SCOTT DEAN OWENS AND KAREN E. OWENS	A.135-G	34	2241	449					0.1448 (0.358)					0.1448 (0.358)	0.1448 (0.358)	C	
8024	SCOTT DEAN OWENS AND KAREN E. OWENS	A.135-G	34	2241	449	72.1 M2 (776 SF)		72.1 S.F. (776 SF)		---					72.1 M2 (776 SF)	72.1 S.F. (776 SF)	C	
25	MICHAEL JASON & TIFFANY JOHNSON FRENCH	A.135-G	13	20020417	0086054		22.7 M2 (244 SF)	22.7 S.F. (244 SF)		0.2467 (0.610)			159.8 (1,720)	48.9 (526)		0.2490 (0.615)	0.2490 (0.615)	C
26	WILLADEAN ROBERTA SMITH	A.135-G	27	1490 1419	621 183		0.0863 (0.213)	0.0863 (0.213)		0.7648 (1.890)	334.9 (3,605)	0.1120 HA. (0.277 AC.)	398.0 (4,284)		0.8511 (2.103)	0.8511 (2.103)	C	
27	MARATHON FINANCE CO.	D.122-P	16	1312	136		0.0538 (0.133)	0.0538 (0.133)	0.3605 (0.891)				93.5 (1,006)	84.2 (906)	0.4143 (1.024)	0.4143 (1.024)	C	
28	TLC PROPERTIES, INC.	D.122-P	18.01	2183	207		368.0 M2 (3,961 SF)	368 M2 (3,961 SF)	0.2246 (0.555)				0.0619 HA. (0.153 AC.)	131.6 (1,417)	0.2614 (0.646)	0.2614 (0.646)	D	
29	NOT USED	-	-	-	-												-	
30	HAROLD EUGENE LOVLACE	D.122-P	15	2187	1077		235.5 M2 (2,535 SF)	235.5 M2 (2,535 SF)	1.2148 (3.002)			0.0669 HA. (0.165 AC.)	71.4 (769)	43.6 (469)	1.2383 (3.060)	1.2383 (3.060)	D	
31	WILLIAM L. IRWIN	A.135-B	28	1622	901		98.8 M2 (1,063 SF)	98.8 M2 (1,063 SF)		0.5367 (1.326)		0.0874 HA. (0.216 AC.)	11.3 (122)	43.8 (471)	0.5466 (1.351)	0.5466 (1.351)	C	
32	NOT USED	-	-	-	-												-	
33	LYNN B. & WILLIAM B. WEIGEL FOR LIFE REMAINDER TO LBW PROPERTIES, INC. & MBW PROPERTIES INC.	D.122-P	15.01	1961	42		368.5 M2 (3,967 SF)	368.5 M2 (3,967 SF)	0.4666 (1.153)				430.8 (4,637)	148.6 (1,600)	0.5034 (1.244)	0.5034 (1.244)	C	
34	NOT USED	-	-	-	-												-	
35	WILLIAM C. & VIRGINIA G. NASH	A.135-B	29 30 31	1873 1964 2185	418 271 173			0.8119 (2.006)	0.8119 (2.006)		2.0155 (4.980)	102.3 (1,101)	10.2 (110)	273.5 (2,944)	2.8274 (6.987)	2.8274 (6.987)	D	
36	NOT USED	-	-	-	-												-	
37	VULCAN MATERIALS CO.	D.122-P	13	1021 804 764	125 5 75		0.0720 (0.178)	0.0720 (0.178)	3.2591 (8.053)				116.9 (1,258)	289.1 (3,112)	3.3311 (8.231)	3.3311 (8.231)	C	
38	NOT USED	-	-	-	-												-	
39	GARY CULLUM, SR.	A.135-B	32	1970	27		0.0642 (0.159)	0.0642 (0.159)		5.4647 (13.504)			72.9 (1,611)	53.0 (1,171)	5.5289 (13.662)	5.5289 (13.662)	C	
40	NOT USED	-	-	-	-												-	
41	NOT USED	-	-	-	-												-	
42	MILDRED B. IRWIN FAMILY PARTNERSHIP	D.122-P	12.01 12.02	2315	600 604 608 612		0.1340 (0.331)	0.1340 (0.331)	0.8531 (2.108)				136.3 (1,467)	324.9 (3,497)	0.9871 (2,439)	0.9871 (2,439)	C	
43	VILLAGE PLAZA SOUTH, LTD.	D.122-P	12.03	1843	9		0.2548 (0.630)	0.2548 (0.630)	3.3879 (8.372)				90.2 (971)	274.2 (2,951)	3.6427 (9.001)	3.6427 (9.001)	C	
44	MILDRED E. IRWIN	D.122-P	12	1843	9		284.9 M2 (3,066 SF)	284.9 M2 (3,066 SF)	0.9764 (2.413)				189.2 (2,037)	85.3 (918)	1.0049 (2.483)	1.0049 (2.483)	C	
45	ESTATE OF FLORENCE STEPHENSON MONDAY	J.122-0	2.01	2229	548		0.1051 (0.260)	0.1051 (0.260)		2.0046 (4.954)			21.5 (231)	140.7 (1,514)	2.1097 (5.213)	2.1097 (5.213)	C	
46	MILDRED E. IRWIN	D.122-P	12	559 582 599	420 353 96		0.0882 (0.218)	0.0882 (0.218)	5.8971 (14.572)				0.0529 HA. (0.131 AC.)	0.0710 HA. (0.175 AC.)	5.9853 (14.790)	5.9853 (14.790)	C	
47	ROBERT W. MONDAY (LEASED BY SPARTAN RESTAURANTS, INC.)	J.122-0	2.02	1957	551		0.0971 (0.240)	0.0971 (0.240)		0.3846 (0.950)			175.3 (1,887)	57.0 (614)	0.4816 (1.190)	0.4816 (1.190)	D	
48	ROBERT W. MONDAY	J.122-0	2	1957	551		0.1351 (0.334)	0.1351 (0.334)		4.3039 (10.635)			211.4 (2,275)		4.4390 (10.969)	4.4390 (10.969)	D	
49	MILDRED B. IRWIN FAMILY PARTNERSHIP	D.122-P	12.04	2315	600 604 608 612		0.1078 (0.266)	0.1078 (0.266)	0.4009 (0.991)				222.6 (2,331)	472.5 (4,947)	0.5087 (1.257)	0.5087 (1.257)	C	
50	SHAILESH & KALPANA SHAN	D.122-P	8	2257	1189		0.5463 (1.350)	0.5463 (1.350)	---						0.5463 (1.350)	0.5463 (1.350)	C	
51	SOUTHGATE SHOPPING CENTER, INC.	J.122-0	3	1368	779		0.0965 (0.238)	0.0965 (0.238)		2.0856 (5.154)			134.9 (1,452)	287.6 (3,096)	2.1821 (5.392)	2.1821 (5.392)	C	

ORIGIN LEGEND

- D = DEED
- TM = TAX MAP
- C = CALCULATED
- ① FOR CONSTRUCTION OF DRAINAGE DITCH
- ② FOR CONSTRUCTION OF SLOPE
- ③ FOR CONSTRUCTION OF EROSION CONTROL AND FOR WORKING ROOM
- ④ TOTAL TAKE
- ⑤ LOSS OF ACCESS
- ⑥ CONSTRUCTION EASEMENT FOR REMOVAL OF STRUCTURES
- ⑦ CONSTRUCTION EASEMENT FOR BUILDING THE RETAINING WALL IS TO BE LIMITED TO ONE YEAR.

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2000	47026-2268-04	3C

REV. 04-13-2001 REVISED AREAS FOR TRACTS 37, 39, 42, 43, 45, AND 46
 REV. 05-15-2001 ADDED TRACTS 66 & 70.
 REV. 28-FEB-2003 REVISED TABLE CONTENTS FROM TRACTS 37-74 TO 26-51. ADDED TRACT 265. REVISED R.O.W., SLOPE, AND CONST. EASEMENTS TRACTS 35, 39, 43, 44, 45, 46, 49, 50, AND 51. REVISED SLOPE, CONST. AND DRAINAGE EASEMENT TRACT 31.
 REV. 15-MAR-2004 ADDED NOTES 6, 7 AND 8. REVISED ROW SLOPE, AND CONST. EASEMENT TRACTS 43, 44, 46, AND 49. REVISED ROW TRACT 50. REMOVED TRACT 265. REVISED CONST EASEMENT TRACT 27 AND 42. ADDED TRACT 24, 8024 AND 25 FROM SHEET 3A.
 REV. 24-SEP-2004; REMOVED NOTE # 8. REVISED OWNER NAME, COUNTY RECORDS AND AREA TO BE ACQUIRED ON TRACT 44.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

R.O.W. ACQUISITION TABLE

DESIGN DIVISION

FILE NO. ---

R.O.W. ACQUISITION TABLE

TRACT NO	PROPERTY OWNERS	COUNTY RECORDS				AREA TO BE ACQUIRED HECTARES (ACRES)			AREA REMAINING HECTARES (ACRES)		EASEMENT SO. M (SO. FT.)			TOTAL AREA HECTARES (ACRES)			ORIGIN	
		TAX MAP NO.	PARCEL NO.	DEED BK.	PAGE	LEFT	RIGHT	TOTAL	LEFT	RIGHT	PERM. DRAINAGE	SLOPE ②	CONST. ③	LEFT	RIGHT	TOTAL		
																		LEFT
52	RICHARD M. & CAROL LYNN WARREN	D.122-P	6, 7	1806, 1676	135, 683	0.6015 (1.486)		0.6015 (1.486)	---				0.6015 (1.486)		0.6015 (1.486)	C		
53	MILDRED E. IRWIN	D.122-P	12	1843	9				0.3415 (0.844)				74.4 (801)	56.9 (612)	0.3415 (0.844)	C		
54	ALVIN WILBERT & GRACE MILDRED RUTH	D.122-P	5	1181	155	148.0 M2 (1,593 SF)		148 S.F. (1,593 SF)	0.3291 (0.813)				0.0967 HA. (0.239 AC.)	12.5 (135)	0.3439 (0.850)	C		
54S	ALVIN WILBERT & GRACE MILDRED RUTH	D.122-P	5	1181	155				0.3291 (0.813)						0.3291 (0.813)	C		
55	ELIZABETH HARB ET AL. & CHRISTOPHER RAMSEY HARB ET AL.	J.122-0	4	1595 WB 75	963, 980		289.9 M2 (3,120 SF)	289.9 M2 (3,120 SF)		0.0596 (0.147)				94.8 (1,020)	0.0886 (0.219)	0.0886 (0.219)	C	
56	MARY EVELIN FOSTER	D.122-P	2	1286, 1286	173, 18	2.7235 (6.730)		2.7235 (6.730)	0.4803 (1.187)						3.2038 (7.917)	3.2038 (7.917)	C	
57	ROBERT KENNETH VANN AND ROBERT W. MONDAY	J.122-0	5	2015	146		0.6038 (1.492)	0.6038 (1.492)		0.5777 (1.428)					1.1815 (2.920)	1.1815 (2.920)	C	
58	MILDRED E. IRWIN	D.122-P	12	1843	9	0.0839 (0.207)		0.0839 (0.207)	4.0926 (10.113)						4.1765 (10.320)	4.1765 (10.320)	C	
59	E. BRUCE FOSTER, SR.	D.122-P	3, 4	1917	455	0.6249 (1.544)		0.6249 (1.544)	---						0.6249 (1.544)	0.6249 (1.544)	C	
60	NOT USED	-	-	-	-												(0.000)	-
61	MARK ALLEN AND JANET POWELL LOWE	J.122-0	6	2068	1036		258.2 M2 (2,719 SF)	258.2 M2 (2,719 SF)		0.1963 (0.485)					0.2221 (0.549)	0.2221 (0.549)	C	
62	WELDON W. & HOPE G. WILLIAMS	A.122-0	15	1098	491		0.0805 (0.199)	0.0805 (0.199)		0.2898 (0.716)					0.3703 (0.915)	0.3703 (0.915)	C	
63	MELANIE DOSS	A.122-J	6	1985	414		0.0653 (0.161)	0.0653 (0.161)		0.5240 (1.036)					0.4845 (1.197)	0.4845 (1.197)	C	
64	ELVIN ELLIS OVERTON & HARRIET BROWN OVERTON	A.122-0	+	1073	159					0.2538 (0.627)					0.2538 (0.627)	0.2538 (0.627)	C	
65	NOT USED	-	-	-	-												(0.000)	-
66	WADE H. BOSWELL & RUBY N. BOSWELL	A.122-J	4	1426	647					0.4858 (1.200)					0.4858 (1.200)	0.4858 (1.200)	C	
67	E. BRUCE FOSTER	D.122-P, C.122-1	1, 12	1097	379	3.1858 (7.872)		3.1858 (7.872)	1.4888 (3.679)						4.6746 (11.551)	4.6746 (11.551)	C	
8068	GEOFFERY S. AND ALICIA H. KRANZ	A.122-J	5	2010	107		0.6312 (1.560)	0.6312 (1.560)		---					0.6312 (1.560)	0.6312 (1.560)	C	
69	CLIFFORD E. BARBOUR, JR.	A.122-J	7, 8, 10, 12	2275	1059		0.8976 (2.218)	0.8976 (2.218)		1.8180 (4.492)					2.7155 (6.710)	2.7155 (6.710)	C	
70	HOWARD L. THOMAS & GAYNELL H. THOMAS	A.122-J	3	1079	547					0.5084 (1.256)					0.5084 (1.256)	0.5084 (1.256)	C	
71	JAMES G. INGRAM	A.122-J	16	2132	1181		237.3 M2 (2,554 SF)	237.3 M2 (2,554 SF)		0.6217 (1.536)					0.6454 (1.595)	0.6454 (1.595)	C	
72	EDITH M. QUINN	A.122-J	9	935	343		0.6002 (1.483)	0.6002 (1.483)		---					0.6002 (1.483)	0.6002 (1.483)	C	
73	HAROLD W., JR. & MARGARET L. NICHOLSON	A.122-J	29	1663	131		0.2900 (0.717)	0.2900 (0.717)		---					0.2900 (0.717)	0.2900 (0.717)	D	
74	WILLIAM EARL MARTELLA	A.122-J	28	1669	652		0.3460 (0.855)	0.3460 (0.855)		---					0.3460 (0.855)	0.3460 (0.855)	C	
75	NOT USED	-	-	-	-												(0.000)	-
76	THOMAS J. & CAROL R. GREER	A.122-J	26, 27	1845	341		0.2352 (0.581)	0.2352 (0.581)		0.3910 (0.966)					0.6262 (1.547)	0.6262 (1.547)	C	
76S	THOMAS J. & CAROL R. GREER	A.122-J	26, 27	1845	341		228.0 M2 (2,454 SF)	228.0 M2 (2,454 SF)		0.3682 (0.910)					0.3910 (0.966)	0.3910 (0.966)	C	
77	NOT USED	-	-	-	-												(0.000)	-

ORIGIN LEGEND

- D = DEED
- TM = TAX MAP
- C = CALCULATED

- ① FOR CONSTRUCTION OF DRAINAGE DITCH
- ② FOR CONSTRUCTION OF SLOPE
- ③ FOR CONSTRUCTION OF EROSION CONTROL AND FOR WORKING ROOM
- ④ TOTAL TAKE
- ⑤ LOSS OF ACCESS
- ⑥ CONSTRUCTION EASEMENT FOR REMOVAL OF STRUCTURES
- ⑦ CONSTRUCTION EASEMENT FOR BUILDING THE RETAINING WALL IS TO BE LIMITED TO ONE YEAR.

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2000	47026-2268-04	3E

REV. 04-13-2001; REVISED AREAS FOR TRACT 95.
 REV. 03-01-2002; REVISED AREAS OF TRACT 98.
 REV. 28-FEB-2003; REVISED TABLE CONTENTS FROM TRACTS 75-104 TO 52-77. LINED OUT TRACTS 64,66, AND 70. ADDED TRACTS 53,54S, 58,61,71 AND 76S. REVISED R.O.W. SLOPE, AND CONST. EASEMENTS TRACTS 52,54,55,56, 57 AND 59. REVISED R.O.W. TRACTS 62,63,67, AND 69. RENAMED TRACT 68 TO 8068.
 REV. 15-MAR-2004; ADDED NOTES 6 AND 7. REMOVED LOSS OF ACCESS FROM TRACTS 52,54 AND 54S MADE TOTAL TAKES. REVISED ROW TRACT 53,56 AND 57. REVISED AREA TRACT 74.



STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 R.O.W.
 ACQUISITION
 TABLE
 TRACT NO. 52-77

R.O.W. ACQUISITION TABLE

TRACT NO	PROPERTY OWNERS	COUNTY RECORDS				AREA TO BE ACQUIRED HECTARES (ACRES)			AREA REMAINING HECTARES (ACRES)		EASEMENT SQ. M (SQ. FT.)			TOTAL AREA HECTARES (ACRES)		ORIGIN		
		TAX MAP NO.	PARCEL NO.	DEED BK.	PAGE	LEFT	RIGHT	TOTAL	LEFT	RIGHT	PERM. DRAINAGE	SLOPE ②	CONST. ③	LEFT	RIGHT		TOTAL	
78	I.C. & JUNE W. KING JR.	B.122-1	9	1146	137				0.2230 (0.551)			5.0 (54)	30.5 (328)	0.2230 (0.551)	0.2230 (0.551)	C		
79	NOT USED	-	-	-	-										(0.000)	C		
80	THOMAS FRANCON & ASTRA SILVIA WILLIAMS	B.122-1	10	1416	244				0.2153 (0.532)			162.8 (1,752)	82.3 (886)	0.2153 (0.532)	0.2153 (0.532)	C		
81	J. PORTER & EDNA TAYLOR ⑧	A.122-J	24, 25	820, 912	147, 23		0.2334 (0.577)	0.2334 (0.577)		0.5083 (1,256)				0.7417 (1,833)	0.7417 (1,833)	C		
82	EVERETTE & SANDRA ORICK	B.122-1	11	1519	38				0.2296 (0.567)			229.5 (2,470)	73.2 (788)	0.2296 (0.567)	0.2296 (0.567)	C		
83	VIRGINIA R. LUND INSTRUMENT #199908270016735 ⑧	A.122-J	23	2187	651		0.1403 (0.347)	0.1403 (0.347)		0.3465 (0.856)				0.4868 (1,203)	0.4868 (1,203)	C		
84	E.L. & BETTY T. WILLIAMS	B.122-1	12	1860	519				0.2230 (0.551)			82.6 (889)	83.7 (901)	0.2230 (0.551)	0.2230 (0.551)	C		
85	NOT USED	-	-	-	-										(0.000)	-		
86	NOT USED	-	-	-	-										(0.000)	-		
87	NOT USED	-	-	-	-					9.3654 (23,142)				9.3654 (23,142)	9.3654 (23,142)	-		
88	EUGENE M. AND BETTY C. WELLS ③	A.1225	22	1724	564		0.2673 (0.660)	0.2673 (0.660)		0.4578 (1,131)				0.7251 (1,792)	0.7251 (1,792)	C		
89	NOT USED	-	-	-	-										(0.000)	-		
90	NOT USED	-	-	-	-										(0.000)	-		
91	EDWIN D. AND GERI M. GRAVES ⑧	A.122-J	31	2194	1009		0.2717 (0.671)	0.2717 (0.671)		0.6091 (1,505)				0.8808 (2,176)	0.8808 (2,176)	C		
92	NOT USED	-	-	-	-										(0.000)	-		
93	THOMAS P. AND MARTHA N. COOKE ⑧	A.122-J	30	1949	29		0.2685 (0.663)	0.2685 (0.663)		(4)---				0.2685 (0.663)	0.2685 (0.663)	C		
94	NOT USED	-	-	-	-										(0.000)	-		
95	KEITH & MELANIE ROBINSON	B.122-1	13	2259	695				0.2246 (0.555)			46.4 (499)	109.3 (1,176)	0.2246 (0.555)	0.2246 (0.555)	C		
96	RICHARD G. AND PAULA G. ISAAC	C.122-1	15	1708	57	49.9 M2 (537 SF)	0.1048 (0.259)	0.1098 (0.271)		0.9094 (2,247)				49.9 M2 (537 SF)	1.0142 (2,506)	1.0192 (2,518)	C	
96S	RICHARD G. AND PAULA G. ISAAC	C.122-1	15	1708	57	127.7 M2 (1,375 SF)	127.7 M2 (1,375 SF)							127.7 M2 (1,375 SF)	127.7 M2 (1,375 SF)	C		
97	THE UNIVERSITY OF TENNESSEE	135	11	1223	1082	463.0 M2 (4,984 SF)	463 M2 (4,984 SF)			99.1021 (244,886)	493.5 (5,312)			99.1484 (245,000)	99.1484 (245,000)	D		
98	ALLEN B. MILLER, JR.	B.135-H	10	2215	210	160.2 M2 (1,724 SF)	9.5 M2 (102 SF)	169.7 M2 (1,827 SF)	0.3568 (0.882)			7.5 (81)	186.3 (2,005)	76.4 (822)	0.3728 (0.921)	9.5 M2 (102 SF)	0.3738 (0.924)	C
99	DONALD W. FREEMAN AND MARIE M. FREEMAN	B.135-H	11	1756, 2013	347, 52	52.2 M2 (562 SF)		52.2 M2 (562 SF)	0.8311 (2,054)			8.1 (87)	281.3 (3,092)	134.9 (1,452)	0.8363 (2,067)	0.8363 (2,067)	C	
100	TENNESSEE VALLEY AUTHORITY	-	-	650	45		1.0410 (2,572)	1.0410 (2,572)		2747 M2 (6,679)				1.3157 (3,251)	1.3157 (3,251)	-		
101	NOT USED	-	-	-	-										(0.000)	-		
102	NOT USED	-	-	-	-										(0.000)	-		
103	ERNEST WAYNE HEATON	B.135-H	5	1846	48		53.8 M2 (579 SF)	53.8 M2 (579 SF)		0.2148 (0.531)			40.1 (432)		0.2202 (0.544)	0.2202 (0.544)	C	
104	NOT USED	-	-	-	-										(0.000)	-		

ORIGIN LEGEND

D = DEED
TM = TAX MAP
C = CALCULATED

- ① FOR CONSTRUCTION OF DRAINAGE DITCH
- ② FOR CONSTRUCTION OF SLOPE
- ③ FOR CONSTRUCTION OF EROSION CONTROL AND FOR WORKING ROOM
- ④ TOTAL TAKE
- ⑤ LOSS OF ACCESS
- ⑥ CONSTRUCTION EASEMENT FOR REMOVAL OF STRUCTURES
- ⑦ CONSTRUCTION EASEMENT FOR BUILDING THE RETAINING WALL IS TO BE LIMITED TO ONE YEAR.
- ⑧ ENCUMBERED BY TVA FLOWAGE EASEMENT

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2000	47026-2268-04	3F

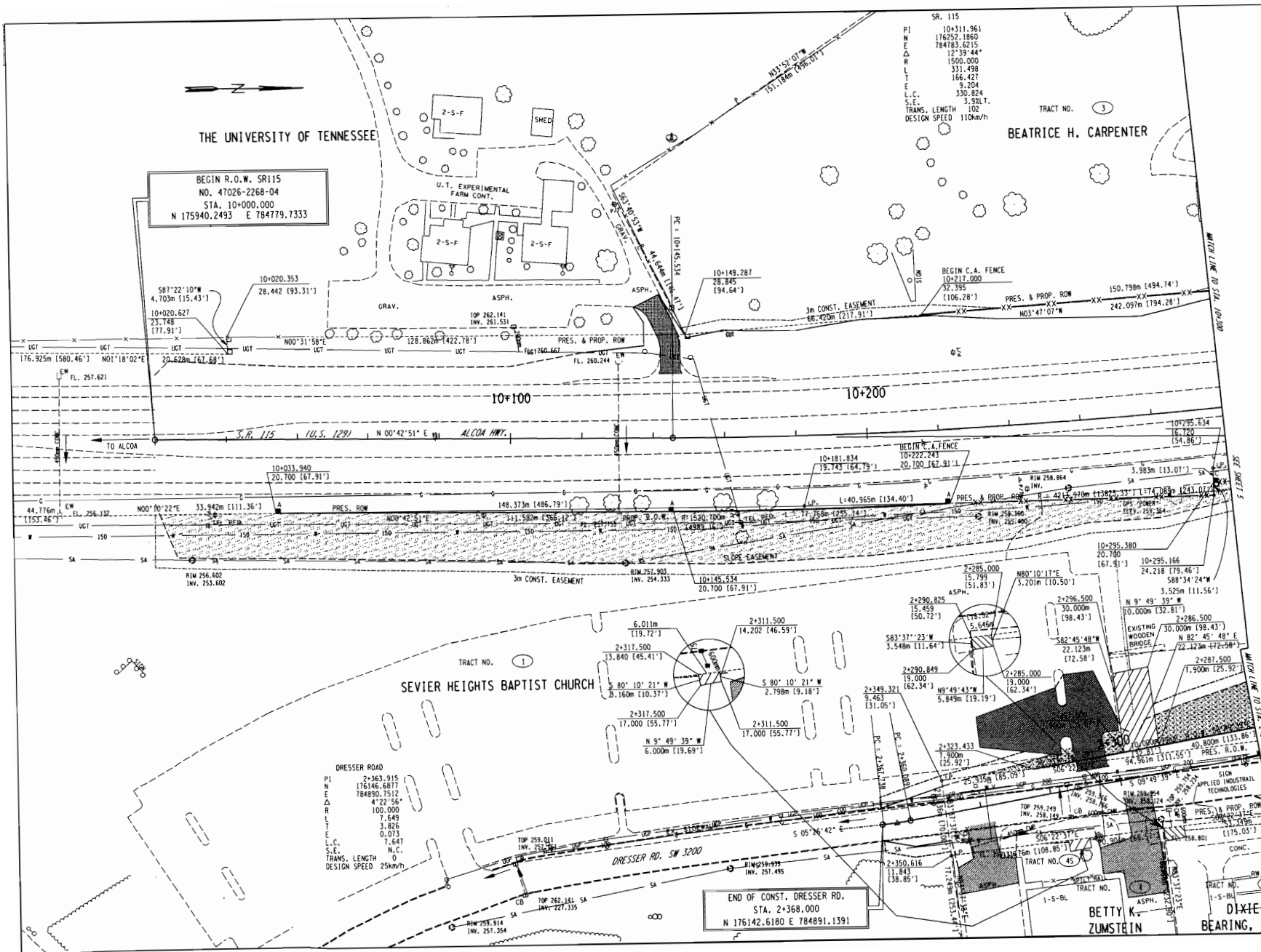
REV. 28-FEB-2003: ADDED SHEET AND MOVED TRACTS 78-104 FROM SHEET 3E. REVISED R.O.W. TRACTS 81,83,88,91,93 AND 96. ADDED TRACT 80S.
REV. 15-MAR-2004: ADDED NOTES 6 AND 7. REVISED TRACT 80S. ADDED TRACT 96S. MADE TRACT 93 A TOTAL TAKE.
REV. 24-SEP-2004: REVISED AREA TO BE ACQUIRED TRACT 91. ADDED TRACT 100. ADDED NOTE 8.



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
**R.O.W.
ACQUISITION
TABLE**
TRACT NO. 78-104

TYPE	YEAR	PROJECT NO.	SHEET NO.
ROW	2000	47026-2268-04	4

REV. 04-13-2001: ADDED SIGN IN FRONT OF TRACT #5
 REV. 28-FEB-2005: MOVED BEGINNING OF C.A. FENCE LT. FROM STA. 10+000.000 TO STA. 10+217.000
 MOVED BEGINNING OF C.A. FENCE RT. FROM STA. 10+000.000 TO STA. 10+222.243
 REVISED R.O.W. FOR TRACT 1. CHANGED PAVEMENT WIDTH FOR DRESSER RD. ADJUSTED DRIVEWAYS TO ACCOMMODATE CURB & GUTTER. REVISED DRAINAGE EASEMENT ON TRACTS 4 AND 5.
 REV. 15-MAR-2004: LABELED EX. DRIVEWAY TRACT 5 CONC. ADDED TRACT #5 DRAINAGE EASEMENT STA. 2+290.000
 DRESSER ROAD. ADDED DRIVEWAY STA 2+307RT. RT. LOCATED BRIDGE AND DITCH STA. 2+290.000
 REV. 24-SEP-2004: ADDED DRIVEWAY STA. 0+019 LT TRACT 1.



BEGIN R.O.W. SR115
 NO. 47026-2268-04
 STA. 10+000.000
 N 175940.2493 E 784779.7333

END OF CONST. DRESSER RD.
 STA. 2+368.000
 N 176142.6180 E 784891.1391

DRESSER ROAD
 P.I. 2+363.915
 N 176146.8871
 E 784890.7512
 Δ 4' 22" 56"
 R 100.000
 L 7.649
 T 3.826
 E 0.013
 S.E. 7.647
 N.C.
 TRANS. LENGTH 0
 DESIGN SPEED 25km/h

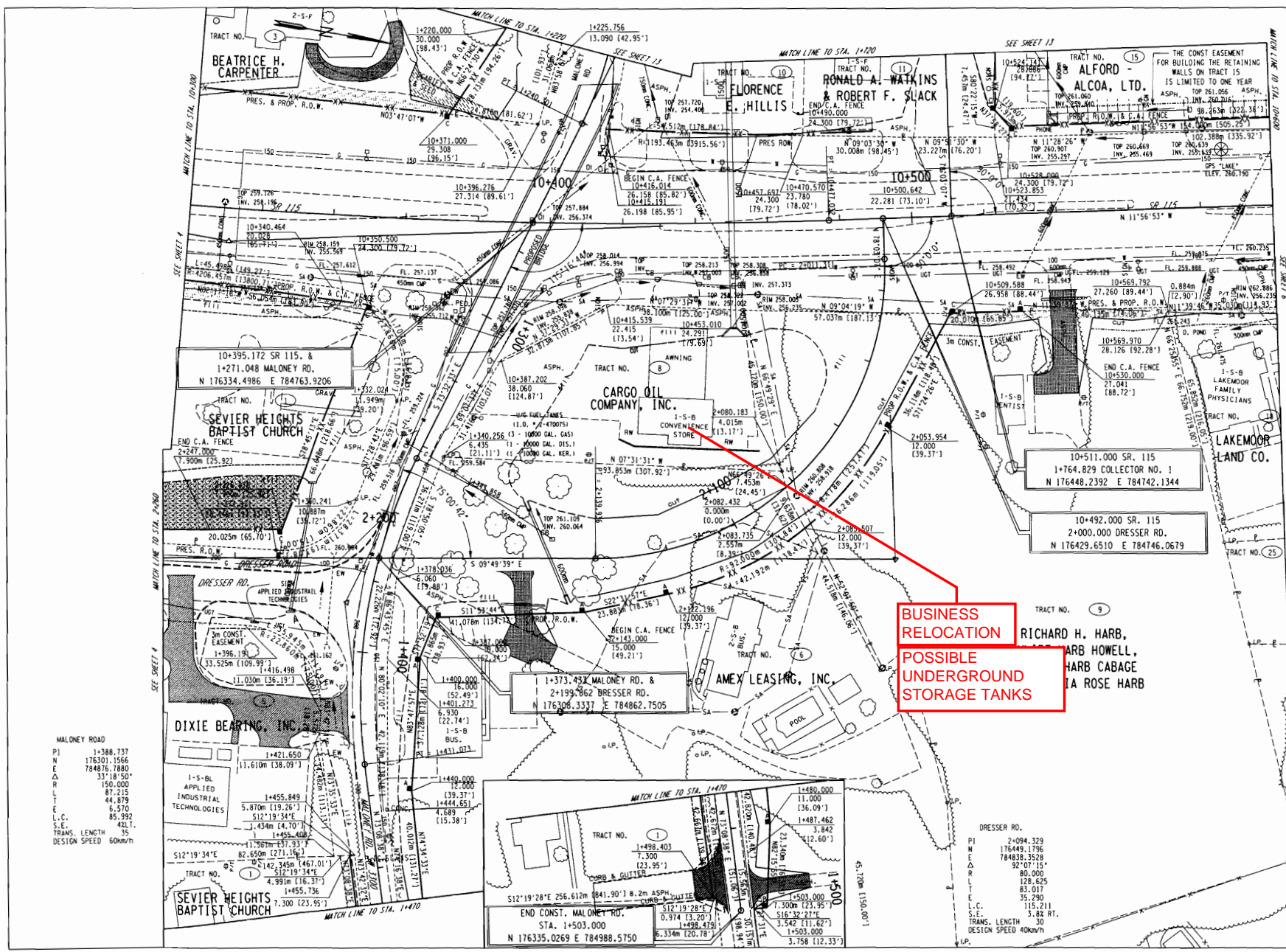


COORDINATE VALUES ARE NAD 83 (1995) AND ARE DATUM ADJUSTED BY THE FACTOR OF 1.00009166 & TIED TO THE TGRN.
 STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

PRESENT LAYOUT
 STA. 10+000 - STA. 10+300
 STA. 2+260 - STA. 2+368
 SCALE: 1:500

TYPE	YEAR	PROJECT NO.	SHEET NO.
ROW	2000	47026-2268-04	5

REV. 04-13-2001: ADDED SIGN IN FRONT OF TRACT #5
 REV. 28-FEB-2003: REVISED R.O.W. ON TRACT 1; ADJUSTED DRIVEWAY TO ACCOMMODATE CURB & GUTTER ON DRESSER ROAD; ADDED TRACT #25.
 REV. 15-MAR-2004: REMOVED SIDE DRAIN STA. 10+310+; SCARIFY REMAINDER OF EXISTING DRIVEWAY I.T STA. 10+310+; ADDED NOTE FOR CONST. EASEMENT TRACT 15; ADDED EXISTING 24" DRIVEWAY STA. 1+454; REVISED DRIVEWAY STA. 1+454 AND 1+497 MALONEY ROAD. REV. 24-SEP-2004: ADDED SANITARY SEWER LINES TRACT NO. 6.



BUSINESS RELOCATION
POSSIBLE UNDERGROUND STORAGE TANKS

TRACT NO. 9
 RICHARD H. HARB,
 HARB HOWELL,
 HARB CABAGE
 TA ROSE HARB

MALONEY ROAD
 P 1+386.737
 N 176301.1566
 E 784876.7880
 Δ 33°18'50"
 L 150.000
 T 87.215
 C 44.879
 E 6.570
 L.C. 85.392
 S.E. 421.1
 TRANS. LENGTH 35
 DESIGN SPEED 60km/h

TRACT NO. 1
 1+498.403
 7.300
 (23.95')
 S12°19'28"E 256.612m (841.90') R. 2m ASPH. CUTTIE CURB & GUTTER
 0.974 (3.20')
 1+498.475
 1+503.000
 6.334m (20.78')
 3.758 (12.33')
 END CONST. MALONEY RD.
 STA. 1+503.000
 N 176335.0269 E 784988.5750

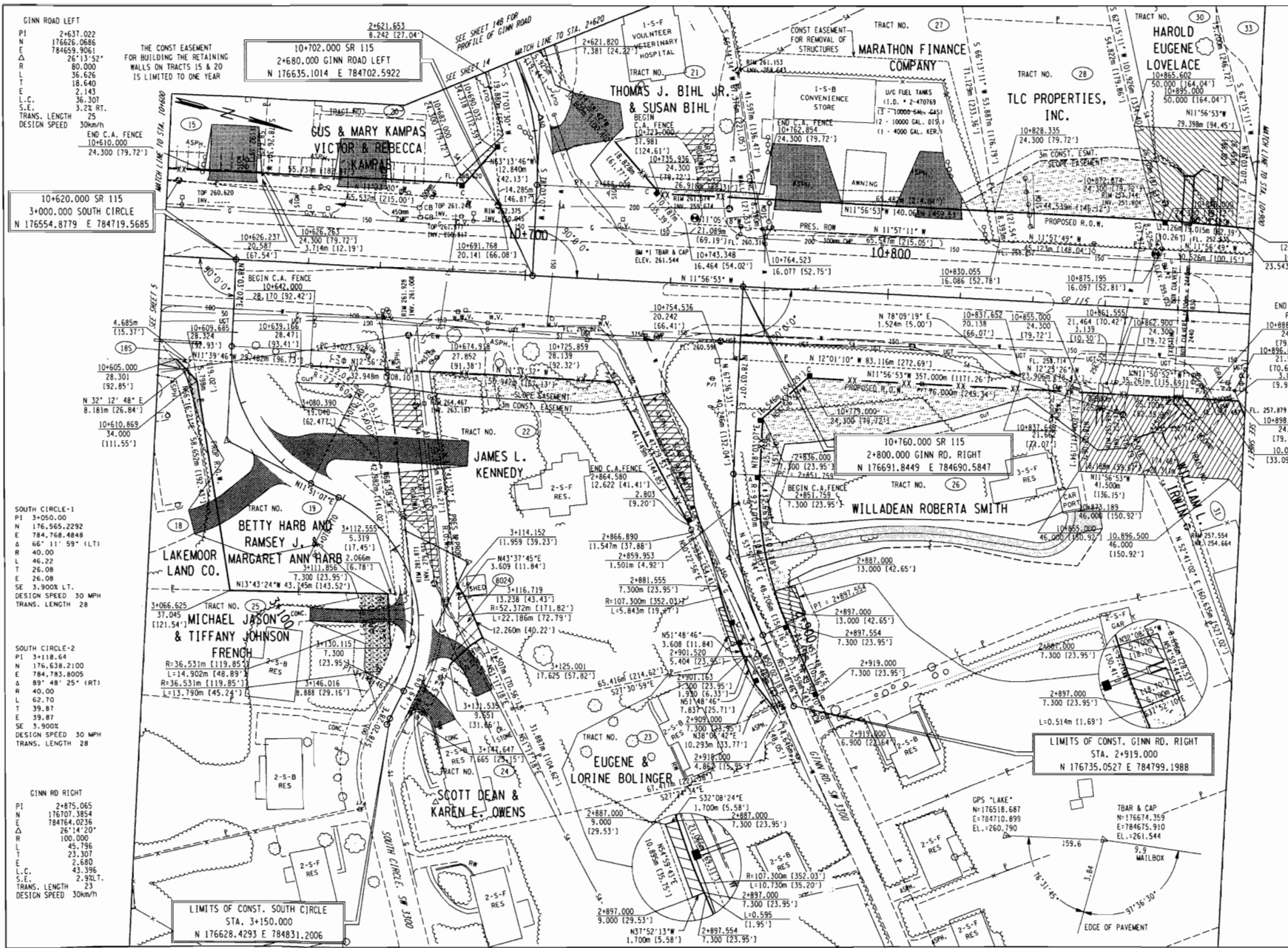
DRESSER RD.
 P1 2+094.329
 N 176449.1796
 E 784838.3528
 Δ 92°07'15"
 L 80.000
 T 128.625
 C 83.017
 E 35.280
 L.C. 115.211
 S.E. 3.82 RT.
 TRANS. LENGTH 30
 DESIGN SPEED 40km/h

COORDINATE VALUES ARE NAD 83 (1995) AND ARE DATUM ADJUSTED BY THE FACTOR OF 1.00009166 & TIED TO THE TORN.

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

PRESENT LAYOUT
 STA. 10+300 - STA. 10+600
 STA. 1+220 - STA. 1+503
 STA. 1+720 - STA. 1+764.829
 STA. 2+000 - STA. 2+260
 SCALE: 1:500





TYPE	YEAR	PROJECT NO.	SHEET NO.
REV.	04-13-2001	REVISED DRAINAGE EASEMENT ON TRACT #31. REVISED DISTANCE ON PROPOSED R.O.W. LT STA. 10+800. ADDED CARPORT TO TRACT #26	6
REV.	28-FEB-2003	REVISED ALIGNMENT & R.O.W. FOR SOUTH CIRCLE. REMOVE DRIVEWAY STA. 10+894. CLEANED UP DRAFTING. REVISED SOUTH CIRCLE DRIVEWAYS. ADDED TRACT #25 & LINED OUT TRACT #24 NOT USED.	
REV.	15-MAR-2004	RESTORED CONST. EASEMENT TRACT 26. HATCHED SLOPE EASEMENT TRACT 25. REVISED LOCATION OF TRACT 8024. RESTORED TRACT 24. ADDED CONST. EASEMENT FOR REMOVAL OF STRUCTURES TRACT 27. ADDED NOTE FOR CONST. EASEMENT ON TRACTS 15 & 20. ADDED TRACT 185.	

REV. 04-13-2001: REVISED DRAINAGE EASEMENT ON TRACT #31. REVISED DISTANCE ON PROPOSED R.O.W. LT STA. 10+800. ADDED CARPORT TO TRACT #26

REV. 28-FEB-2003: REVISED ALIGNMENT & R.O.W. FOR SOUTH CIRCLE. REMOVE DRIVEWAY STA. 10+894. CLEANED UP DRAFTING. REVISED SOUTH CIRCLE DRIVEWAYS. ADDED TRACT #25 & LINED OUT TRACT #24 NOT USED.

REV. 15-MAR-2004: RESTORED CONST. EASEMENT TRACT 26. HATCHED SLOPE EASEMENT TRACT 25. REVISED LOCATION OF TRACT 8024. RESTORED TRACT 24. ADDED CONST. EASEMENT FOR REMOVAL OF STRUCTURES TRACT 27. ADDED NOTE FOR CONST. EASEMENT ON TRACTS 15 & 20. ADDED TRACT 185.

END C.A. FENCE
10+888.000
24.300
10+896.818
(19.72')
10+896.818
(10.66')
3.040
15.931'
FL. 257.479
24.300
10+898.085
(19.72')
10.085
(33.09')

END C.A. FENCE
10+888.000
24.300
10+896.818
(19.72')
10+896.818
(10.66')
3.040
15.931'
FL. 257.479
24.300
10+898.085
(19.72')
10.085
(33.09')

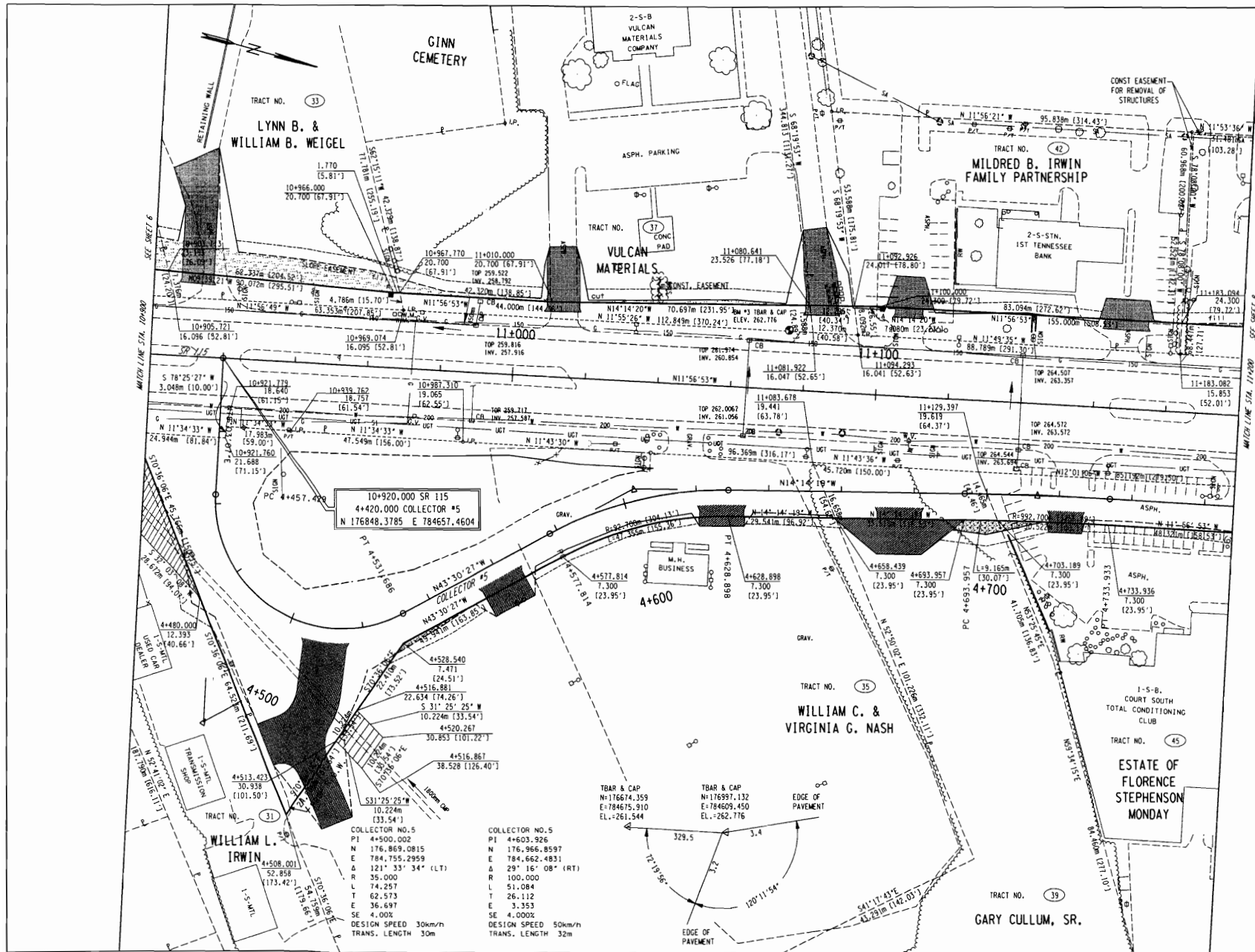
COORDINATE VALUES ARE NAD 83 (1995) AND ARE DATUM ADJUSTED BY THE FACTOR OF 1.00009166 & TIED TO THE TGN.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PRESENT LAYOUT
STA. 10+600 - STA. 10+900
STA. 2+620 - STA. 2+680
STA. 2+800 - STA. 2+919
STA. 3+000 - STA. 3+150
SCALE: 1"=50'

TYPE	YEAR	PROJECT NO.	SHEET NO.
ROW	2000	47026-2268-04	7

REV. 04-13-2001; REMOVED BRIDGE @ STA. 11+090.542 SR115 AND VULCAN ROAD. REVISED PROP. R.O.W. AND ADDED DRIVEWAYS. ADDED MOBILE HOMES TO TRACT #35
 REV. 28-FEB-2003; ADDED ALIGNMENT & DRIVEWAYS FOR COLLECTOR NO.5 & REVISED R.O.W. CLEANED UP DRAFTING.
 REV. 15-MAR-2004; REMOVED MOBILE HOMES TRACT 35. REVISED DRIVEWAYS ON TRACT 42 & 45. ADDED EXISTING PARKING SPACES TRACT 45. REVISED CONST. EASEMENT TRACT 42 FOR REMOVAL OF STRUCTURES.
 REV. 24-SEP-2004; ADD DRIVEWAYS TRACT 35 STA. 0+303.01 AND 4+560.71 MOVED DRIVEWAY FROM STA. 10+913 LT TO STA. 10+912 LT. AND WIDENED.



COORDINATE VALUES ARE NAD 83 (1995) AND ARE DATUM ADJUSTED BY THE FACTOR OF 1.00009166 & TIED TO THE TORN.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

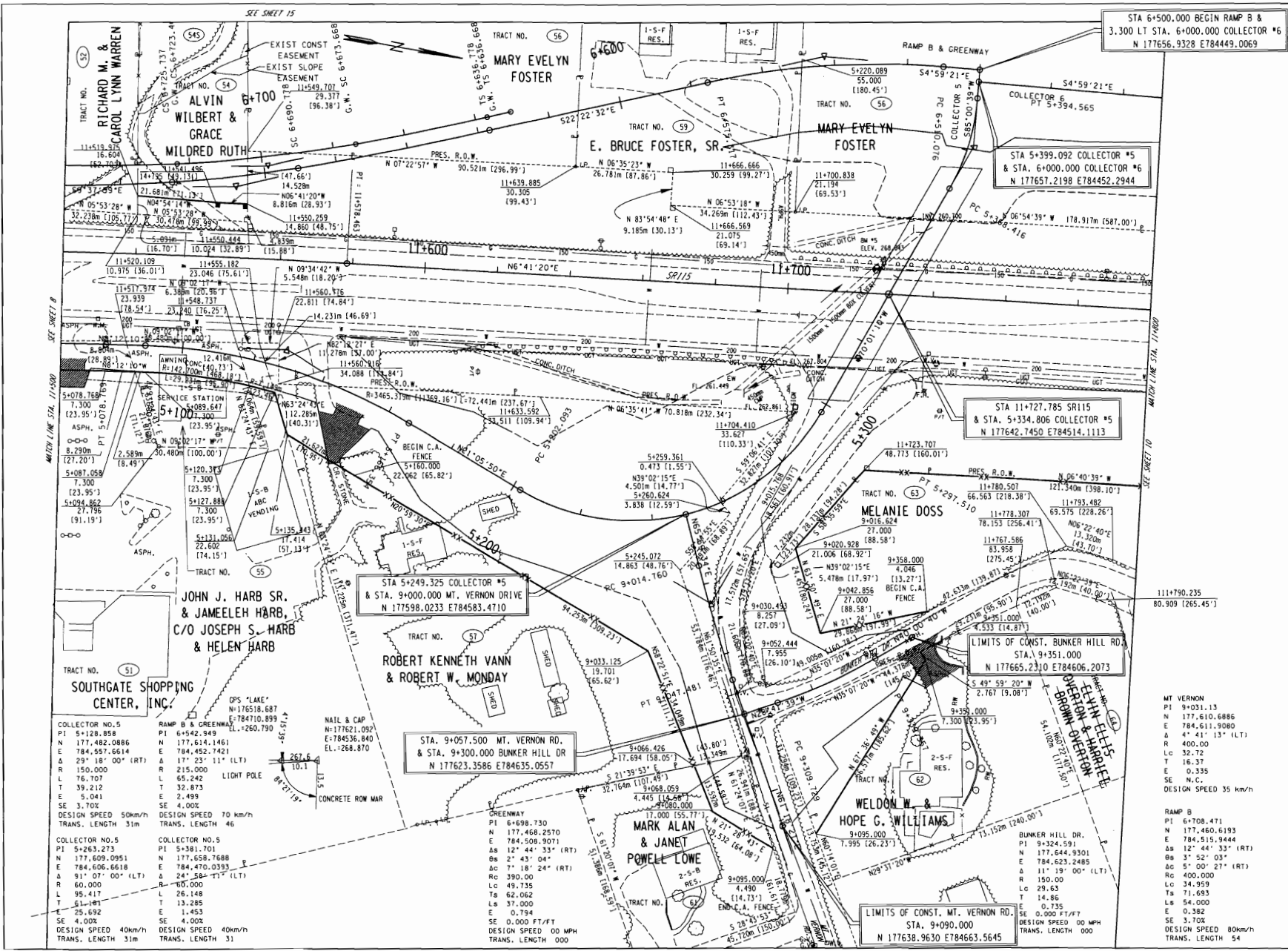
PRESENT LAYOUT
 STA. 10+900 - STA. 11+200
 STA. 4+420 - STA. 4+768
 SCALE: 1"=50'

COLLECTOR NO. 5
 PI 4+500.002
 N 176,869.0815
 E 784,755.2959
 Δ 121° 33' 34" (LT)
 R 35.000
 L 74.257
 T 62.573
 E 36.697
 SE 4.000
 DESIGN SPEED 30km/h
 TRANS. LENGTH 30m

COLLECTOR NO. 5
 PI 4+603.926
 N 176,966.8597
 E 784,662.4831
 Δ 29° 16' 08" (RT)
 R 100.000
 L 51.084
 T 26.112
 E 3.353
 SE 4.000
 DESIGN SPEED 50km/h
 TRANS. LENGTH 32m

COLLECTOR NO. 5
 PI 4+693.957
 N 176,997.132
 E 784,609.450
 EL. = 262.776

COLLECTOR NO. 5
 PI 4+733.933
 N 176,966.8597
 E 784,662.4831
 Δ 29° 16' 08" (RT)
 R 100.000
 L 51.084
 T 26.112
 E 3.353
 SE 4.000
 DESIGN SPEED 50km/h
 TRANS. LENGTH 32m



TYPE	YEAR	PROJECT NO.	SHEET NO.
ROW	2000	47026-2268-04	9

REV. 28-FEB-2003: ADDED ALIGNMENT & CURVE DATA FOR COLLECTOR NO. 5, NO. 6, MT. VERNON RD., RAMP B & GREENWAY. ADJUSTED R.O.W. & DRIVEWAYS ACCORDINGLY. REVISED MATCH LINES. SHIFTED SR15 TO THE WEST. OMITTED PREVIOUS ALIGNMENTS. REVISED DRAINAGE NEAR STA. 11+690. LINED OUT TRACT #54, NOT USED. REV. 15-MAR-2004: REVISED EXISTING ROW TRACT 54. ADDED TRACT 54S. MADE TRACTS 52 & 54S TOTAL TAKES. TRACT 57 MOVED PROP. R.O.W. 2M EAST AND ELIMINATED CONST. EASEMENT. ADDED DRIVEWAY TRACT 62.

SEE SHEET 15
SEE SHEET 8
WATER LINE STA. 11+500

SEE SHEET 10
MATCH LINE STA. 11+800

COLLECTOR NO. 5
PI 5+128.858
N 177,482.0886
E 784,557.6614
A 29° 18' 00" (RT)
R 150.000
L 76.707
T 39.212
E 5.041
SE 3.702
DESIGN SPEED 50km/h
TRANS. LENGTH 31m

COLLECTOR NO. 5
PI 5+263.273
N 177,609.0951
E 784,606.6618
A 91° 07' 00" (LT)
R 60.000
L 95.417
T 51.149
E 25.692
SE 4.002
DESIGN SPEED 40km/h
TRANS. LENGTH 31m

COLLECTOR NO. 5
PI 5+381.701
N 177,609.0951
E 784,470.0393
A 24° 58' 00" (LT)
R 60.000
L 26.148
T 13.285
E 1.453
SE 4.002
DESIGN SPEED 40km/h
TRANS. LENGTH 31

RAMP B & GREENWAY
LL-260.790
N 177,614.1461
E 784,522.7421
A 17° 23' 11" (LT)
R 215.000
L 65.242
T 32.873
E 2.499
SE 4.002
DESIGN SPEED 70 km/h
TRANS. LENGTH 46

OPS "LAKE"
E-784710.899
N-1776518.687
LL-260.790

NAIL & CAP
E-784536.840
N-177621.092
EL-268.870

CONCRETE ROW MAR
267.6
10.1
87° 17' 19"

STA 5+249.325 COLLECTOR #5
& STA. 9+000.000 MT. VERNON DRIVE
N 177598.0233 E784583.4710

STA. 9+057.500 MT. VERNON RD.
& STA. 9+300.000 BUNKER HILL DR
N 177623.3586 E784635.0557

MARK ALAN
POWELL LOWE

WELDON W. &
HOPE G. WILLIAMS

COLLECTOR NO. 5
PI 6+698.730
N 177,468.2570
E 784,508.9071
A 12° 44' 33" (RT)
B 2° 43' 04"
A 7° 18' 24" (RT)
R 390.000
L 49.735
T 62.062
L 37.000
E 0.794
SE 0.000 FT/FT
DESIGN SPEED 00 MPH
TRANS. LENGTH 000

COLLECTOR NO. 5
PI 9+095.000
N 177,638.9630
E 784,663.5645
A 0° 00' 00" (RT)
R 0.000
L 0.000
T 0.000
E 0.000
SE 0.000 FT/FT
DESIGN SPEED 00 MPH
TRANS. LENGTH 000

STA 5+399.092 COLLECTOR #5
& STA. 6+000.000 COLLECTOR #6
N 177657.2198 E784452.2944

STA 11+727.785 SR15
& STA. 5+334.806 COLLECTOR #5
N 177642.7450 E784514.1113

LIMITS OF CONST. BUNKER HILL RD.
STA. 9+351.000
N 177665.2310 E784606.2073

LIMITS OF CONST. MT. VERNON RD.
STA. 9+090.000
N 177638.9630 E784663.5645

MT VERNON
PI 9+031.13
N 177,610.6886
E 784,611.9080
A 4° 41' 15" (LT)
R 400.000
Lc 32.72
T 16.37
E 0.335
SE 4.002
DESIGN SPEED 35 km/h

RAMP B
PI 6+708.471
N 177,460.6193
E 784,515.9444
A 12° 44' 33" (RT)
B 3° 52' 03"
A 11° 19' 00" (LT)
R 150.000
Lc 29.63
T 14.86
E 0.735
SE 0.000 FT/FT
DESIGN SPEED 00 MPH
TRANS. LENGTH 000

BUNKER HILL DR.
PI 9+324.591
N 177,644.9301
E 784,623.2485
A 11° 19' 00" (LT)
R 150.000
Lc 29.63
T 14.86
E 0.735
SE 0.000 FT/FT
DESIGN SPEED 00 MPH
TRANS. LENGTH 000

COLLECTOR NO. 5
PI 11+789.501
N 177,783.482
E 785,153.2286
A 110° 22' 40"
R 43.701
Lc 13.300
T 13.300
E 0.437

COLLECTOR NO. 5
PI 11+778.307
N 177,783.482
E 785,153.2286
A 110° 22' 40"
R 43.701
Lc 13.300
T 13.300
E 0.437

COLLECTOR NO. 5
PI 11+767.586
N 177,783.482
E 785,153.2286
A 110° 22' 40"
R 43.701
Lc 13.300
T 13.300
E 0.437

COLLECTOR NO. 5
PI 11+723.707
N 177,783.482
E 785,153.2286
A 110° 22' 40"
R 43.701
Lc 13.300
T 13.300
E 0.437

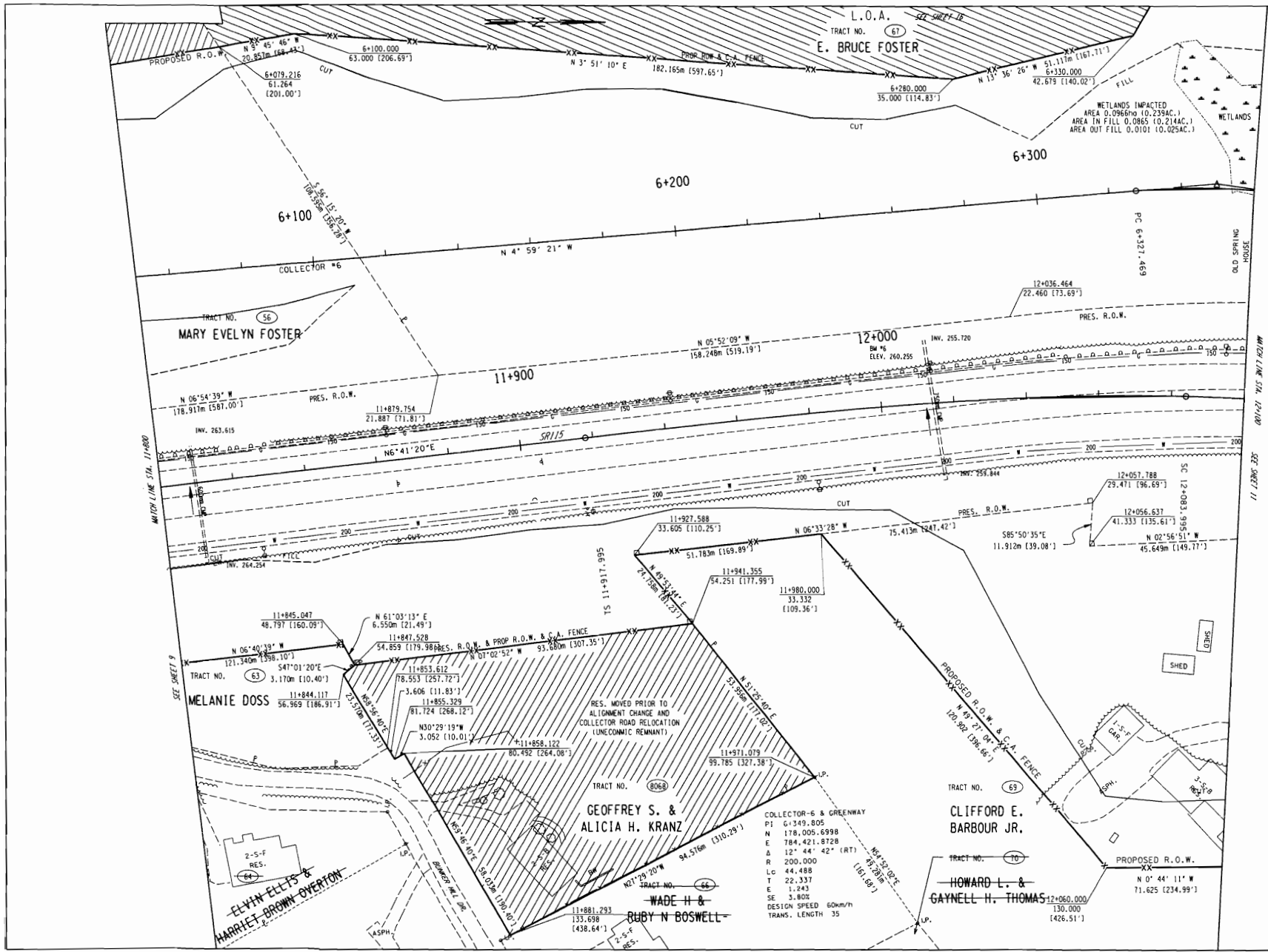


COORDINATE VALUES ARE NAD 83 (1995) AND ARE DATUM ADJUSTED BY THE FACTOR OF 1.00009166 & TIED TO THE TURN.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PRESENT
LAYOUT

STA. 11+500 - STA. 11+800
STA. 5+067 - STA. 5+399.092
STA. 9+000 - STA. 6+705
STA. 9+000 - STA. 9+090
STA. 9+300 - STA. 9+351
STA. 9+000 - STA. 6+000
SCALE: 1:500



TYPE	YEAR	PROJECT NO.	SHEET NO.
ROW	2000	47026-2268-04	10

REV. 04-13-2001: REVISE SLOPE LIMITS COLLECTOR NO. 6
 REV. 05-15-2001: REMOVED RETAINING WALL STA 111+920 TO STA. 111+980 ADJUSTED PROP. R.O.W. & SLOPE LIMITS ACCORDINGLY. ADDED TRACTS 66 & 70.
 REV. 28-FEB-2003: SHIFTED SR115 TO THE WEST. ADDED COLLECTOR NO. 6. REVISED R.O.W. LINED OUT TRACTS 64, 66 AND 70, NOT USED. RENUMBERED TRACT 68 TO 8068.
 REV. 15-MAR-2004: CROSS HATCHED LOSS OF ACCESS TRACT 67. CROSS HATCHED TRACT 8068 AS UNECONOMIC REMNANT AND LABELED HOUSE AS REMOVED PRIOR TO ALIGNMENT CHANGE.



COORDINATE VALUES ARE NAD 83 (1995) AND ARE DATUM ADJUSTED BY THE FACTOR OF 1.0009166 & TIED TO THE TORN.

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

PRESENT LAYOUT

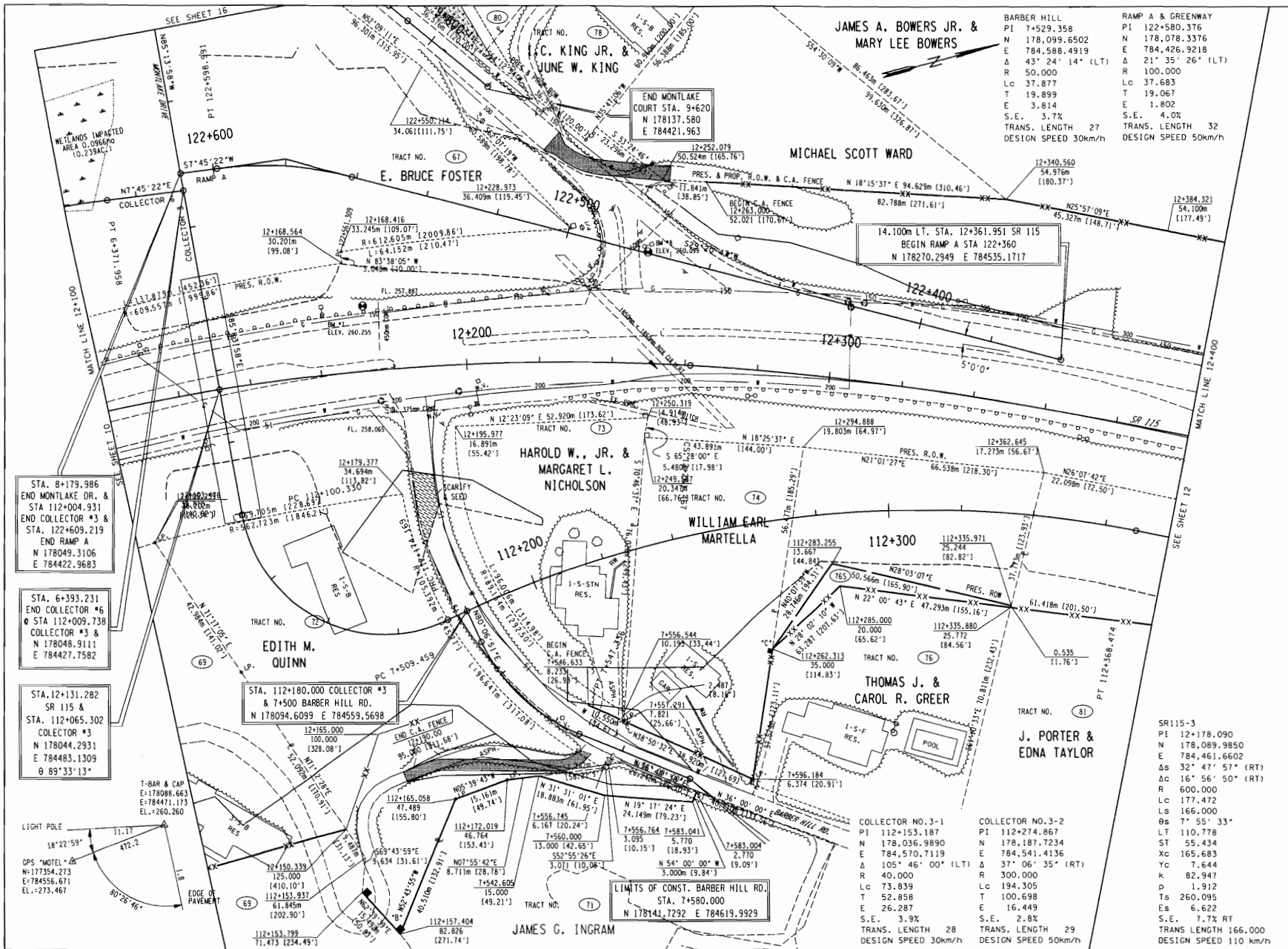
STA. 11+800 - STA. 12+100
 STA. 6+050 - STA. 6+360
 SCALE: 1:500

COLLECTOR-6 & GREENWAY
 PI 6+349.805
 N 178.005.6998
 E 784.421.8728
 Δ 12° 44' 42" (RT)
 R 200.000
 Lc 44.488
 T 22.337
 E 1.243
 SE 3.802
 DESIGN SPEED 60km/h
 TRANS. LENGTH 35

MATCH LINE STA. 11+800

SEE SHEET 9

MATCH LINE STA. 12+100



TYPE	YEAR	PROJECT NO.	SHEET NO.
ROW	2000	47026-2268-04	11

REV. 28-FEB-2003; SHIFTED SR115 TO THE WEST. REVISED ALIGNMENT FOR COLLECTOR NO. 6, NO. 3 & MONTLAKE DR. REVISED MATCH LINES. ADJUSTED R.O.W. & DRIVEWAYS ACCORDINGLY. REV. 15-MAR-2004; ADDED TRACT 76S. REVISED ACCESS CONTROL FENCE TRACT 72. RESTORED ACQUISITION TO PRIOR CONDITION TRACT 78 & 80.



COORDINATE VALUES ARE NAD 83 (1995) AND ARE DATUM ADJUSTED BY THE FACTOR OF 1.00009166 & TIED TO THE TGRN.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PRESENT LAYOUT
 STA. 12+100 - STA. 12+400
 STA. 112+004.931 - STA. 112+378
 STA. 122+360 - STA. 122+609.219
 STA. 7+500 - STA. 7+580
 STA. 6+360 - STA. 6+393.231
 STA. 9+585 - STA. 9+620
 SCALE: 1:500

STA. 8+179.986
END MONTLAKE DR. &
STA 112+004.931
END COLLECTOR #3 &
STA. 122+609.219
END RAMP A
N 178049.3106
E 784422.9683

STA. 6+393.231
END COLLECTOR #6
STA 112+009.738
COLLECTOR #3 &
N 178048.9111
E 784427.7582

STA. 12+131.282
SR 115 &
STA. 112+065.302
COLLECTOR #3
N 178044.2931
E 784483.1309
@ 89°33'13"

T-BAR & CAP
E-178088.663
E-784471.173
EL.-260.260

LIGHT POLE
18°22'59"
472.2

STA. 112+180.000 COLLECTOR #3
& 7+500 BARBER HILL RD.
N 178094.6099 E 784559.5698

LIMITS OF CONST. BARBER HILL RD.
STA. 7+500.000
N 178141.7292 E 784619.9329

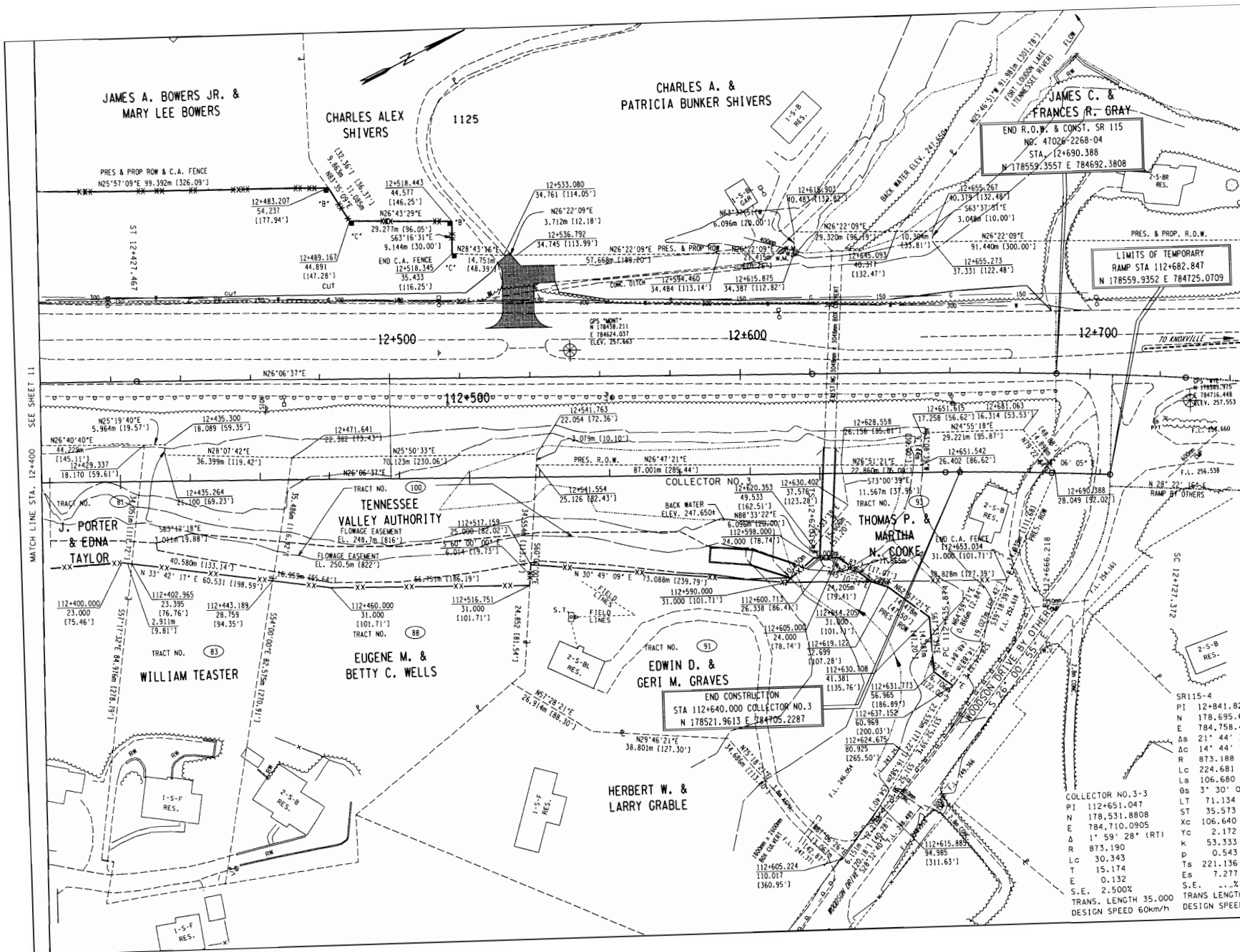
COLLECTOR NO. 3-1
PI 112+153.187
N 178,036.9890
E 784,570.7119
L 105° 46' 00" (LT)
R 40,000
Lc 73,839
T 52,858
E 26,287
S.E. 3.9%TRANS. LENGTH 28
DESIGN SPEED 30km/h

COLLECTOR NO. 3-2
PI 112+274.867
N 178,187.7234
E 784,541.4136
L 37° 06' 35" (RT)
R 300,000
Lc 194,305
T 100,698
E 16,449
S.E. 2.8%TRANS. LENGTH 29
DESIGN SPEED 50km/h

SR115-3
PI 12+178.090
N 178,089.9850
E 784,461.6602
AS 32° 47' 51" (RT)
AC 16° 56' 50" (RT)
R 600,000
Lc 177,472
Ls 166,000
Ts 110.778
ST 55.434
Xc 165.683
Yc 7.644
K 82.947
p 1.912
Ts 260.095
Es 6.622
S.E. 7.7% RT
TRANS LENGTH 166,000
DESIGN SPEED 110 km/h

TYPE	YEAR	PROJECT NO.	SHEET NO.
ROW	2000	47026-2268-04	12

REV. 28-FEB-2003: REVISED ALIGNMENT FOR SR115 TO FACILITATE TIE-IN WITH ADJACENT PROJECT. MODIFIED R.O.W. ACCORDINGLY.
 REV. 15-MAR-2004: RESTORED TRACT 93 TO TOTAL TAKE.
 REV. 24-SEP-2004: REVISED PROP R.O.W. TRACT 91. ADDED I.V.A. FLOWAGE EASEMENT TRACT #100. ADDED RETAINING WALL STA 12+600'.



SR115-4
 PI 12+841.828
 N 178,695.6117
 E 784,758.4875
 Δ 21° 44' 34"
 Δ 14° 44' 34"
 R 873.188
 Lc 224.681
 Ls 106.680
 Δ 3° 30' 00"
 LT 71.134
 ST 35.573
 Xc 106.640
 Yc 2.172
 Δ 1° 59' 28" (RT)
 R 873.190
 Lc 30.343
 T 15.174
 E 0.132
 S.E. 2.500X
 TRANS. LENGTH 35.000
 DESIGN SPEED 60km/h

SR115-4
 PI 12+841.828
 N 178,695.6117
 E 784,758.4875
 Δ 21° 44' 34"
 Δ 14° 44' 34"
 R 873.188
 Lc 224.681
 Ls 106.680
 Δ 3° 30' 00"
 LT 71.134
 ST 35.573
 Xc 106.640
 Yc 2.172
 Δ 1° 59' 28" (RT)
 R 873.190
 Lc 30.343
 T 15.174
 E 0.132
 S.E. 2.500X
 TRANS. LENGTH 35.000
 DESIGN SPEED 100

COORDINATE VALUES ARE NAD 83 (1995) AND ARE DATUM ADJUSTED BY THE FACTOR OF 1.0009166 & TIED TO THE T.G.R.N.

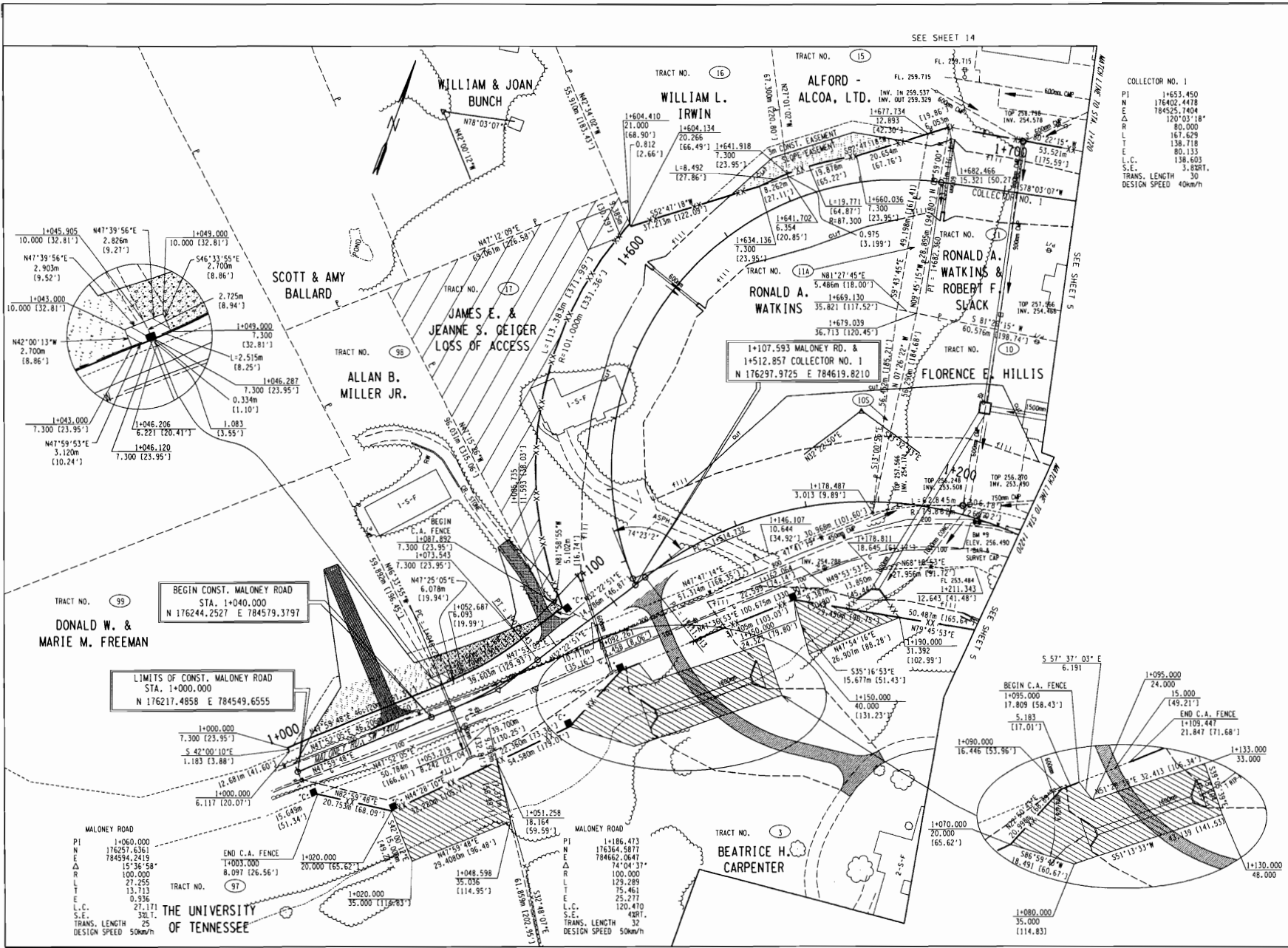
STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

PRESENT LAYOUT

STA. 12+400 - STA. 12+690.388
 STA. 12+140 - STA. 12+682.847
 SCALE: 1:500

MATCH LINE STA. 12+400 SEE SHEET 11

TYPE	YEAR	PROJECT NO.	SHEET NO.
ROW	2000	47026-2268-04	13



COLLECTOR NO. 1
 P.I. 1+653.450
 N 176402.4478
 184925.1404
 120'03'18"
 80.000
 161.629
 138.718
 80.133
 138.603
 S.E. 3.9821.
 TRANS. LENGTH 30
 DESIGN SPEED 40km/h

REV. 03-01-2002 REVISED ROW
 DRAINAGE, SLOPE AND CONST EASEMENTS
 TRACTS 17 & 98, REMOVED DRIVEWAY
 TRACT 17, AND MODIFIED DRIVEWAY
 TRACT 98, PROVIDED A 15m OPENING
 IN C.A. FENCE AT TRACT 3.
 REV. 28-FEB-2003; ADDED DRIVEWAY
 STA. 1+080 & PIPE, REVISED DRAINAGE
 EASEMENT.
 REV. 15-MAR-2003; ADDED PROP
 DRIVEWAY STA 1+030 L.I.

BEGIN CONST. MALONEY ROAD
 STA. 1+040.000
 N 176244.2527 E 784579.3797

LIMITS OF CONST. MALONEY ROAD
 STA. 1+000.000
 N 176217.4858 E 784549.6555

TRACT NO. 99
 DONALD W. &
 MARIE M. FREEMAN

TRACT NO. 97
 THE UNIVERSITY
 OF TENNESSEE

PI 1+060.000
 N 176257.6361
 E 784594.2419
 Δ 15'36"58"
 100.000
 L 27.255
 T 13.713
 E 0.936
 L.C. 27.171
 S.E. 30.1
 TRANS. LENGTH 25
 DESIGN SPEED 50km/h

PI 1+186.473
 N 176364.5877
 E 784662.0847
 Δ 74'04'31"
 100.000
 L 129.289
 T 75.461
 E 25.277
 L.C. 120.470
 S.E. 42.817
 TRANS. LENGTH 32
 DESIGN SPEED 50km/h

BEGIN C.A. FENCE
 1+095.000
 17.809 (58.43')
 5.183
 (17.01')
 1+090.000
 16.446 (53.96')

END C.A. FENCE
 15.000
 149.21'
 21.847 (71.68')

1+130.000
 48.000



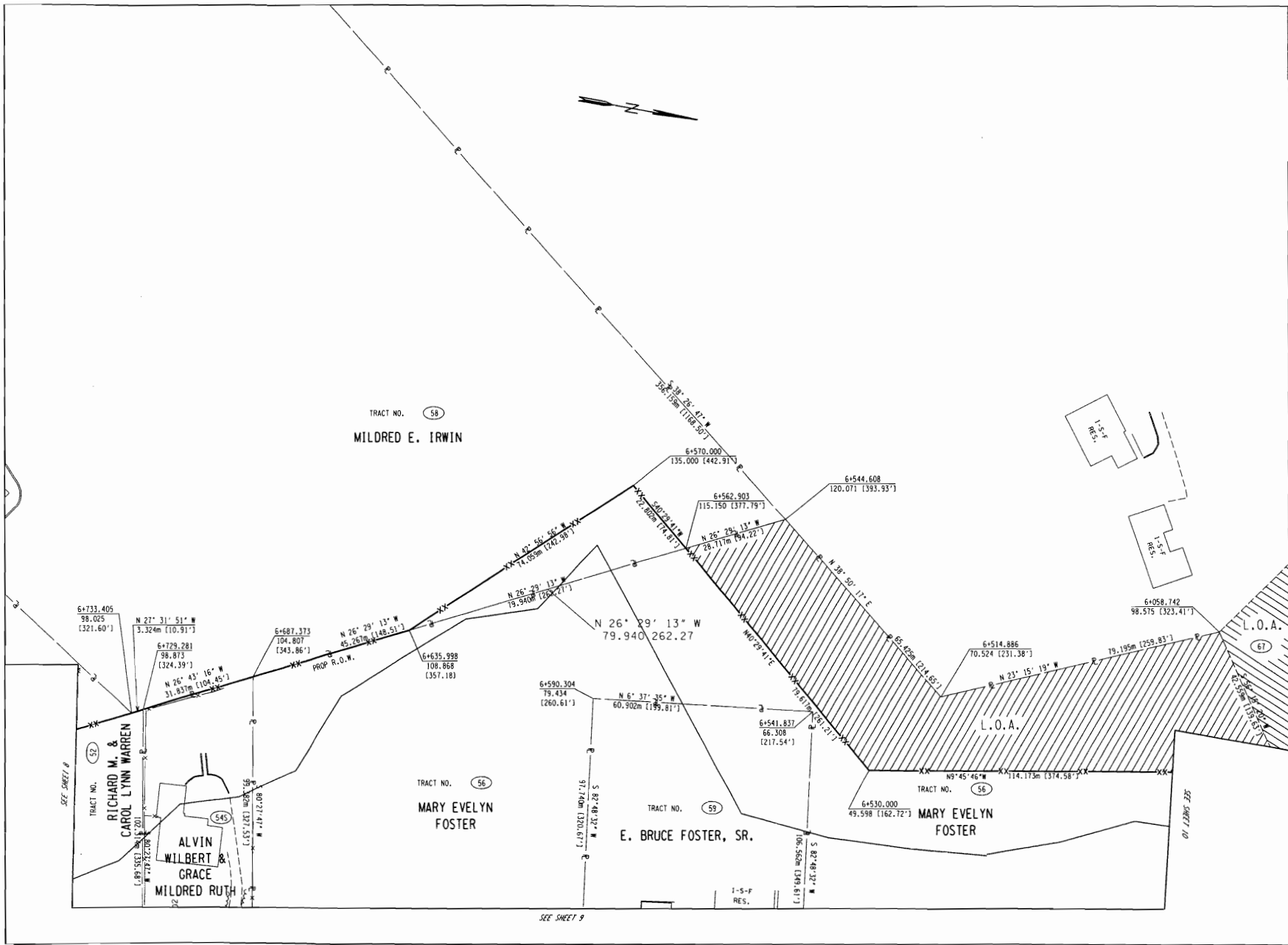
COORDINATE VALUES ARE NAD 83
 (1995) AND ARE DATUM ADJUSTED
 BY THE FACTOR OF 1.00009166 &
 TIED TO THE TGN.

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

PRESENT
 LAYOUT
 STA. 1+000 - STA. 1+220
 STA. 1+500 - STA. 1+720
 SCALE: 1:500

TYPE	YEAR	PROJECT NO.	SHEET NO.
ROW	2000	47026-2268-04	15

REV. 28-FEB-2003: ADDED NEW SHEET TO ILLUSTRATE R.O.W. & SLOPE LIMITS. REPLACED OLD SHEET OF COLLECTOR NO. 2 & VULCAN RD.
 REV. 15-MAR-2004: RENAMED TRACT 54 TO 54S. REMOVED LOSS OF ACCESS FROM TRACTS 52 AND 54S AND MADE TOTAL TAKES. REMOVED SOUTH LOSS OF ACCESS FROM TRACT 56 AND MADE PROP R.O.W.



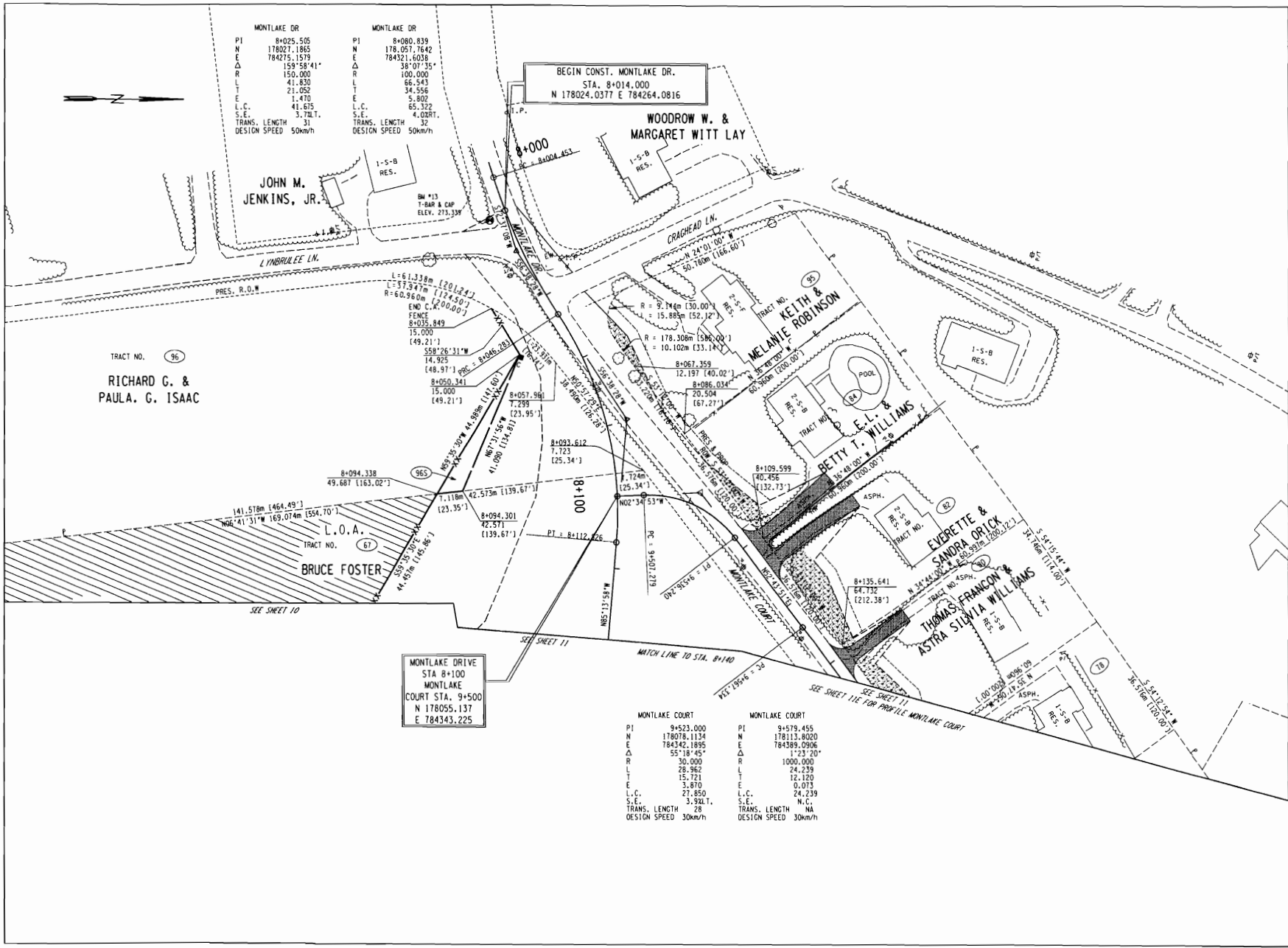
COORDINATE VALUES ARE NAD 83 (1995) AND ARE DATUM ADJUSTED BY THE FACTOR OF 1.00009166 & TIED TO THE 10RN.

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION

PRESENT LAYOUT

STA. 11+500 - STA. 11+800

SCALE: 1:500



MONTLAKE DR		MONTLAKE DR	
PI	8+025.505	PI	8+080.839
N	178027.1865	N	178.057.7642
E	784275.1579	E	784321.6039
Δ	159°58'41"	Δ	38°07'35"
R	150.000	R	100.000
L	41.830	L	66.243
T	21.052	T	34.556
L.C.	41.675	L.C.	5.802
S.E.	3.721	S.E.	65.322
TRANS. LENGTH	31	TRANS. LENGTH	4.02RT.
DESIGN SPEED	50km/h	DESIGN SPEED	50km/h

BEGIN CONST. MONTLAKE DR.
STA. 8+014.000
N 178024.0377 E 784264.0816

MONTLAKE DRIVE
STA 8+100
MONTLAKE
COURT STA. 9+500
N 178055.137
E 784343.225

MONTLAKE COURT		MONTLAKE COURT	
PI	9+523.000	PI	9+579.455
N	178078.1134	N	178113.8020
E	784342.1895	E	784389.0906
Δ	55°18'45"	Δ	172°21'00"
R	30.000	R	1000.000
L	28.962	L	24.239
T	15.721	T	12.120
E	3.870	E	0.073
L.C.	27.850	L.C.	24.239
S.E.	3.941	S.E.	N.C.
TRANS. LENGTH	28	TRANS. LENGTH	NA
DESIGN SPEED	30km/h	DESIGN SPEED	30km/h

TYPE	YEAR	PROJECT NO.	SHEET NO.
ROW	2000	47026-2268-04	16

REV. 04-13-2001: REVISED SLOPE EASEMENT AND CONST. EASEMENT ON TRACT #95
REV. 28-FEB-2003: REMOVED OLD ALIGNMENT FOR MT. VERNON RD.
REV. 15-MAR-2004: RESTORED ACQUISITION TO PRIOR CONDITION TRACTS 78 AND 80. ADDED TRACT 95S.



COORDINATE VALUES ARE NAD 83 (1995) AND ARE DATUM ADJUSTED BY THE FACTOR OF 1.00009166 & TIED TO THE TORN.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PRESENT LAYOUT
STA. 8+014 - STA. 8+140
STA. 9+500 - STA. 9+585
SCALE: 1:500

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

KNOX COUNTY

U.S.-129 (S.R. 115, ALCOA HIGHWAY)
FROM: WOODSON ROAD
TO: SOUTH END OF TENNESSEE RIVER BRIDGE

R.O.W.

STATE HIGHWAY NO. 115 F.A.H.S. NO. N/A

TENN.	YEAR	SHEET NO.
	2002	1
FED. AID PROJ. NO.	STP-NHE-115(27)	
STATE PROJ. NO.		



Index Of Sheets

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2-2B	TYPICAL SECTIONS
3-3A	PROPERTY MAP
4-12	PRESENT LAYOUT & PROPOSED R.O.W.
4A-12A	PROPOSED LAYOUT
4B-12B	PROFILES
13-19	SIDEROAD PROFILES
20-22	PRIVATE DRIVE PROFILES
23-24	DRAINAGE MAP
25	CULVERT CROSS-SECTIONS
	ROADWAY CROSS-SECTIONS



NO EXCLUSIONS
NO EQUATIONS

STP-NHE-115(27)
END PROJECT 0000-0000-00
(R.O.W.) STA. 185+46.63

STP-NHE-115(27)
BEGIN PROJECT 0000-0000-00
STA. 109+12.81 (R.O.W.)

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED MARCH 1, 1995 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT

TDOT TRANS. MGR. 1 CLIFFORD STEWART
DESIGNED BY: ROBERT G. CAMPBELL & ASSOCIATES L.P.
DESIGNER: JAMES R. GARRETT, P.E.
P.E. NO.: 47026-1270-14

PIN 100241.03

R.O.W. LENGTH = 1.446 MILES

SCALE: 1" = 2000'

TRAFFIC DATA	
ADT (1999)	59,350
ADT (2019)	83,100
DHV (2019)	8,310
D	60 - 40
T (ADT)	5 %
T (DHV)	3 %
V	60 MPH

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: _____
DIVISION ADMINISTRATOR

DATE: _____

APPROVED: JHC Jones
DIRECTOR, DESIGN DIVISION

DATE: _____

APPROVED: James R. Garrett
COMMISSIONER

FOR PRELIMINARY FIELD REVIEW ONLY

DESIGN DIVISION

FILE NO.

11/15/01 09:03 AM STP-NHE-115(27)-R.O.W.-116.dgn 06/17/2002 03:55:06 PM

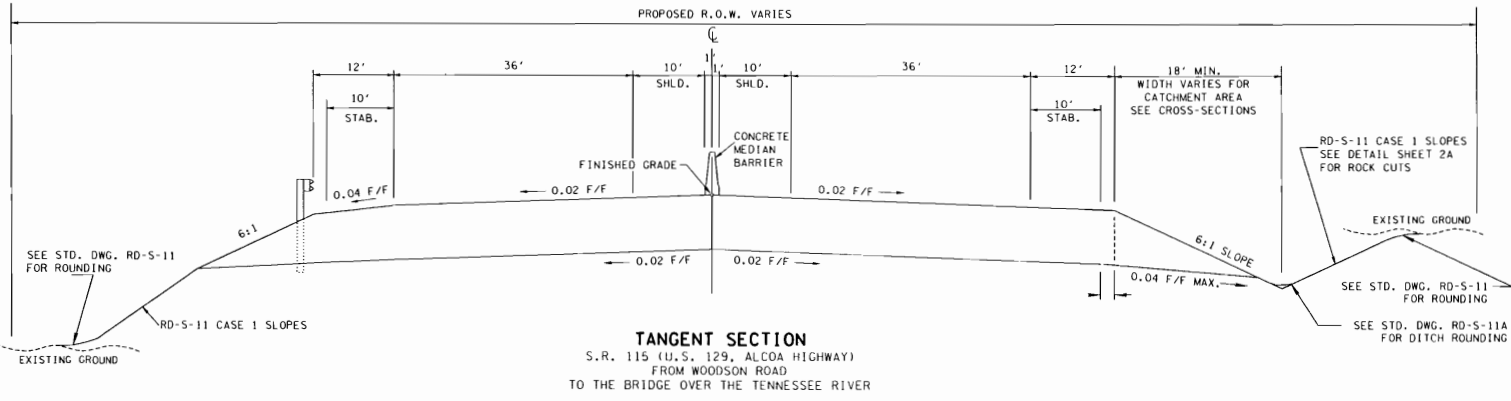
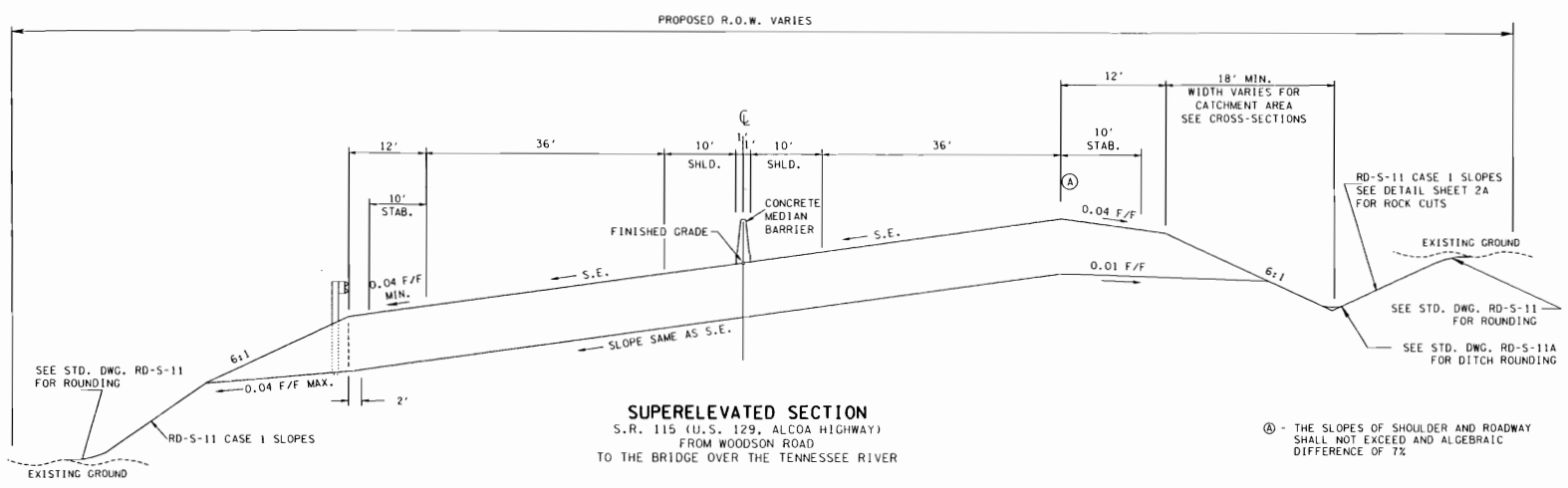


DESIGN DIVISION

FILE NO.

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2005	STP-NHE-115(29)	2

KNOX CO. S.R. 115 (ALCOA HWY)
47026-1270-14 (R.O.W.)



PROPOSED PAVEMENT SCHEDULE

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF PLANNING & DEVELOPMENT

TYPICAL
SECTIONS

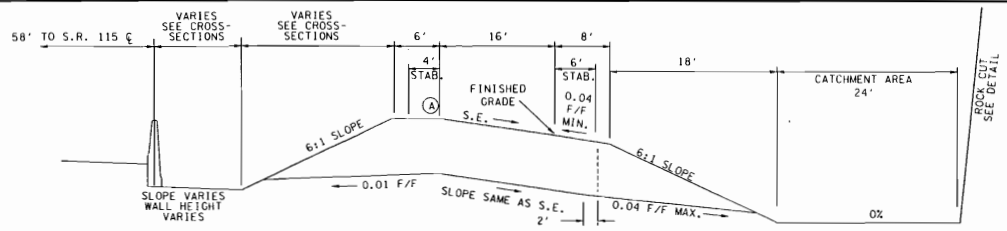
FOR PRELIMINARY FIELD REVIEW ONLY

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DESIGN DIVISION

FILE NO.

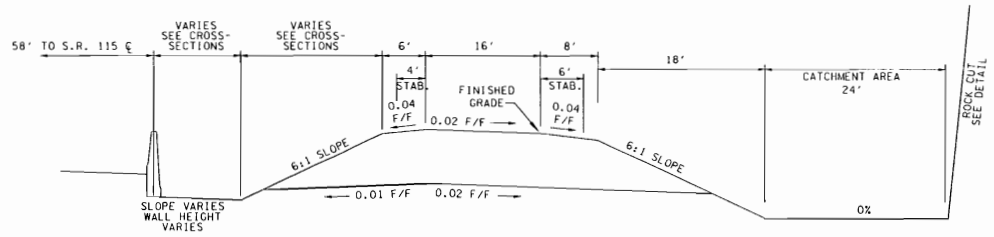


TYPICAL RAMP SUPERELEVATED SECTION
RAMP 'A'

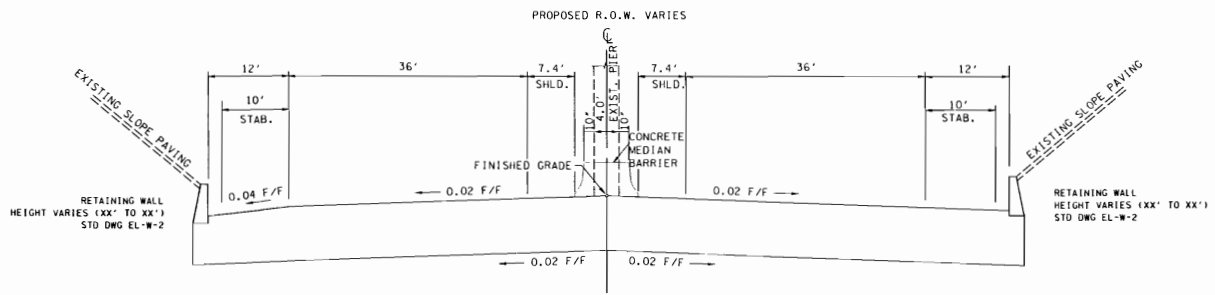
Ⓐ - THE SLOPES OF SHOULDER AND ROADWAY SHALL NOT EXCEED AND ALGEBRAIC DIFFERENCE OF 7%

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2003	STP-NHE-115(29)	2A

KNOX CO. S.R. 115 (ALCOA HWY)
47026-1270-14 (R.O.W.)



TYPICAL RAMP TANGENT SECTION
RAMP 'A'



TANGENT SECTION
S.R. 115 (U.S. 129, ALCOA HIGHWAY)
UNDER HOSPITAL DRIVE BRIDGE

FOR PRELIMINARY FIELD REVIEW ONLY

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF PLANNING & DEVELOPMENT

TYPICAL SECTIONS

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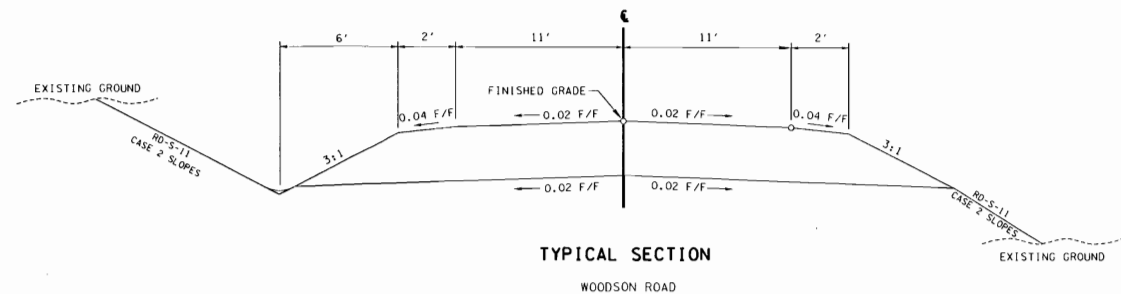
DESIGN DIVISION

FILE NO.

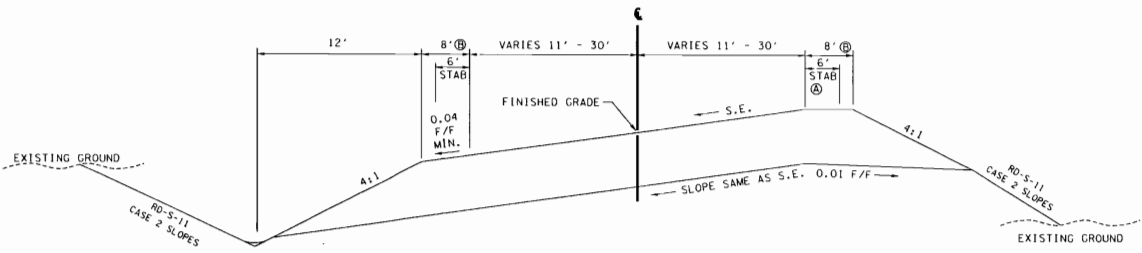
- Ⓐ - THE SLOPES OF SHOULDER AND ROADWAY SHALL NOT EXCEED AND ALGEBRAIC DIFFERENCE OF 7%
- Ⓓ - CHEROKEE TRAIL SHOULDER = 10'

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2003	STP-NHE-115(29)	2B

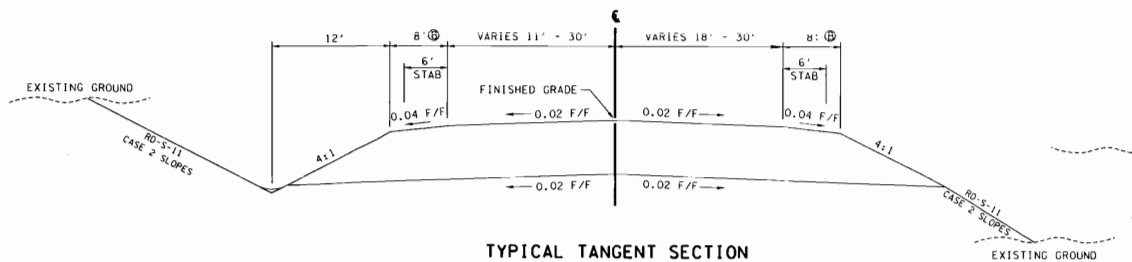
KNOX CO. S.R. 115 (ALCOA HWY)
47026-1270-14 (R.O.W.)



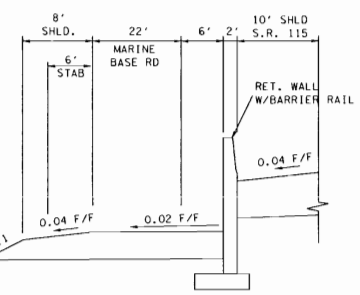
TYPICAL SECTION
WOODSON ROAD



TYPICAL SUPERELEVATED SECTION
HOSPITAL DRIVE
CHEROKEE TRAIL
MARINE BASE ROAD



TYPICAL TANGENT SECTION
HOSPITAL DRIVE
CHEROKEE TRAIL
MARINE BASE ROAD



**DETAIL OF MARINE BASE ROAD
PARALLEL TO S.R. 115**

FOR PRELIMINARY FIELD REVIEW ONLY

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF PLANNING & DEVELOPMENT

**TYPICAL
SECTIONS**

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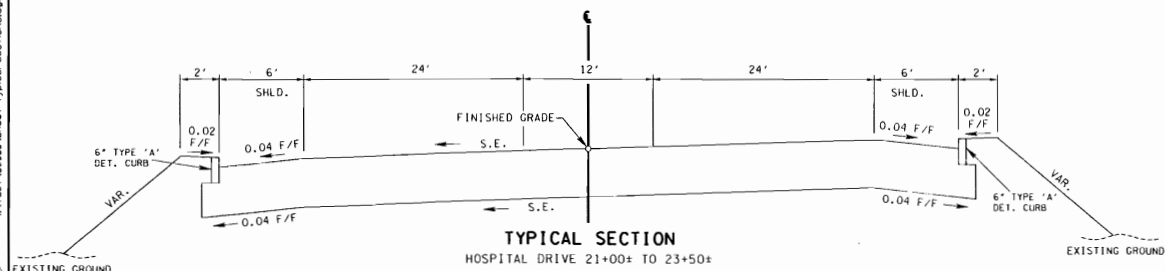
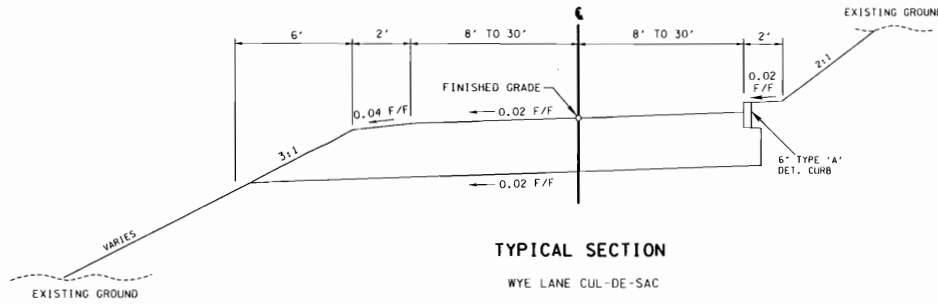
DESIGN DIVISION

FILE NO.

Ⓢ - THE SLOPES OF SHOULDER AND ROADWAY SHALL NOT EXCEED AND ALGEBRAIC DIFFERENCE OF 1%

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2003	STP-NHE-115(29)	2C

KNOX CO, S.R. 115 (ALCOA HWY)
47026-1270-14 (R.O.W.)



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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF PLANNING & DEVELOPMENT

TYPICAL
SECTIONS

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DESIGN DIVISION

FILE NO.

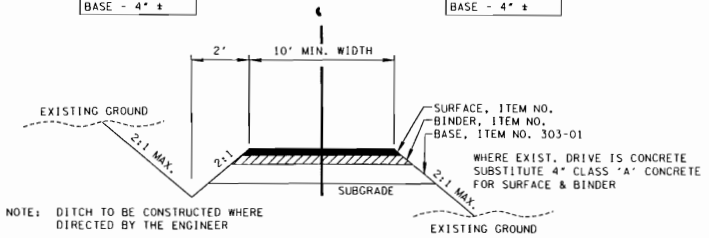
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R.O.W.	2003	STP-NHE-115129	20

KNOX CO. S.R. 115 (ALCOA HWY)
47026-1270-14 (R.O.W.)

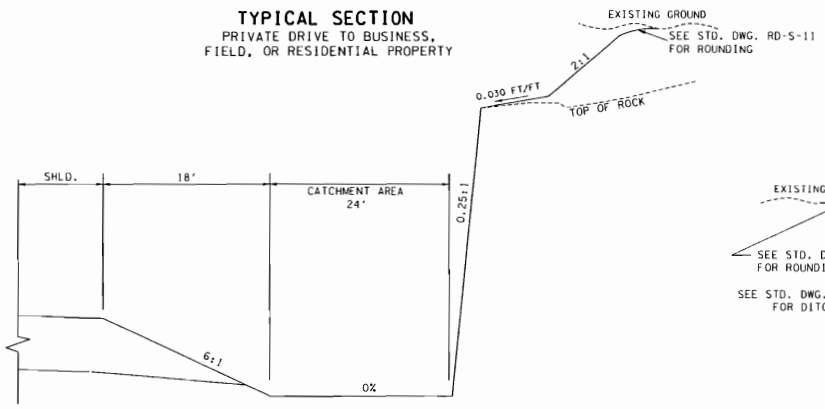
Ⓐ - THE SLOPES OF SHOULDER AND ROADWAY SHALL NOT EXCEED AND ALGEBRAIC DIFFERENCE OF 7%

BUSINESS
SURFACE - 1 1/2" ±
BINDER - 1 1/2" ±
BASE - 4" ±

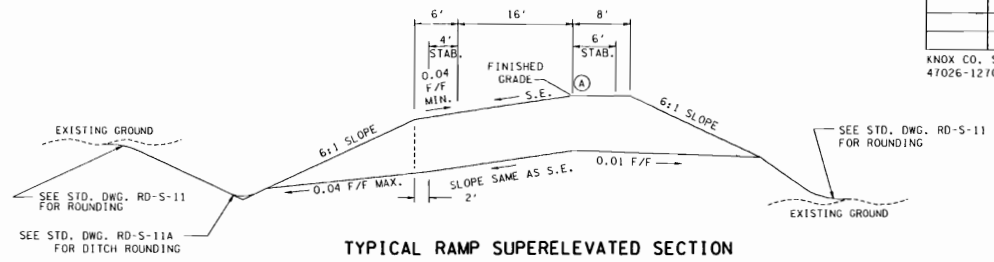
FIELD OR RESIDENTIAL
SURFACE - 1 1/2" ±
BINDER - NONE
BASE - 4" ±



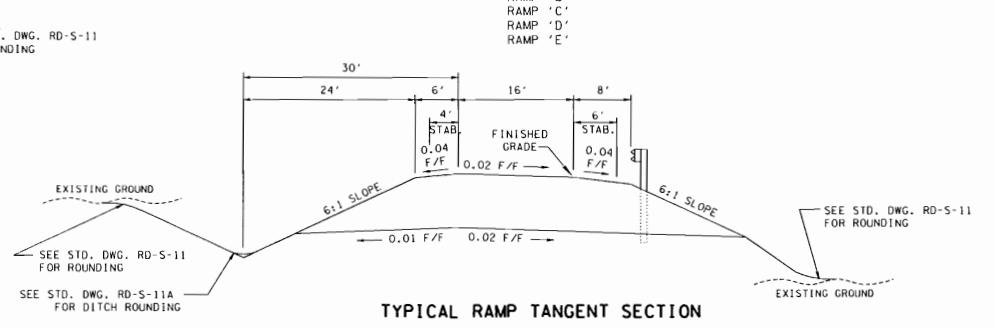
TYPICAL SECTION
PRIVATE DRIVE TO BUSINESS,
FIELD, OR RESIDENTIAL PROPERTY



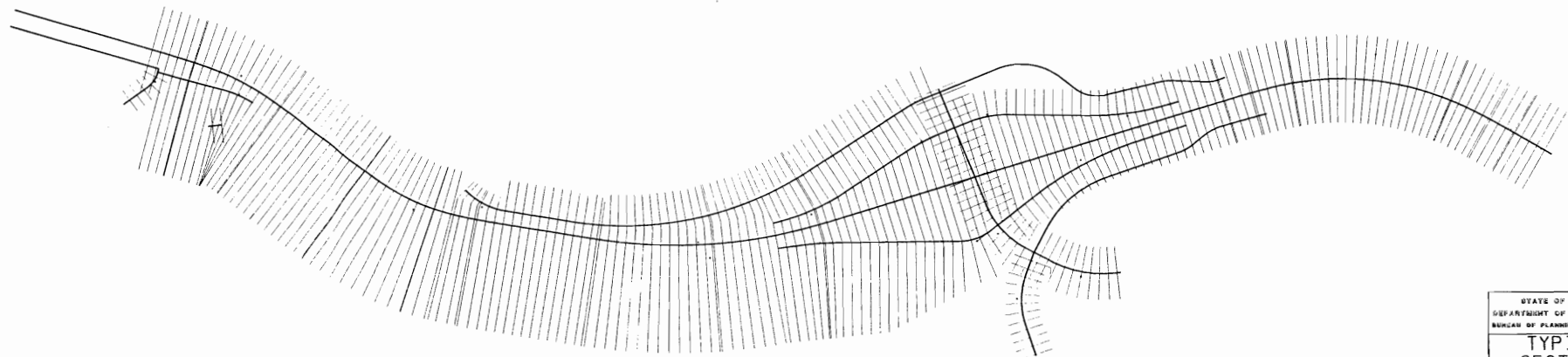
DETAIL OF ROCK CUT SLOPES



TYPICAL RAMP SUPERELEVATED SECTION



TYPICAL RAMP TANGENT SECTION



FOR PRELIMINARY FIELD REVIEW ONLY

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF PLANNING & DEVELOPMENT

**TYPICAL
SECTIONS
AND METHOD
OF CROSS-
SECTIONS**

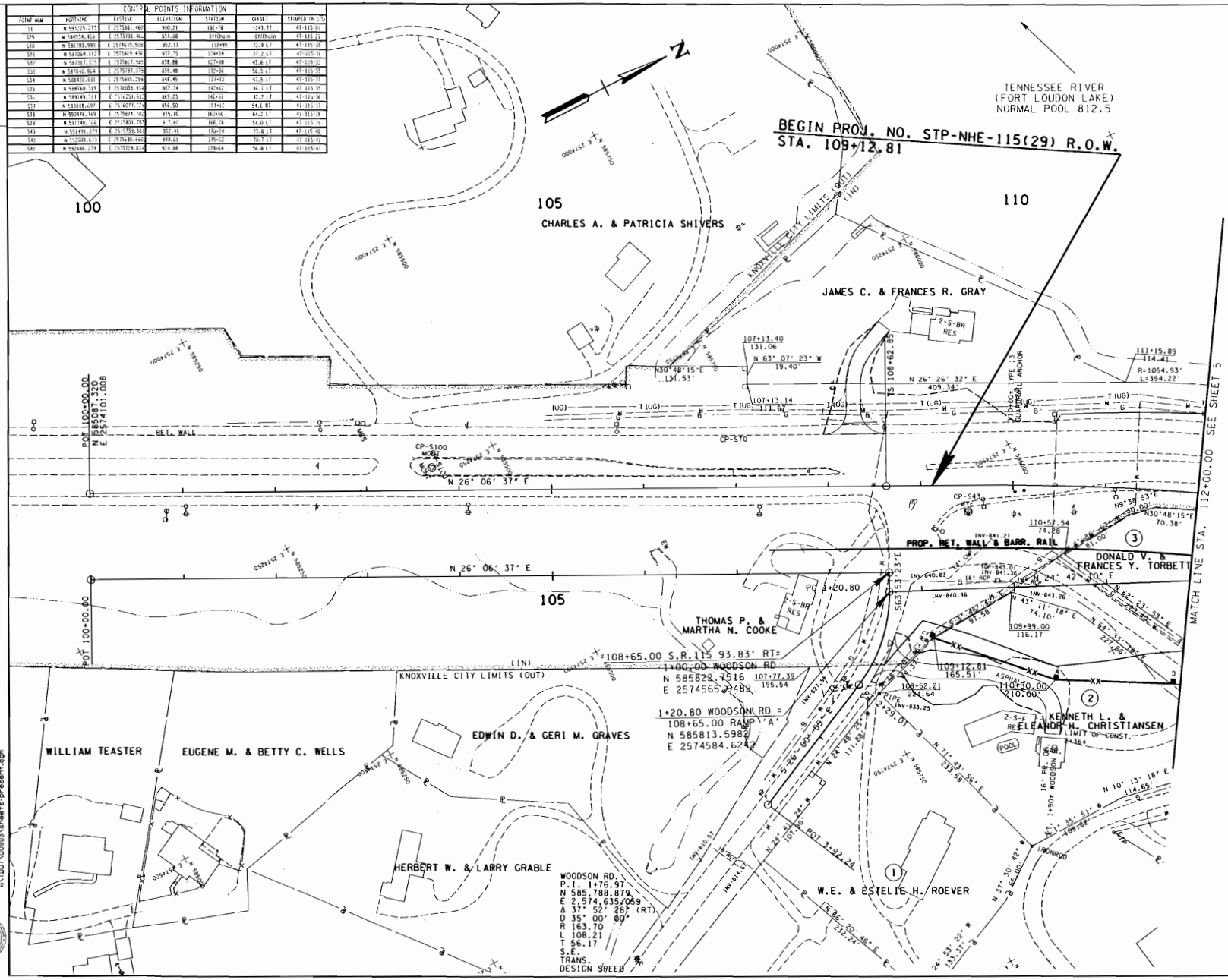
NOT TO SCALE

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DESIGN DIVISION

FILE NO.

POINT NO.	NORTHING	EASTING	ELEVATION	STATION	OFFSET	SHOWN ON LOTS
11	N 93225.71	E 257940.40	500.21	10+00	-20.71	47-115-21
12	N 94934.25	E 257931.84	511.68	10+00	0.00	47-115-21
13	N 96783.93	E 257443.52	522.13	10+29	22.93	47-115-21
14	N 98564.13	E 256949.84	532.59	11+04	32.41	47-115-21
15	N 99717.57	E 256417.50	538.80	12+08	43.13	47-115-21
16	N 98740.64	E 257391.22	539.88	13+06	54.12	47-115-21
17	N 98820.47	E 257405.24	540.40	13+12	61.13	47-115-21
18	N 98760.13	E 257408.22	542.24	13+12	66.13	47-115-21
19	N 98749.71	E 257261.41	543.25	14+07	72.13	47-115-21
20	N 98700.47	E 256271.24	544.50	15+12	74.87	47-115-21
21	N 98596.18	E 257104.22	539.30	16+00	84.13	47-115-21
22	N 98146.20	E 257489.23	532.40	16+74	94.13	47-115-21
23	N 98141.37	E 257393.30	532.40	17+04	104.13	47-115-21
24	N 97931.41	E 257385.40	540.40	18+02	114.13	47-115-21
25	N 97566.19	E 257378.40	544.80	19+04	124.13	47-115-21



TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2003	STP-NHE-115(29)	4

KNOX CO. S.R. 115 (ALCOA HWY)
47026-1270-14 (R.O.W.)

BEGIN PROJ. NO. STP-NHE-115(29) R.O.W.
STA. 109+12.81

TENNESSEE RIVER
(FORT LOUDON LAKE)
NORMAL BULK 812.5

FOR PRELIMINARY FIELD REVIEW ONLY

MATCH LINE STA. 112+00.00 SEE SHEET 5

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COORDINATE VALUES ARE NAD83(99) AND ARE DATUM ADJUSTED BY THE FACTOR 1.0001 & TIED TO THE IORN. ELEVATIONS ARE NAVD 1988

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF PLANNING & DEVELOPMENT

**PRESENT LAYOUT
&
PROPOSED R.O.W.**

STA. 109+12.81 TO STA. 112+00.00

SCALE: 1"=50'

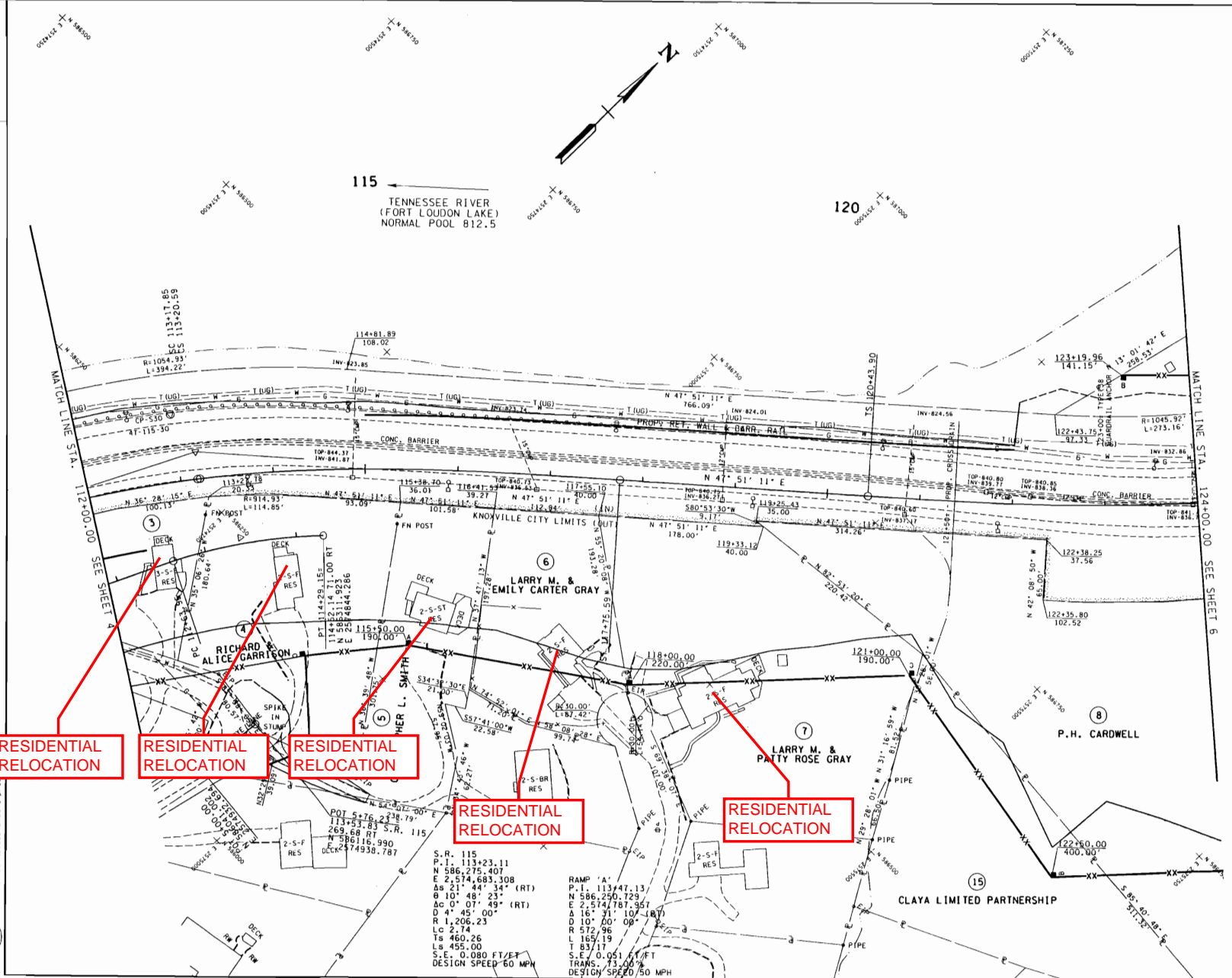
WOODSON RD.
P.I. 1+76.97
N 585788.87
E 2574635.069
Δ 37° 52' 28" (RT)
D 35' 00' 00"
R 163.70
L 108.21
T 56.17
S.E.
TRANS.
DESIGN SPEED

108+65.00 S.R. 115 93.83' RT =
1+00.00 WOODSON RD
N 585822.7516 107.77.39
E 2574565.9482 195.54
1+20.80 WOODSON RD =
108+65.00 RAMP "A"
N 585813.5982
E 2574584.6242

DESIGN DIVISION
FILE NO.

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2003	STP-NHE-115(29)	5

ANDR. CO. S.R. 115 (ALCOA HWY)
47026-1270-14 (R.O.W.)



FOR PRELIMINARY FIELD REVIEW ONLY

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S.R. 115
P.I. 113+23.11
N 586,275.407
E 2,574,683.308
Δθ 21° 44' 34" (RT)
θ 10° 48' 23"
Δθ 0° 07' 49" (RT)
D 4' 45" 00"
R 1,206.23
LC 2.74
TS 450.26
LS 455.00
S.E. 0.080 FT/FT
DESIGN SPEED 60 MPH

RAMP 'A'
P.I. 113+47.13
N 586,250.729
E 2,574,787.957
Δθ 16° 31' 10" (RT)
D 10' 00" 00"
R 572.96
LC 165.19
TS 455.17
LS 455.17
S.E. 0.051 FT/FT
TRANS. 73.00 FT
DESIGN SPEED 50 MPH

COORDINATE VALUES ARE NAD83(99) AND
ARE DATUM ADJUSTED BY THE FACTOR
1.0001 & TIED TO THE TORN.
ELEVATION AND 1988

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF HIGHWAYS & DEVELOPMENT

**PRESENT LAYOUT
&
PROPOSED R.O.W.**

STA. 112+00.00 TO STA. 124+00.00

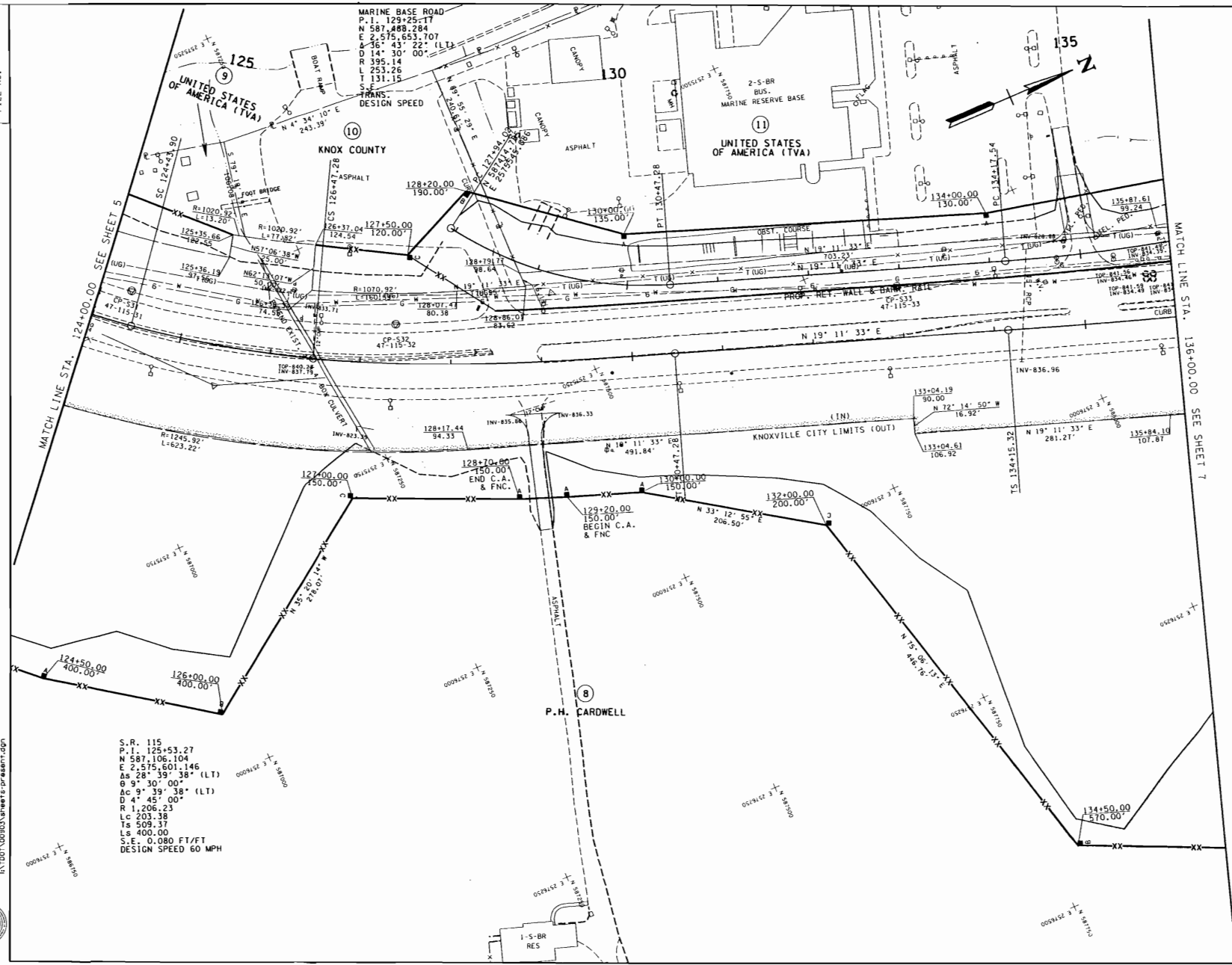
SCALE: 1"=50'

DESIGN DIVISION

FILE NO.

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2003	STP-NHE-115(29)	6

KNOX CO. S.R. 115 (ALCOA HWY)
47026-1270-14 R.O.W. 1



COORDINATE VALUES ARE NAD83(99) AND ARE DATUM ADJUSTED BY THE FACTOR 1.0001 & TIED TO THE TORN. ELEVATIONS ARE NAVD 1988

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF PLANNING & DESIGN

**PRESENT LAYOUT
&
PROPOSED R.O.W.**

STA. 124+00.00 TO STA. 136+00.00

SCALE: 1"=50'

FOR PRELIMINARY FIELD REVIEW ONLY

03/25/04 PM 06/17/2002
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S.R. 115
P.I. 125+53.27
N 587,106.104
E 2,575,601.146
A=36° 43' 22" (LT)
B=9° 30' 00"
AC=9° 39' 38" (LT)
D=4° 45' 00"
R=1,206.23
LC=203.38
Ts=509.37
L=400.00
S.E. 0.080 FT/FT
DESIGN SPEED 60 MPH

MARINE BASE ROAD
P.I. 129+25.17
N 587,488.284
E 2,575,653.707
A=36° 43' 22" (LT)
B=14° 30' 00"
R=395.14
LC=253.26
Ts=131.15
S.E. 0.080 FT/FT
DESIGN SPEED

P.H. CARDWELL

UNITED STATES OF AMERICA (TVA)

KNOX COUNTY

MARINE RESERVE BASE

UNITED STATES OF AMERICA (TVA)

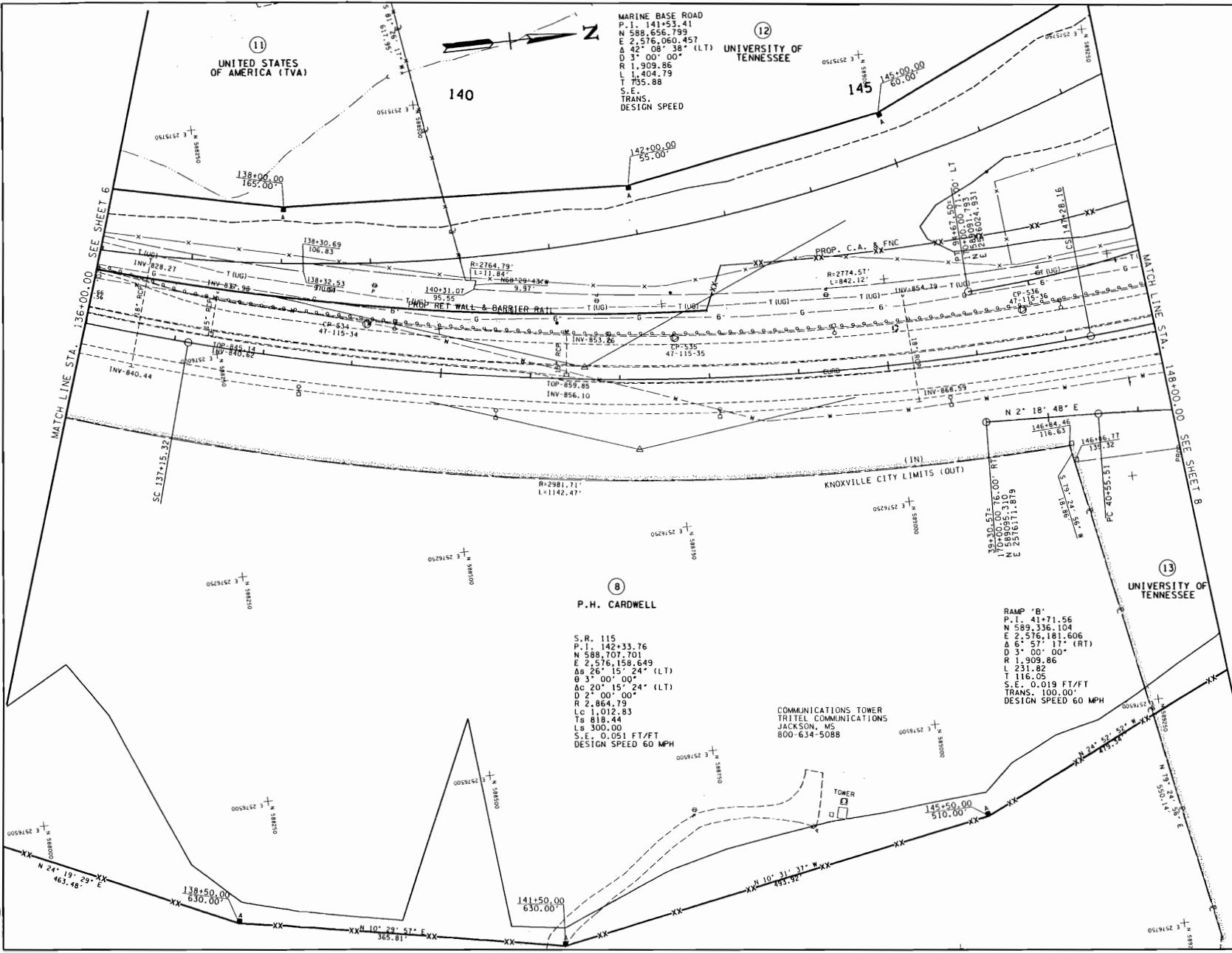
KNOXVILLE CITY LIMITS (OUT)

1-S-BR RES

MATCH LINE STA. 124+00.00 SEE SHEET 5

MATCH LINE STA. 136+00.00 SEE SHEET 7

DESIGN DIVISION
FILE NO.



MARINE BASE ROAD
P.I. 141+53.41
N 588.656.793
E 2,576.060.457
Δ 42° 08' 38" (LT)
D 3° 00' 00"
R 1,909.86
L 1,404.79
T 755.88
S.E.
TRANS. DESIGN SPEED

12
UNIVERSITY OF TENNESSEE

11
UNITED STATES OF AMERICA (TVA)

140

145

8
P. H. CARDWELL

S.R. 115
P.I. 142+33.76
N 588.707.701
E 2,576.158.649
Δ 26° 15' 24" (LT)
D 3° 00' 00"
Δc 20° 15' 24" (LT)
D 2° 00' 00"
R 2,864.79
LC 1,012.83
Ts 818.44
Ls 300.00
S.E. 0.051 FT/FT
DESIGN SPEED 60 MPH

COMMUNICATIONS TOWER
TRITEL COMMUNICATIONS
JACKSON, MS
800-634-5088

RAMP 'B'
P.I. 41+71.56
N 589.336.104
E 2,576.181.606
Δ 6° 57' 17" (RT)
D 3° 00' 00"
R 1,909.86
L 231.82
T 116.05
S.E. 0.019 FT/FT
TRANS. 100.00'
DESIGN SPEED 60 MPH

13
UNIVERSITY OF TENNESSEE

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2003	STP-NHE-115(29)	7

KNOX CO., S.R. 115 (ALCOA HWY)
47026-1270-14 (R.O.W.)

FOR PRELIMINARY FIELD REVIEW ONLY

COORDINATE VALUES ARE NAD83(995) AND ARE DATUM ADJUSTED BY THE FACTOR 1.0001. ELEVATIONS ARE MVD 1988.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF PLANNING & DESIGN
**PRESENT LAYOUT
&
PROPOSED R.O.W.**
STA. 136+00.00 TO STA. 148+00.00

SCALE: 1"=50'

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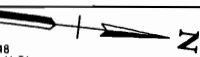
DESIGN DIVISION

FILE NO.

03/26/11 PM 06/17/2002
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MARINE BASE ROAD RAMP 'D'
 P.I. 155+50.26 P.I. 85+00.00
 N 590.004.725 E 2,516.715.248
 A 10° 43' 50" (RT) A 11° 16' 05" (LT)
 D 5' 00" 00" D 5' 00" 00"
 R 716.20 R 1,145.92
 L 134.13 L 225.36
 T 67.26 T 113.05
 S.E. 0.050 FT/FT S.E. 0.038 FT/FT
 TRANS. 130.00' TRANS. 105.00'
 DESIGN SPEED 30 MPH DESIGN SPEED 30 MPH

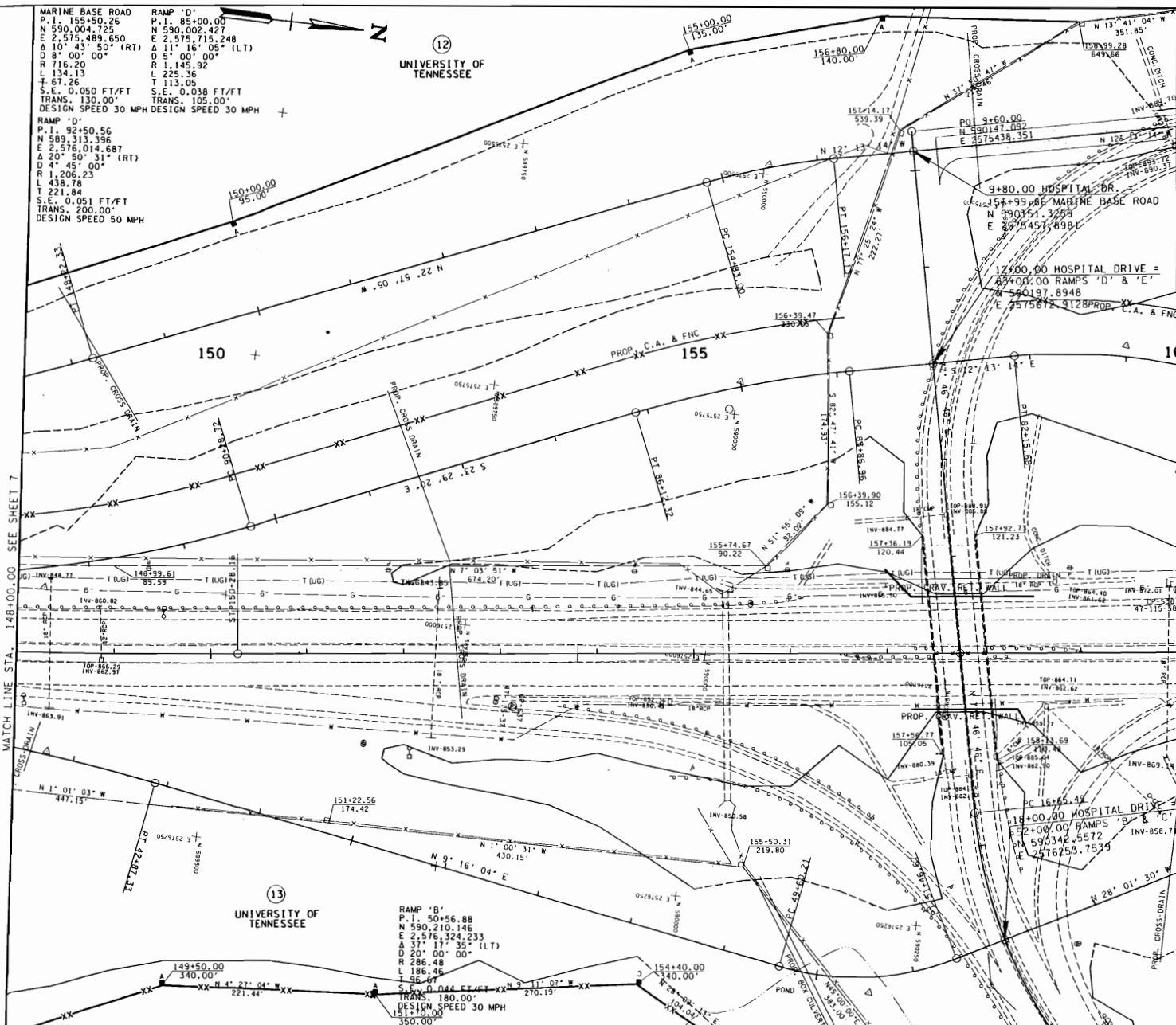
RAMP 'D'
 P.I. 92+50.56
 N 589.313.396
 E 2,576.014.687
 A 20° 50' 31" (RT)
 D 4' 45" 00"
 R 1,206.23
 L 438.78
 T 221.84
 S.E. 0.051 FT/FT
 TRANS. 200.00'
 DESIGN SPEED 50 MPH



12
 UNIVERSITY OF
 TENNESSEE

13
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 TENNESSEE

RAMP 'B'
 P.I. 50+56.88
 N 590.210.146
 E 2,576.324.233
 A 37° 17' 35" (LT)
 D 20' 00" 00"
 R 286.48
 L 186.46
 T 96.67
 S.E. 0.044 FT/FT
 TRANS. 180.00'
 DESIGN SPEED 30 MPH
 151+70.00
 350.00'



MATCH LINE STA. 148+00.00 SEE SHEET 7

MATCH LINE STA. 160+00.00 SEE SHEET 9

MATCH TO SHEET 10

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2003	STP-NHE-115(29)	8

KNOX CO. S.R. 115 (ALCOA HWY)
 47026-1270-14 (R.O.W.)

COORDINATE VALUES ARE NAD83(99) AND ARE DATUM ADJUSTED BY THE FACTOR 1.0001 & TIED TO THE TERN. ELEVATIONS ARE NAVD 1988

STATE OF TENNESSEE
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF PLANNING & DEVELOPMENT

**PRESENT LAYOUT
 &
 PROPOSED R.O.W.**

STA. 148+00.00 TO STA. 160+00.00

SCALE: 1"=50'

FOR PRELIMINARY FIELD REVIEW ONLY

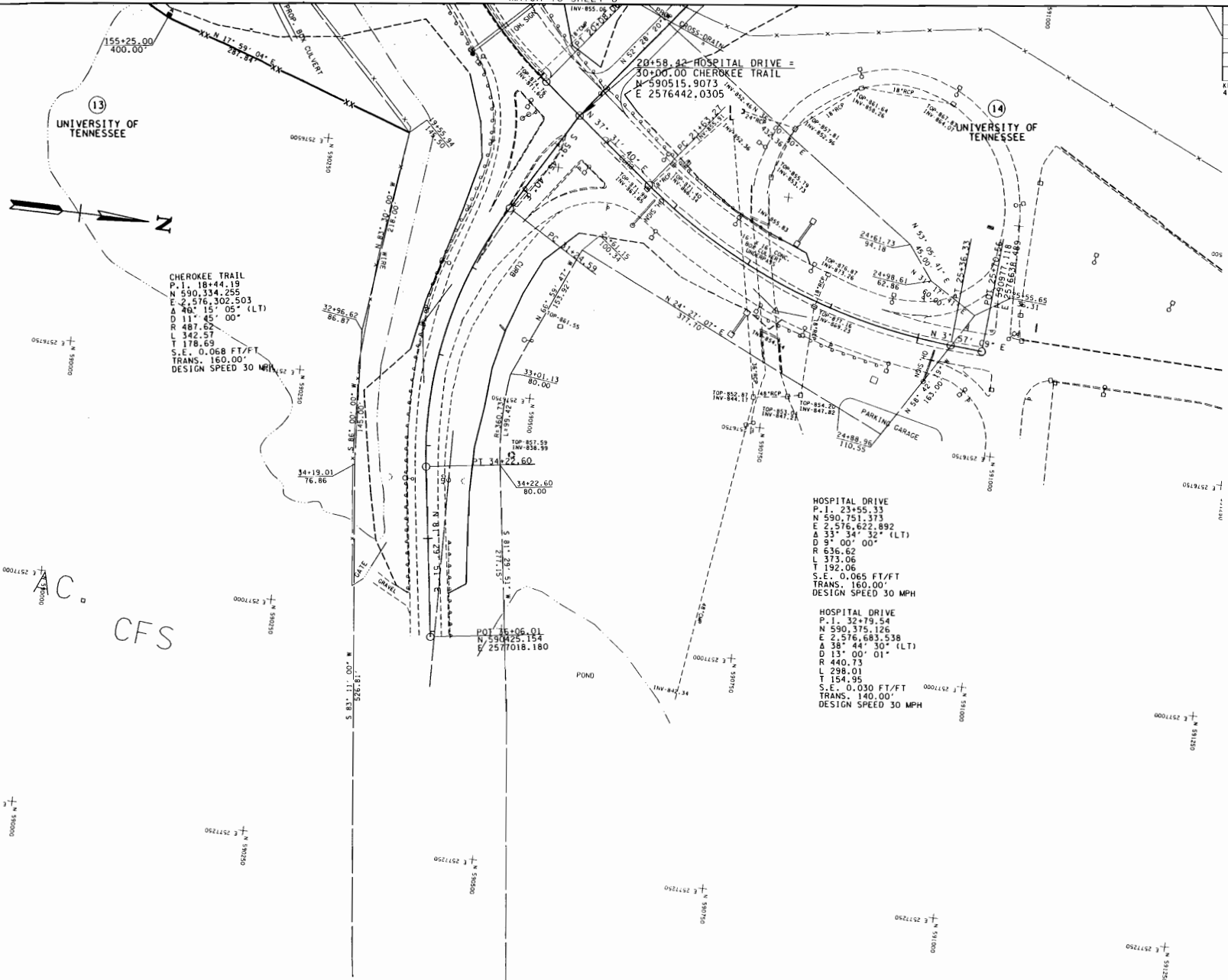
DESIGN DIVISION

FILE NO.

MATCH TO SHEET B

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2003	STP-NHE-115129	10

KNOX CO. S.R. 135 (ALCOA HWY)
47026-1270-14 (R.O.W.)



FOR PRELIMINARY FIELD REVIEW ONLY

COORDINATE VALUES ARE MADE UP BY US AND ARE DATUM ADJUSTED BY THE FACTOR 0.0001 & TIED TO THE TGN. ELEVATIONS ARE NAVD 1988

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF PLANNING & DESIGN SERVICES

**PRESENT LAYOUT
&
PROPOSED R.O.W.**

SCALE: 1"=50'

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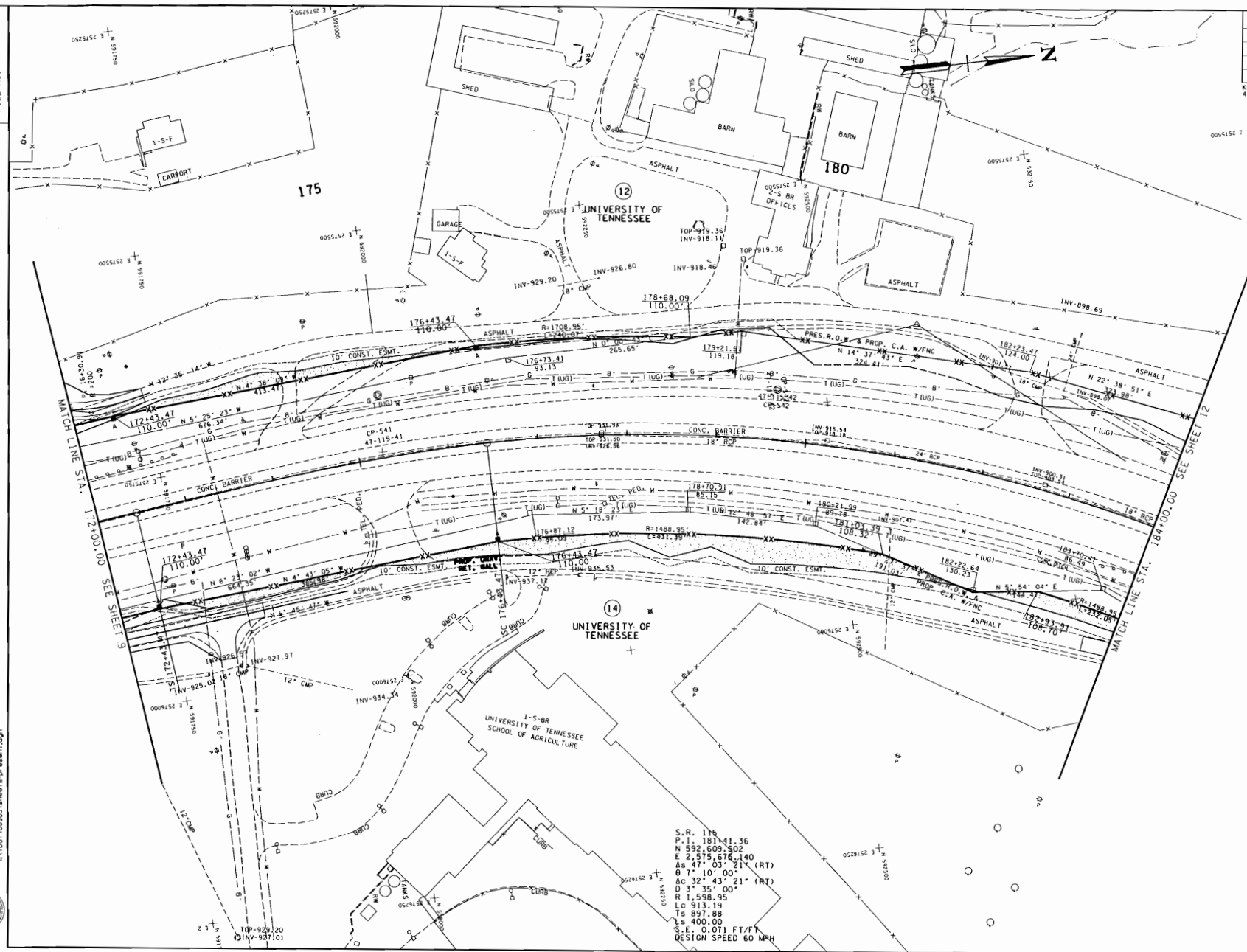


DESIGN DIVISION

FILE NO.

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2003	STP-NHE-115(29)	11

KNOX CO. S.R. 115 (ALCOA HWY)
47026-1270-14 (R.O.W.)



FOR PRELIMINARY FIELD REVIEW ONLY

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S.R. 115
P.I. 181+41.36
N 592.609 S02
E 2,575.676 L40
AS 47' 03" 21' (RT)
G 7' 10' 00"
GC 32' 43" 21' (RT)
D 3' 35' 00"
R 1,598.95
LC 913.19
Ts 897.88
Ls 400.00
S.E. 0.071 FT/FT
DESIGN SPEED 60 MPH

COORDINATE VALUES ARE NAD83(995) AND ARE DATUM ADJUSTED BY THE FACTOR 1.0001 & TIED TO THE TGRN. ELEVATIONS ARE NAVD 1988

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF PLANNING & DEVELOPMENT

**PRESENT LAYOUT
&
PROPOSED R.O.W.**

STA. 172+00.00 TO STA. 184+00.00

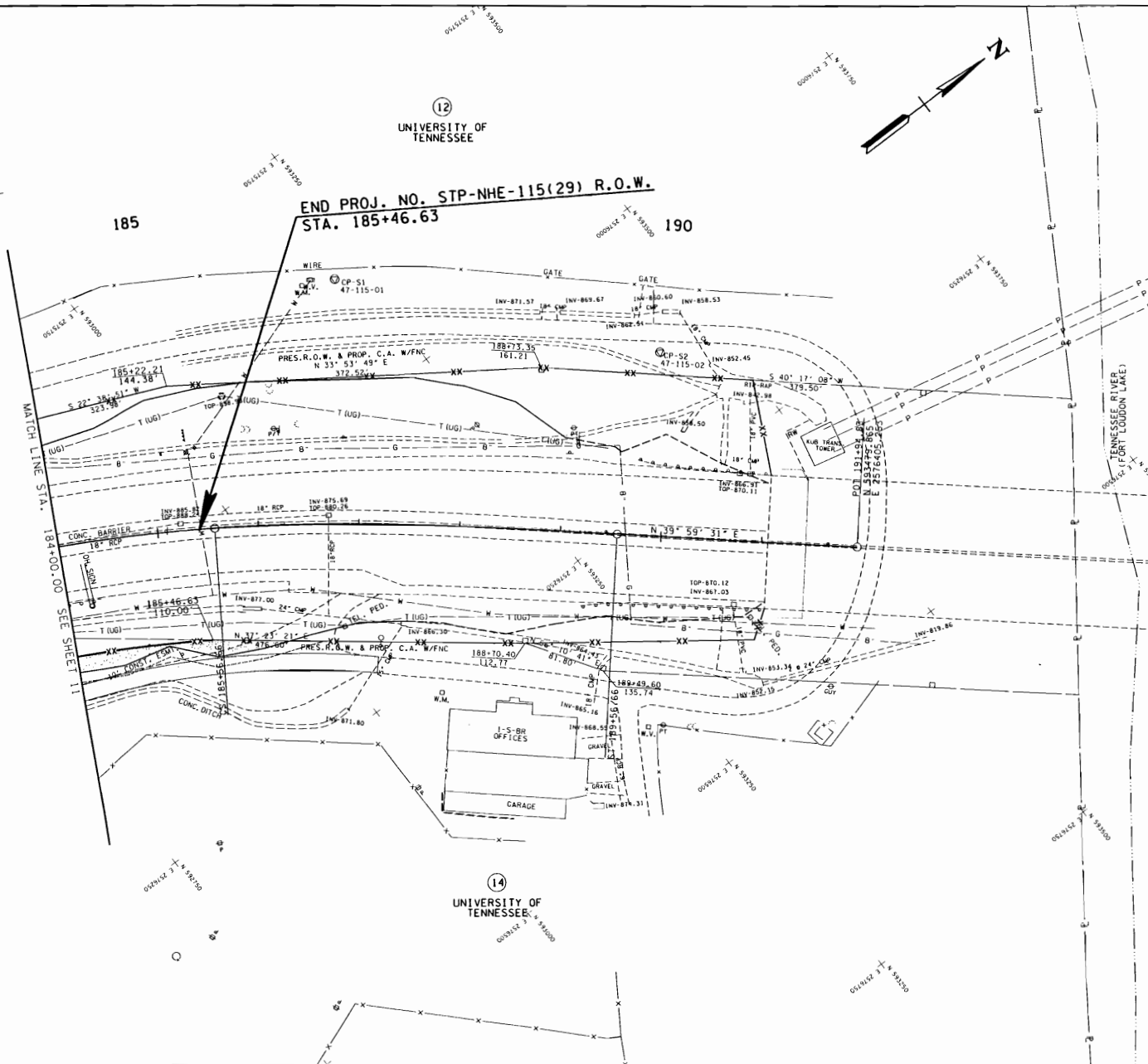
SCALE: 1"=50'

DESIGN DIVISION

FILE NO.

TYPE	YEAR	PROJECT NO.	SHEET NO.
R.O.W.	2003	STP-NHE-115(29)	12

KNOX CO. S. R. 115 (ALCOA HWY)
47026-1270-14 (R.O.W.)



FOR PRELIMINARY FIELD REVIEW ONLY

COORDINATE VALUES ARE NAD83(1995) AND ARE DATUM ADJUSTED BY THE FACTOR 1.0001 & TIED TO THE TORN. ELEVATIONS ARE NAVD 1988

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF PLANNING & DEVELOPMENT

**PRESENT LAYOUT
&
PROPOSED R.O.W.**

STA. 184+00.00 TO STA. 185+46.63

SCALE: 1"=50'

03/25/03 PM 06/17/2002
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Attachment C:
Air Quality and Noise Coordination
Updated Air Quality Report with
Mobile Source Air Toxics (MSATs) Evaluation Update

Hayes, Robbie

From: Darlene Reiter <Darlene.Reiter@tn.gov>
Sent: Thursday, October 17, 2013 3:08 PM
To: Hayes, Robbie
Subject: RE: Initial Coordination Reply from TDEC Air Pollution Control SR115 Alcoa Hwy Reply
Attachments: SR 115 (Alcoa Highway) Air Quality Report October 2013.doc; SR 115 (Alcoa Highway) Air Quality Report October 2013.pdf

Follow Up Flag: Follow up
Flag Status: Flagged

Robbie –

The noise study for this project was conducted in accordance with TDOT's current noise policy and remains valid.

The air quality study has been updated and is attached.

Darlene

Darlene Reiter, Ph.D., P.E.
TDOT Environmental Division Consultant
(615) 574-8102

Air Quality Evaluation for State Route 115 (Alcoa Highway) from Pellissippi Parkway (I-140) to Cherokee Trail Blount and Knox Counties, Tennessee

**PIN Numbers: 100241.01, 100241.02, 100241.03 and
100241.04**

**Project Numbers: 05005-1231-14, 47026-1269-14,
47026-1270-14 and 47026-1263-04**

Completed By:



Updated October 2013

TABLE OF CONTENTS

Executive Summary 1

1.0 Introduction 1

2.0 Air Quality Evaluation 1

 2.1 National Ambient Air Quality Standards (NAAQS) 1

 2.2 Transportation Conformity 2

 2.2.1 PM_{2.5} Hot-Spot Analysis 2

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 2.4 Greenhouse Gas Emissions (Climate Change) 2

 2.4.1 Mitigation for Global GHG Emissions 4

 2.4.2 Summary 4

 2.5 Construction Air Quality 5

 2.6 Indirect and Cumulative Effects 5

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Appendix A: PM_{2.5} Hot-Spot Determination, IAC Concurrence Responses, and Clearance Record
 Appendix B: MSATs Background Information

LIST OF TABLES

Table 1: Project Inclusion in TIP and LRTP 3

EXECUTIVE SUMMARY

The air quality evaluation was conducted in accordance with Section 5.3.5 (Air Quality) of the *Tennessee Environmental Procedures Manual*. [1] The evaluation concluded that the project is located in the Knoxville PM_{2.5} and ozone nonattainment areas. All four segments of this project are included in the Knoxville Long Range Mobility Plan 2040 adopted April 12, 2013. Therefore, the project conforms to the State Implementation Plan (SIP). A PM_{2.5} Hot-Spot Determination was also completed for the project that concluded that the project is “Not a Project of Air Quality Concern.” The Knoxville area Interagency Consultation (IAC) group concurred with this determination.

The project is a “Project with Low Potential Mobile Source Air Toxic (MSAT) Effects” and is not predicted to create any adverse MSAT effects. The project is also not predicted to have adverse effects on greenhouse gas emissions.

The project may cause temporary generation of construction-related pollutant emissions and dust that could result in short-term air quality impacts. These construction-related impacts will be mitigated through the implementation of Best Management Practices, which are included in *TDOT's Standard Specifications for Road and Bridge Construction*.

Finally, the project is not anticipated to create any adverse indirect impacts.

1.0 INTRODUCTION

This report updated the previous air quality analysis dated December 2010 for the widening of State Route 115 (Alcoa Highway) from Pellissippi Parkway (I-140) to Cherokee Trail in Blount and Knox Counties. The purposes of this analysis to address transportation conformity; Mobile Source Air Toxics (MSATs); climate change; and construction air quality.

2.0 AIR QUALITY EVALUATION

This study was conducted in accordance with Section 5.3.5 (Air Quality) of the *Tennessee Environmental Procedures Manual*.

2.1 National Ambient Air Quality Standards (NAAQS)

The United States Environmental Protection Agency (EPA) has established allowable concentrations and exposure limits called the National Ambient Air Quality Standards (NAAQS) for various “criteria” pollutants. These pollutants include carbon monoxide (CO), nitrogen oxides (NO_x), ozone (O₃), particulate matter (PM₁₀ and PM_{2.5}), sulfur oxides (SO_x), and lead (Pb).

In accordance with the Clean Air Act Amendments of 1990 (CAAA of 1990), EPA identified areas that did not meet the NAAQS for the criteria pollutants and designated them as “nonattainment” areas. Once a nonattainment area meets the NAAQS, it is redesignated as a “maintenance” area.

The project is located in the Knoxville PM_{2.5} and ozone nonattainment areas.

Transportation conformity is a process required of Metropolitan Planning Organizations (MPOs) pursuant to the Clean Air Act Amendments (CAAA) of 1990. CAAA require that transportation plans, programs, and projects in nonattainment or maintenance areas that are funded or approved by the Federal Highway Administration (FHWA) be in conformity with the State Implementation Plan (SIP), which represents the State’s plan to either achieve or maintain the National Ambient Air Quality Standard (NAAQS) for a particular pollutant.

Projects in nonattainment and maintenance areas are in conformity with the SIP if they are included in a fiscally constrained and conforming Long Range Transportation Plan (LRTP) or Transportation Improvement Program (TIP).

As shown in Table 1, all four segments of this project are included in the Knoxville Long Range Mobility Plan 2040 adopted April 12, 2013.

Three of the segments are also included in the Year 2011-2014 Transportation Improvement Program adopted by the TPO Executive Board on September 22, 2010 and in the draft Year 2014-2017 Transportation Improvement Program. The segment from Maloney Road to Woodson Drive is state-funded but regionally significant. As shown in Table 1, this project is included in the Knoxville Long Range Mobility Plan 2040.

2.2.1 PM_{2.5} Hot-Spot Analysis

On March 10, 2006, EPA issued a Final Conformity Rule regarding the localized or “hot-spot” analysis requirements for PM nonattainment areas (40 CFR Part 93). To meet statutory requirements, the Transportation Conformity Rule requires PM hot-spot analyses to be performed for “projects of air quality concern” located in PM_{2.5} and PM₁₀ nonattainment or maintenance areas.

In March 2006, EPA and FHWA issued *Transportation Conformity Guidance for Qualitative Hot-spot Analyses on PM_{2.5} and PM₁₀ Nonattainment and Maintenance Areas*. This guidance was updated in December 2010 to require a quantitative analysis for “projects of air quality concern.” The updated guidance is titled *Transportation Conformity Guidance for Quantitative Hot-spot Analyses on PM_{2.5} and PM₁₀ Nonattainment and Maintenance Areas*.

This guidance provides information for State and local agencies to meet the PM_{2.5} and PM₁₀ hot-spot analysis requirements established in the Final Conformity Rule. The guidance includes examples of projects that are most likely to be an air quality concern, as well as examples of projects that are not considered an air quality concern.

TDOT’s Environmental Division developed a PM_{2.5} Hot Spot Determination Process and Procedures document that details the hot-spot analysis process for TDOT projects. This document was reviewed and approved by the Knoxville and Chattanooga Interagency Consultations (IAC) Groups.

Build Alternatives may be low because the roadway for which traffic projections were developed did not include all of the roads from which traffic would be diverted.

The emissions increase is offset somewhat by lower MSAT emission rates due to increased speeds; according to EPA's MOVES2010b model, emissions of all of the priority MSAT decrease as speed increases. Because the estimated VMT under each of the Alternatives are nearly the same it is expected there would be no appreciable difference in overall MSAT emissions among the various alternatives. Also, regardless of the alternative chosen, emissions will likely be lower than present levels in the design year as a result of EPA's national control programs that are projected to reduce annual MSAT emissions by over 80 percent between 2010 and 2050. Local conditions may differ from these national projections in terms of fleet mix and turnover, VMT growth rates, and local control measures. However, the magnitude of the EPA-projected reductions is so great (even after accounting for VMT growth) that MSAT emissions in the study area are likely to be lower in the future in nearly all cases. Additionally, travel speeds for the Build Alternative are expected to be higher than for the No-Build Alternative.

The additional travel lanes contemplated for the Build Alternative will have the effect of moving some traffic closer to nearby sensitive land uses; therefore, under the Build Alternative there may be localized areas where ambient concentrations of MSATs could be higher than under the No-Build Alternative.

However, the magnitude and the duration of these potential increases compared to the No-Build Alternative cannot be reliably quantified due to incomplete or unavailable information in forecasting project-specific MSAT health impacts.

In sum, when a highway is widened, the localized level of MSAT emissions for the Build Alternative could be higher relative to the No-Build Alternative, but this could be offset due to increases in speeds and reductions in congestion (which are associated with lower MSAT emissions). Also, MSATs will be lower in other locations when traffic shifts away from them. However, on a regional basis, EPA's vehicle and fuel regulations, coupled with fleet turnover, will over time cause substantial reductions that, in almost all cases, will cause region-wide MSAT levels to be significantly lower than current levels.

Substantial construction-related MSAT emissions are not anticipated for this project as construction is not planned to occur over an extended building period. However, construction activity may generate temporary increases in MSAT emissions in the project area.

2.4 Greenhouse Gas Emissions (Climate Change)

Climate change is an important national and global concern. While the earth has gone through many natural changes in climate in its history, there is general agreement that the earth's climate is currently changing at an accelerated rate and will continue to do so for the foreseeable future. Anthropogenic (human-caused) greenhouse gas (GHG) emissions contribute to this rapid change. Carbon dioxide (CO₂) makes up the largest component of these GHG emissions. Other prominent transportation GHGs include methane (CH₄) and nitrous oxide (N₂O).

Many GHGs occur naturally. Water vapor is the most abundant GHG and makes up approximately two thirds of the natural greenhouse effect. However, the burning of fossil fuels and

Appendix A
PM_{2.5} Hot-Spot Determination, IAC Concurrence Responses, and Clearance Record



STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
ENVIRONMENTAL DIVISION
SUITE 900 - JAMES K. POLK BUILDING
505 DEADERICK STREET
NASHVILLE, TENNESSEE 37243-0334

PM_{2.5} Hot-Spot Determination

Project Name: Alcoa Highway (SR-115/US-129)

<i>From</i>	<i>To</i>	<i>County</i>	<i>PIN #</i>	<i>Federal Project #</i>
Pellissippi Pkwy (I-140)	Knox/Blount County Line	Blount	100241.01	STP-NHE-115(26)
Knox/Blount County Line	Maloney Road	Knox	100241.02	STP-NHE-115(27)
Maloney Road	Woodson Drive		100241.04	State-Funded
Woodson Drive	Cherokee Trail		100241.03	STP-NHE-115(29)

Counties: Blount and Knox
Date: November 19, 2010

Statement of Purpose and Legal Requirements

Section 176(c) of the Clean Air Act, as amended, requires that transportation agencies, such as TDOT, demonstrate that all proposed transportation projects that are located in nonattainment or maintenance areas, and using federal money, are consistent with the air quality goals found in the State Implementation Plan (SIP) and the corresponding Transportation Improvement Program (TIP) or other conforming plan.

The process to ensure this consistency is called Transportation Conformity. Conformity to the SIP means that transportation activities will not cause new violations of the National Ambient Air Quality Standards (NAAQS), will not worsen existing violations, and will not delay attainment of the NAAQS.

Project-level conformity is required by Title 40 Code of Federal Regulations (CFR) Part 93, more commonly known as the Transportation Conformity Rule. When evaluating project-level conformity for PM_{2.5}, the process is called a PM_{2.5} Hot Spot Determination.

The Transportation Conformity Rule instructs the U.S. Department of Transportation (DOT) to ensure that all proposed transportation projects are in conformity before releasing federal funds for the project. To accomplish this, the FHWA and/or FTA require that all proposed transportation projects in a nonattainment or maintenance area be classified as: 1) Exempt, 2) Project Not of Air Quality Concern, or 3) Project of Air Quality Concern.

In §93.126 and §93.128, the Transportation Conformity Rule establishes a list of transportation projects that are categorically exempt from a project-level conformity determination. For nonexempt projects in nonattainment areas, TDOT must determine if the project has the potential to adversely impact air quality and FHWA and/or FTA must make the same determination.

This proposed transportation project is located in a jurisdiction currently classified as nonattainment for the PM_{2.5} NAAQS by the U.S. Environmental Protection Agency. This project is not classified as exempt. Therefore, TDOT is presenting the following PM_{2.5} Hot Spot Determination to the Interagency Consultation (IAC) group to demonstrate this project is not of air quality concern and that it does conform to the SIP.

Project Description

This project involves the widening of SR-115 (Alcoa Highway) from Pellissippi Parkway (I-140) in Blount County to north of Cherokee Trail in Knox County, a distance of approximately 9.8 miles. SR-115 will generally be widened from four through travel lanes (two in each direction) to six through travel lanes (three in each direction). The 1.5 mile section of SR-115 between Singleton Station Road and Topside Road (SR 333) in Blount County will also include auxiliary lanes.

A design speed of 60 mph will be used for the length of the project with the exception of the section between Maloney Road and Montlake Drive where there are numerous access driveways. A 45 mph design speed will be required for this section and curbs and gutters will be used in lieu of outside shoulders.

The project also includes the redesign of eight at-grade intersections (both signalized and unsignalized) to interchanges to improve traffic operations as described in Table 1. The proposed configurations of each of these interchanges are shown in Attachment A.

Table 1: Existing and Proposed Intersection Control

<i>Intersection</i>	<i>Existing Traffic Control</i>	<i>Proposed Traffic Control</i>
Singleton Station Road	Traffic signal	Partial cloverleaf interchange
Hillside Drive	Traffic signal	One-half diamond interchange
SR 333 (Topside Road)	Traffic signal	Diamond interchange
Topside Road	Traffic signal	Mix of two-way ramps and two-way quadrants
SR 168 (John Sevier Hwy)	Traffic signal, ramps	Partial cloverleaf interchange
Maloney Road	Traffic signal, ramps	Mix of two-way ramps and two-way quadrants
Montlake Drive/Woodson Drive/Barber Hill Road	Traffic signals	Mix of two-way ramps and two-way quadrants
Cherokee Trail/Hospital Drive	Traffic signal, ramps	Partial cloverleaf interchange

SR 168 (John Sevier Highway) and SR 333 (Topside Road) will cross over SR 115 while the other roadways will cross under SR 115.

PM_{2.5} Hot Spot Determination Questions and Answers

1. Is this project in a conforming Plan/TIP?

Yes. All four segments of this project are included in the September 2010 Amendments to the 2034 Knoxville Regional Transportation Planning Organization Long Range Transportation Plan adopted by TPO Executive Board on September 22, 2010. Three of the segments are also included in the Year 2011-2014 Transportation Improvement Program adopted by the TPO Executive Board on September 22, 2010. FHWA and FTA approved the Conformity Determination for the LRTP and TIP on November 5, 2010.

The projects are summarized in Table 2. The LRTP and TIP sheets and the FHWA/FTA Conformity Determination are provided in Attachment B.

Table 2: LRTP and TIP Project Summary

<i>From</i>	<i>To</i>	<i>LRTP Project</i>	<i>LRTP Description</i>	<i>Horizon Year</i>	<i>TIP Project</i>	<i>TIP Description</i>
Blount County						
Pellissippi Pkwy (I-140)	Knox/Blount County Line	216	Widen 4-lane to 6-lane plus 2 auxiliary lanes between Singleton Station Rd and Topside Rd (SR 333)	2024	2011-003/216	Reconstruct from 4 to 6 lanes with 2 auxiliary lanes between Singleton Station Rd and Topside Rd (SR 333)
Knox County						
Knox/Blount County Line	Maloney Road	628	Widen 4-lane to 6-lane	2024	2011-004/628	Widen from 4 to 6 lanes, including pedestrian and bicycle facilities.
Maloney Road	Woodson Drive	627	Widen 4-lane to 6-lane	2024	Not Included	
Woodson Drive	Cherokee Trail	653	Widen 4-lane to 6-lane	2024	2011-002/653	Widen from 4 lanes to 6 lanes.

2. Is the project on a new or expanded highway or expressway that serves a significant volume of diesel truck traffic, such as a facility with greater than 125,000 annual average daily traffic (AADT) and 8% or more of such AADT is diesel truck traffic?

No. Traffic forecasts for the project were prepared by Sain Associates are summarized in Table 3 and provided in Attachment C.

As indicated, the projected design year 2036 Annual Average Daily Traffic (AADT) volumes on Alcoa Highway range from 45,150 vehicles per day (vpd) between I-140 and Singleton Station Road and 60,600 vpd between Woodson Drive and Cherokee Trail in the year 2016 for the No-Build Alternative. The projected daily truck percentage for Alcoa Highway is 7% resulting in projected truck volumes between 1,825 and 6,245 trucks per day.

Table 3: Traffic Projections

<i>Termini</i>	<i>Base Year 2016 (No-Build)</i>			<i>Design Year 2036 (No-Build)</i>			<i>Design Year 2036 (Build)</i>		
	<i>AADT</i>	<i>% Trucks</i>	<i>Trucks</i>	<i>AADT</i>	<i>% Trucks</i>	<i>Trucks</i>	<i>AADT</i>	<i>% Trucks</i>	<i>Trucks</i>
I-140 to Singleton Station Road	45,150	7%	3,161	24,100	7%	1,687	26,070	7%	1,825
Singleton Station Road to Topside Road	52,690	7%	3,688	74,020	7%	5,181	84,540	7%	5,918
Topside Road to Blount/Knox County Line	56,150	7%	3,931	78,020	7%	5,461	87,800	7%	6,146
Blount/Knox County Line to Maloney Road	58,210	7%	4,075	75,260	7%	5,268	89,220	7%	6,245
Maloney Road to Woodson Drive	58,050	7%	4,064	74,680	7%	5,228	86,690	7%	6,068
Woodson Drive to Cherokee Trail	60,600	7%	4,242	75,540	7%	5,288	87,290	7%	6,110

3. Does the project construct new exit ramps or other highway facility improvements that connect a highway or expressway to a major freight, bus, or intermodal terminal?

No. The project will not provide any new connections to major freight, bus, or intermodal facilities.

4. Does the project expand an existing highway or other facility that already has a congested intersection (Operates at LOS D, E, or F) and will this project result in a significant increase in the number of diesel trucks?

As described previously, the project includes the redesign of eight at-grade intersections (both signalized and unsignalized) to interchanges to improve traffic operations. These improvements will improve the LOS of all eight intersections and will significantly reduce idle operations and pollutant emissions.

Sain Associates conducted a LOS analysis for the roadway segments for Year 2009 and for Years 2016 and 2036 for both the No-Build and Build Alternatives. The results are provided in Attachment C and summarized in Table 4.

As shown, implementation of the Build Alternative will improve the LOS on most segments of Alcoa Highway in Years 2016 and 2036 with the exception of the segment from Topside Road (SR 333) to the Knox/Blount County Line that is not scheduled for completion until 2024.

Table 4: Level of Service Projections

<i>From</i>	<i>To</i>	<i>Year 2009</i>	<i>Year 2016⁽¹⁾</i>		<i>Year 2036⁽¹⁾</i>	
		<i>No-Build</i>	<i>No-Build</i>	<i>Build</i>	<i>No-Build</i>	<i>Build</i>
Pellissippi Pkwy (I-140)	Singleton Station Road	D	D D		B	A
Singleton Station Road	Topside Road (SR 333)	D	E	E	F E	
Topside Road (SR 333)	Knox/Blount County Line	D	E	n/a ⁽²⁾	F	E
Knox/Blount County Line	Maloney Road	D	E	D	F	F
Maloney Road	Woodson Drive	D	F	D	F	F
Woodson Drive	North of Cherokee Trail	C	F	D	F	E

(1) With construction of Alcoa Bypass and Pellissippi Parkway Extension.

(2) This section is not scheduled for completion until 2024.

5. Does the highway project involve a significant increase in the number of diesel transit buses and/or diesel trucks?

No. As described above, the project is not predicted to significantly increase the number of diesel trucks on the area roadway network. The project is not expected to serve a significant volume of diesel transit buses.

6. Will this project cause or worsen an existing violation?

No. The improved traffic flow due to increased capacity and the elimination of traffic signals along the corridor will reduce excessive idling and is expected to reduce mobile source emissions in the project area thereby improving air quality from the No-Build Alternative.

Darlene Reiter - RE: PM2.5 Hot-Spot Determination for Alcoa Highway (SR-115/US-129)

From: <Tameka.Macon@dot.gov>
To: <Darlene.Reiter@tn.gov>, <asmcdaniel@aqm.co.knox.tn.us>, <laliddington@aqm.co.knox.tn.us>, <Britta.Stein@dot.gov>, <Elizabeth.Martin@dot.gov>, <Benjamin.Lynorae@epa.gov>, <Sheckler.Kelly@epa.gov>, <smith.dianna@epa.gov>, <LeighAnn.Tribble@dot.gov>, <Jeff.Welch@knoxtrans.org>, <Mike.Conger@knoxtrans.org>, <Shannon.Tolliver@knoxtrans.org>, <richd@mymorristown.com>, <jim_renfro@nps.gov>, <teresa_cantrell@nps.gov>, <Alan.Jones@tn.gov>, <Angela.Midgett@tn.gov>, <Marc.Corrigan@tn.gov>, <Mark.McAdoo@tn.gov>, <Robert.Rock@tn.gov>, <Ronnie.Porter@tn.gov>
Date: 11/29/2010 8:05 AM
Subject: RE: PM2.5 Hot-Spot Determination for Alcoa Highway (SR-115/US-129)
CC: <Jerry.Melson@tn.gov>, <Jim.Ozment@tn.gov>, <JonnaLeigh.Stack@tn.gov>, <Mike.Russell@tn.gov>

Good Morning Darlene:

FHWA concurs that this project is not of AQ concern.

Thanks and have a great day!

Tameka A. Macon
Community Planner
404 BNA Drive, Suite 508
Nashville, Tennessee 37217
P: 615-781-5767 F: 615-781-5773

From: Darlene Reiter [mailto:Darlene.Reiter@tn.gov]
Sent: Friday, November 19, 2010 11:56 AM
To: A McDaniel; L Liddington; Stein, Britta (FHWA); Martin, Elizabeth (FTA); Lynorae Benjamin; Kelly Sheckler; Dianna Smith; Tribble, Leigh Ann (FHWA); Macon, Tameka (FHWA); Jeff Welch; Mike Conger; Shannon Toliver; Rich D; Jim Renfro; Teresa Cantrell; Alan Jones; Angela Midgett; Marc Corrigan; Mark McAdoo; Robert Rock; Ronnie Porter
Cc: Jerry Melson; Jim Ozment; JonnaLeigh Stack; Mike Russell
Subject: PM2.5 Hot-Spot Determination for Alcoa Highway (SR-115/US-129)

Knoxville Area IAC:

TDOT has evaluated the following project and determined that it is NOT A PROJECT OF AIR QUALITY CONCERN.

County	PIN	Description
Blount	100241.01	Widening - Pellissippi Pkwy (I-140) to Knox/Blount County Line
Knox	100241.02 100241.04 100241.03	Widening - Knox/Blount County Line to Cherokee Trail

The project description and details are provided in the attached PM_{2.5} Hot-Spot Determination.

TDOT requests your concurrence with our determination that this project is NOT A PROJECT OF AIR QUALITY CONCERN. Please respond no later than close of business (4:30 central time) on December 7, 2010. If TDOT does not receive a response to the contrary within 10 business days of this email then TDOT will assume that you concur with this determination.

Thank you.

Darlene D. Reiter, Ph.D., P.E.
TDOT Environmental Division Consultant

Darlene Reiter - Re: PM2.5 Hot-Spot Determination for Alcoa Highway (SR-115/US-129)

From: Marc Corrigan
To: Benjamin, Lynorae; Cantrell, Teresa; Conger, Mike; D, Rich; Jones, Alan; Liddington, L; Macon, Tameka; Martin, Elizabeth; McAdoo, Mark; McDaniel, A; Midgett, Angela; Porter, Ronnie; Reiter, Darlene; Renfro, Jim; Rock, Robert; Sheckler, Kelly; Smith, Dianna; Stein, Britta; Toliver, Shannon; Tribble, Leigh Ann; Welch, Jeff
Date: 11/29/2010 8:26 AM
Subject: Re: PM2.5 Hot-Spot Determination for Alcoa Highway (SR-115/US-129)
CC: Melson, Jerry; Ozment, Jim; Russell, Mike; Stack, JonnaLeigh

Darlene,

TAPCD concurs with TDOT's assessment that these projects are not of air quality concern.

Marc

>>> Darlene Reiter 11:56 AM 11/19/10 >>>
 Knoxville Area IAC:

TDOT has evaluated the following project and determined that it is NOT A PROJECT OF AIR QUALITY CONCERN.

County	PIN	Description
Blount	100241.01	Widening - Pellissippi Pkwy (I-140) to Knox/Blount County Line
Knox	100241.02 100241.04 100241.03	Widening - Knox/Blount County Line to Cherokee Trail

The project description and details are provided in the attached PM_{2.5} Hot-Spot Determination.

TDOT requests your concurrence with our determination that this project is NOT A PROJECT OF AIR QUALITY CONCERN. Please respond no later than close of business (4:30 central time) on December 7, 2010. If TDOT does not receive a response to the contrary within 10 business days of this email then TDOT will assume that you concur with this determination.

Thank you.

Darlene D. Reiter, Ph.D., P.E.
 TDOT Environmental Division Consultant

Darlene Reiter - RE: PM2.5 Hot-Spot Determination for Alcoa Highway (SR-115/US-129)

From: "asmcdaniel" <asmcdaniel@aqm.co.knox.tn.us>
To: "Darlene Reiter" <Darlene.Reiter@tn.gov>
Date: 11/19/2010 12:46 PM
Subject: RE: PM2.5 Hot-Spot Determination for Alcoa Highway (SR-115/US-129)

Knox County Air Quality Management concurs.

Steve McDaniel, P.E.
 Knox County Department of Air Quality Management

From: Darlene Reiter [mailto:Darlene.Reiter@tn.gov]
Sent: Friday, November 19, 2010 12:56 PM
To: A McDaniel; L Liddington; Britta Stein; Elizabeth Martin; Lynorae Benjamin; Kelly Sheckler; Dianna Smith; Leigh Ann Tribble; Tameka Macon; Jeff Welch; Mike Conger; Shannon Toliver; Rich D; Jim Renfro; Teresa Cantrell; Alan Jones; Angela Midgett; Marc Corrigan; Mark McAdoo; Robert Rock; Ronnie Porter
Cc: Jerry Melson; Jim Ozment; JonnaLeigh Stack; Mike Russell
Subject: PM2.5 Hot-Spot Determination for Alcoa Highway (SR-115/US-129)

Knoxville Area IAC:

TDOT has evaluated the following project and determined that it is NOT A PROJECT OF AIR QUALITY CONCERN.

County	PIN	Description
Blount	100241.01	Widening - Pellissippi Pkwy (I-140) to Knox/Blount County Line
Knox	100241.02 100241.04 100241.03	Widening - Knox/Blount County Line to Cherokee Trail

The project description and details are provided in the attached PM_{2.5} Hot-Spot Determination.

TDOT requests your concurrence with our determination that this project is NOT A PROJECT OF AIR QUALITY CONCERN. Please respond no later than close of business (4:30 central time) on December 7, 2010. If TDOT does not receive a response to the contrary within 10 business days of this email then TDOT will assume that you concur with this determination.

Darlene Reiter - RE: Alcoa Highway PM2.5 Hot-Spot Determination

From: "asmcdaniel" <asmcdaniel@aqm.co.knox.tn.us>
To: "Darlene Reiter" <Darlene.Reiter@tn.gov>
Date: 12/6/2010 10:00 AM
Subject: RE: Alcoa Highway PM2.5 Hot-Spot Determination

Darlene,

Thanks. This correction does not change our opinion that a PM2.5 hot spot determination is unnecessary.

Steve McDaniel, P.E.
Knox County Department of Air Quality Management

From: Darlene Reiter [mailto:Darlene.Reiter@tn.gov]
Sent: Monday, December 06, 2010 10:45 AM
To: A McDaniel; Kelly Sheckler; Tameka Macon; Marc Corrigan
Cc: dreiter@bowlbyassociates.com; Mike Conger
Subject: Alcoa Highway PM2.5 Hot-Spot Determination

Dear IAC members:

I believe I have most of your concurrence responses for the Alcoa Highway project. Thank you. I wanted to let you know that the determination erroneously stated that the intersections that will be converted to interchanges are currently signalized. They are currently unsignalized. This does not change the responses to any of the hot-spot questions. However, I have attached a corrected determination dated today.

I apologize for the error.

Darlene

Project-Level Conformity

PIN

Federal Project Number

Conformity Type

County

IAC Group

Type

Priority

**Project Description**

Route Name

Project Description

This project involves the widening of SR-115 (Alcoa Highway) from Pellissippi Parkway (I-140) in Blount County to north of Cherokee Trail in Knox County, a distance of approximately 9.8 miles. SR-115 will generally be widened from four through travel lanes (two in each direction) to six through travel lanes (three in each direction). The 1.5 mile section of SR-115 between Singleton Station Road and Topside Road (SR 333) in Blount County will also include auxiliary lanes.

The project also includes the redesign of eight at-grade intersections to interchanges to improve traffic operations.

Project Origination

Date Need Identified

Determination Requested By

Data Collection

Date Traffic Data Requested

Source of Traffic Data

TDOT Conclusion

Conformity Determination

Date of Determination

Interagency Consultation (IAC)

IAC Status

Deadline for IAC Comments

Agency Concurrence Dates

FHWA EPA

FTA TDEC

Notification And Notes

Date of TDOT Internal Notification

Knox County concurred 11/19/2010.

The November 19, 2010 determination was a resubmission of a previous determination that FHWA rejected in March 2007.

Appendix B
MSATs Background Information

MOBILE SOURCE AIR TOXICS (MSATs)

Background

Controlling air toxic emissions became a national priority with the passage of the Clean Air Act Amendments (CAAA) of 1990, whereby Congress mandated that the U.S. Environmental Protection Agency (EPA) regulate 188 air toxics, also known as hazardous air pollutants. The EPA has assessed this expansive list in their latest rule on the Control of Hazardous Air Pollutants from Mobile Sources (Federal Register, Vol. 72, No. 37, page 8430, February 26, 2007), and identified a group of 93 compounds emitted from mobile sources that are listed in their Integrated Risk Information System (IRIS) (<http://www.epa.gov/iris/>). In addition, EPA identified seven compounds with significant contributions from mobile sources that are among the national and regional-scale cancer risk drivers from their 1999 National Air Toxics Assessment (NATA) (<http://www.epa.gov/ttn/atw/nata1999/>). These are acrolein, benzene, 1,3-butadiene, diesel particulate matter plus diesel exhaust organic gases (diesel PM), formaldehyde, naphthalene, and polycyclic organic matter. While FHWA considers these the priority mobile source air toxics, the list is subject to change and may be adjusted in consideration of future EPA rules. The 2007 EPA rule mentioned above requires controls that will dramatically decrease MSAT emissions through cleaner fuels and cleaner engines. According to an FHWA analysis using EPA's MOBILE6.2 model, even if vehicle activity (vehicle-miles travelled, VMT) increases by 145 percent as assumed, a combined reduction of 72 percent in the total annual emission rate for the priority MSAT is projected from 1999 to 2050, as shown in Figure 1.

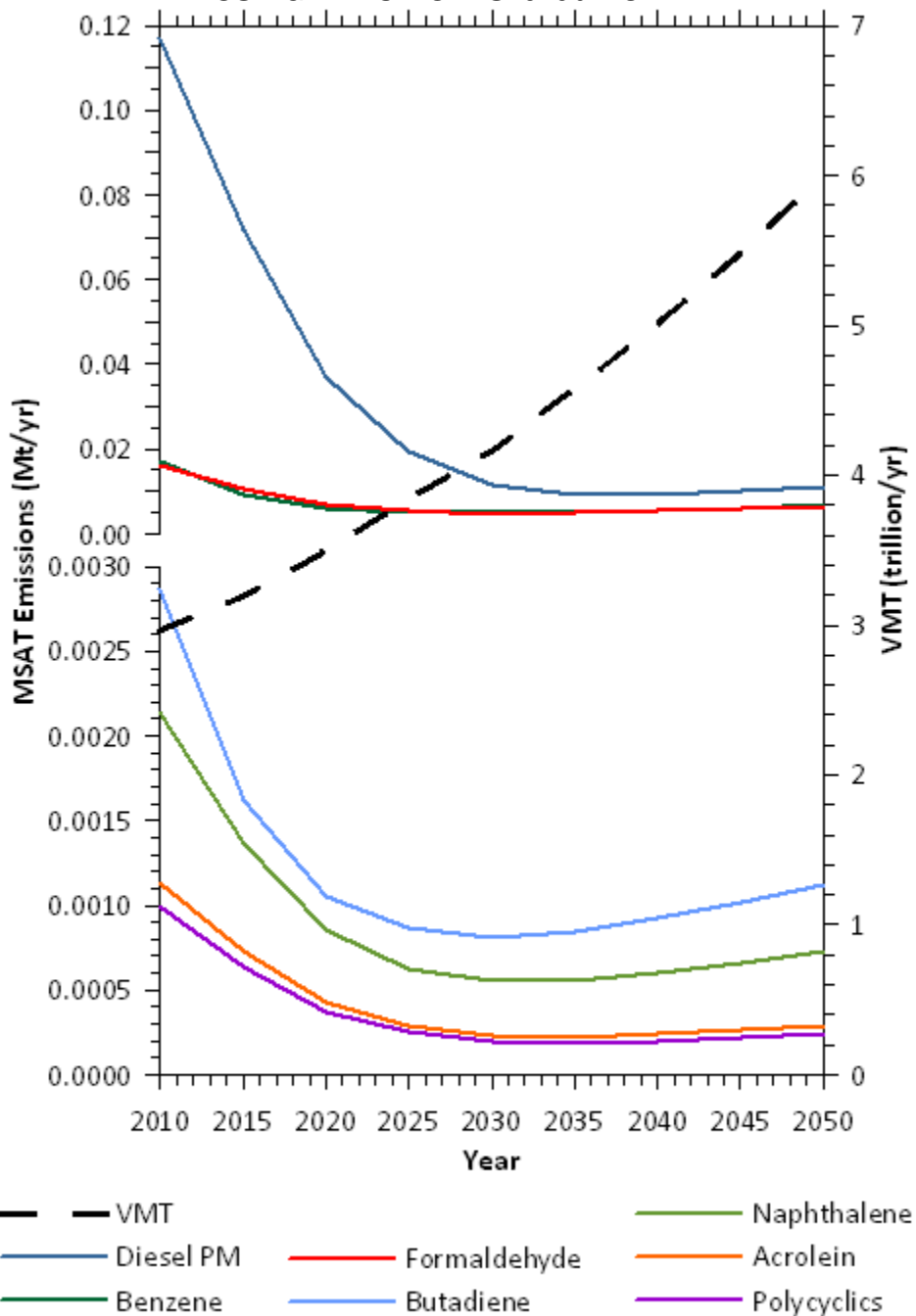
Motor Vehicle Emissions Simulator (MOVES)

According to EPA, MOVES improves upon the previous MOBILE model in several key aspects: MOVES is based on a vast amount of in-use vehicle data collected and analyzed since the latest release of MOBILE, including millions of emissions measurements from light-duty vehicles. Analysis of this data enhanced EPA's understanding of how mobile sources contribute to emissions inventories and the relative effectiveness of various control strategies. In addition, MOVES accounts for the significant effects that vehicle speed and temperature have on PM emissions estimates, whereas MOBILE did not. MOVES2010b includes all air toxic pollutants in NATA that are emitted by mobile sources. EPA has incorporated more recent data into MOVES2010b to update and enhance the quality of MSAT emission estimates. These data reflect advanced emission control technology and modern fuels, plus additional data for older technology vehicles.

Based on an FHWA analysis using EPA's MOVES2010b model, as shown in Figure 1, even if vehicle-miles travelled (VMT) increases by 102 percent as assumed from 2010 to 2050, a combined reduction of 83 percent in the total annual emissions for the priority MSAT is projected for the same time period.

The implications of MOVES on MSAT emissions estimates compared to MOBILE are: lower estimates of total MSAT emissions; significantly lower benzene emissions; significantly higher diesel PM emissions, especially for lower speeds. Consequently, diesel PM is projected to be the dominant component of the emissions total.

**Figure 1: NATIONAL MSAT EMISSION TRENDS 1999 - 2050
FOR VEHICLES OPERATING ON ROADWAYS
USING EPA'S MOVES2010b MODEL**



Note: Trends for specific locations may be different, depending on locally derived information representing vehicle-miles travelled, vehicle speeds, vehicle mix, fuels, emission control programs, meteorology, and other factors
Source: EPA MOVES2010b model runs conducted during May - June 2012 by FHWA.

MSAT Research

Air toxics analysis is a continuing area of research. While much work has been done to assess the overall health risk of air toxics, many questions remain unanswered. In particular, the tools and techniques for assessing project-specific health outcomes as a result of lifetime MSAT exposure remain limited. These limitations impede the ability to evaluate how potential public health risks posed by MSAT exposure should be factored into project-level decision-making

within the context of NEPA.

Nonetheless, air toxics concerns continue to be raised on highway projects during the NEPA process. Even as the science emerges, we are duly expected by the public and other agencies to address MSAT impacts in our environmental documents. The FHWA, EPA, the Health Effects Institute, and others have funded and conducted research studies to try to more clearly define potential risks from MSAT emissions associated with highway projects. The FHWA will continue to monitor the developing research in this field.

NEPA Context

The NEPA requires, to the fullest extent possible, that the policies, regulations, and laws of the Federal Government be interpreted and administered in accordance with its environmental protection goals. The NEPA also requires Federal agencies to use an interdisciplinary approach in planning and decision-making for any action that adversely impacts the environment. The NEPA requires and FHWA is committed to the examination and avoidance of potential impacts to the natural and human environment when considering approval of proposed transportation projects. In addition to evaluating the potential environmental effects, we must also take into account the need for safe and efficient transportation in reaching a decision that is in the best overall public interest. The FHWA policies and procedures for implementing NEPA are contained in regulation at 23 CFR Part 771.

Incomplete or Unavailable Information for Project-Specific MSAT Health Impacts Analysis

In FHWA's view, information is incomplete or unavailable to credibly predict the project-specific health impacts due to changes in MSAT emissions associated with a proposed set of highway alternatives. The outcome of such an assessment, adverse or not, would be influenced more by the uncertainty introduced into the process through assumption and speculation rather than any genuine insight into the actual health impacts directly attributable to MSAT exposure associated with a proposed action.

The U.S. Environmental Protection Agency (EPA) is responsible for protecting the public health and welfare from any known or anticipated effect of an air pollutant. They are the lead authority for administering the Clean Air Act and its amendments and have specific statutory obligations with respect to hazardous air pollutants and MSAT. The EPA is in the continual process of assessing human health effects, exposures, and risks posed by air pollutants. They maintain the Integrated Risk Information System (IRIS), which is "a compilation of electronic reports on specific substances found in the environment and their potential to cause human health effects" (EPA, <http://www.epa.gov/iris/>). Each report contains assessments of non-cancerous and cancerous effects for individual compounds and quantitative estimates of risk levels from lifetime oral and inhalation exposures with uncertainty spanning perhaps an order of magnitude.

Other organizations are also active in the research and analyses of the human health effects of MSAT, including the Health Effects Institute (HEI). Two HEI studies are summarized in Appendix D of FHWA's Interim Guidance Update on Mobile source Air Toxic Analysis in NEPA Documents. Among the adverse health effects linked to MSAT compounds at high exposures are; cancer in humans in occupational settings; cancer in animals; and irritation to the respiratory tract, including the exacerbation of asthma. Less obvious is the adverse human health effects of MSAT compounds at current environmental concentrations (HEI, <http://pubs.healtheffects.org/view.php?id=282>) or in the future as vehicle emissions substantially decrease (HEI, <http://pubs.healtheffects.org/view.php?id=306>).

The methodologies for forecasting health impacts include emissions modeling; dispersion

modeling; exposure modeling; and then final determination of health impacts - each step in the process building on the model predictions obtained in the previous step. All are encumbered by technical shortcomings or uncertain science that prevents a more complete differentiation of the MSAT health impacts among a set of project alternatives. These difficulties are magnified for lifetime (i.e., 70 year) assessments, particularly because unsupportable assumptions would have to be made regarding changes in travel patterns and vehicle technology (which affects emissions rates) over that time frame, since such information is unavailable.

It is particularly difficult to reliably forecast 70-year lifetime MSAT concentrations and exposure near roadways; to determine the portion of time that people are actually exposed at a specific location; and to establish the extent attributable to a proposed action, especially given that some of the information needed is unavailable.

There are considerable uncertainties associated with the existing estimates of toxicity of the various MSAT, because of factors such as low-dose extrapolation and translation of occupational exposure data to the general population, a concern expressed by HEI (<http://pubs.healtheffects.org/view.php?id=282>). As a result, there is no national consensus on air dose-response values assumed to protect the public health and welfare for MSAT compounds, and in particular for diesel PM. The EPA (<http://www.epa.gov/risk/basicinformation.htm#g>) and the HEI (<http://pubs.healtheffects.org/getfile.php?u=395>) have not established a basis for quantitative risk assessment of diesel PM in ambient settings.

There is also the lack of a national consensus on an acceptable level of risk. The current context is the process used by the EPA as provided by the Clean Air Act to determine whether more stringent controls are required in order to provide an ample margin of safety to protect public health or to prevent an adverse environmental effect for industrial sources subject to the maximum achievable control technology standards, such as benzene emissions from refineries. The decision framework is a two-step process. The first step requires EPA to determine an "acceptable" level of risk due to emissions from a source, which is generally no greater than approximately 100 in a million. Additional factors are considered in the second step, the goal of which is to maximize the number of people with risks less than 1 in a million due to emissions from a source. The results of this statutory two-step process do not guarantee that cancer risks from exposure to air toxics are less than 1 in a million; in some cases, the residual risk determination could result in maximum individual cancer risks that are as high as approximately 100 in a million. In a June 2008 decision, the U.S. Court of Appeals for the District of Columbia Circuit upheld EPA's approach to addressing risk in its two step decision framework. Information is incomplete or unavailable to establish that even the largest of highway projects would result in levels of risk greater than deemed acceptable.

Because of the limitations in the methodologies for forecasting health impacts described, any predicted difference in health impacts between alternatives is likely to be much smaller than the uncertainties associated with predicting the impacts. Consequently, the results of such assessments would not be useful to decision makers, who would need to weigh this information against project benefits, such as reducing traffic congestion, accident rates, and fatalities plus improved access for emergency response, that are better suited for quantitative analysis.

Due to the limitations cited, a discussion such as the example provided in this Appendix (reflecting any local and project-specific circumstances), should be included regarding incomplete or unavailable information in accordance with Council on Environmental Quality (CEQ) regulations [40 CFR 1502.22(b)]. The FHWA Headquarters and Resource Center staff Victoria Martinez (787) 766-5600 X231, Bruce Bender (202) 366-2851, and Michael Claggett (505) 820-2047, are available to provide guidance and technical assistance and support.

Attachment D:
Section 106 Coordination

Hayes, Robbie

From: Tammy Sellers <Tammy.Sellers@tn.gov>
Sent: Wednesday, October 16, 2013 7:19 AM
To: Hayes, Robbie
Subject: RE: Reconfirmation of Historic/Architectural Study for SR-115 (PIN 100241.00)

The SHPO letter is still valid.

From: Hayes, Robbie [<mailto:robbie.hayes@urs.com>]
Sent: Tuesday, October 15, 2013 10:59 AM
To: Tammy Sellers
Subject: Reconfirmation of Historic/Architectural Study for SR-115 (PIN 100241.00)

Good morning Tammy,

I am working on the FONSI for this project, the EA was approved on February 27, 2013. I wanted to confirm that the findings were still valid for the Historic/Architectural Report, which found there to be one resource eligible for listing in the NRHP, the Charles Barber House. It was determined, however, that the project would not adversely impact the property. There have been no changes to the project since that time.

Please let me know if you need any supporting materials to accompany this request.

Thanks Tammy,

Robbie

Robbie Hayes, AICP
URS Corporation
1000 Corporate Centre Drive
One Corporate Centre, Suite 250
Franklin, TN 37067
Direct: 615.224.2147
Fax: 615.771.2459
robbie.hayes@urs.com

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Hayes, Robbie

From: Alan Longmire <Alan.Longmire@tn.gov>
Sent: Tuesday, October 15, 2013 11:17 AM
To: Hayes, Robbie
Subject: RE: Reconfirmation of Archaeological Study for SR-115 (PIN 100241.00)

It is still valid.

From: Hayes, Robbie [robbie.hayes@urs.com]
Sent: Tuesday, October 15, 2013 11:51 AM
To: Alan Longmire
Subject: Reconfirmation of Archaeological Study for SR-115 (PIN 100241.00)

Good morning Alan,

I am working on the FONSI for this project, the EA was approved on February 27, 2013. I wanted to confirm that the findings were still valid for the Archaeological Assessment, which found there to be no resources eligible for listing in the NRHP.

Please let me know if you need any supporting materials to accompany this request.

Thanks Alan,

Robbie

Robbie Hayes, AICP
URS Corporation
1000 Corporate Centre Drive
One Corporate Centre, Suite 250
Franklin, TN 37067
Direct: 615.224.2147
Fax: 615.771.2459
robbie.hayes@urs.com<<mailto:robbie.hayes@urs.com>>

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TENNESSEE HISTORICAL COMMISSION
DEPARTMENT OF ENVIRONMENT AND CONSERVATION
2941 LEBANON ROAD
NASHVILLE, TN 37243-0442
(615) 532-1550

November 6, 2001

Ms. Martha Carver
Environmental Planning
TDOT, 9th. Floor Polk Bldg
Nashville, Tennessee, 37219

**RE: ARCHITECTURAL/HISTORIC PROPERTY SURVEY REPORT, FHWA, SR-115/AIRBASE
ROAD TO TN RIVER BR, KNOX, BLOUNT COUNTY**

Ms. Carver:

Pursuant to your request, received on Wednesday, October 31, 2001, this office has reviewed documentation concerning the above-referenced undertaking. This review is a requirement of Section 106 of the National Historic Preservation Act for compliance by the participating federal agency or applicant for federal assistance. Procedures for implementing Section 106 of the Act are codified at 36 CFR 800 (Federal Register, December 12, 2000, 77698-77739)

Considering the information provided, we find that the area of potential effect contains one architectural resource eligible for listing in the National Register of Historic Places affected by this undertaking: the Charles Barber House. We further find that the project as currently proposed will not adversely affect this historic property. You should notify interested persons and make the documentation associated with this finding available to the public.

All borrow areas outside proposed rights-of-way will require separate certification as specified under Section 107.06-Federal Aid Provisions. If your agency proposes any modifications in current project plans or discovers any archaeological remains during the ground disturbance or construction phase, please contact this office to determine what further action, if any, will be necessary to comply with Section 106 of the National Historic Preservation Act.

This office appreciates your cooperation.

Sincerely,

Herbert L. Harper
Executive Director and
Deputy State Historic
Preservation Officer

HLH/jyg



TENNESSEE HISTORICAL COMMISSION
DEPARTMENT OF ENVIRONMENT AND CONSERVATION
2941 LEBANON ROAD
NASHVILLE, TN 37243-0442
(615) 532-1550

February 1, 2002

Mr. Gerald Kline
Tennessee Department of Transportation
Environmental Planning Office
Suite 900, James K. Polk Building
505 Deaderick Street
Nashville, Tennessee 37243-0334

RE: FHWA, ARCHAEOLOGICAL ASSESSMENT, SR-115/AIR BASE ROAD-
BUCK-KARNES BRIDGE, KNOXVILLE, KNOX COUNTY, TN

Dear Mr. Kline:

At your request, our office has reviewed the above-referenced archaeological survey report in accordance with regulations codified at 36 CFR 800 (Federal Register, December 12, 2000, 77698-77739). Based on the information provided, we find that the project area contains no archaeological resources eligible for listing in the National Register of Historic Places.

Therefore, this office has no objection to the implementation of this project. If project plans are changed or archaeological remains are discovered during construction, please contact this office to determine what further action, if any, will be necessary to comply with Section 106 of the National Historic Preservation Act.

Your cooperation is appreciated.

Sincerely,

Herbert L. Harper
Executive Director and
Deputy State Historic
Preservation Officer

HLH/jmb



**STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION**

ENVIRONMENTAL DIVISION
SUITE 900, JAMES K. POLK BUILDING
505 DEADERICK STREET
NASHVILLE, TENNESSEE 37243-1402
(615) 741-3655

JOHN C. SCHROER
COMMISSIONER

BILL HASLAM
GOVERNOR

October 21, 2013

The Cherokee Nation
17675 South Muscogee
Tahlequah, OK 74465
Attn: Dr. Richard Allen, Policy Analyst

SUBJECT: Section 106 Initial Coordination for Proposed SR-115 Widening Project, From I-140 in Blount County to the Bridge Spanning the Tennessee River in Knox County, Tennessee

Dear Dr. Allen:

The Tennessee Department of Transportation (TDOT) in cooperation with the Federal Highway Administration (FHWA) is proposing to widen SR-115 from I-140 in Blount County to the Bridge spanning the Tennessee River in Knox County (maps attached). The project will widen the four-lane highway to a six-lane, full-access control highway with a center median barrier, seven interchanges, and frontage/collectors roads; one section will be eight lanes. The project length is approximately 7.4 miles. Approximately 128 acres of additional right-of-way will be required.

TDOT initially distributed Native American Coordination for this project to nine tribes in May 2000. The Chickasaw Nation responded in August 2000. No other tribes responded. The TN-SHPO concurred in a letter dated February 1, 2002, that the project area contains no archaeological resources eligible for the National Register of Historic Places. Due to the time lapse, TDOT is redistributing the coordination.

The National Historic Preservation Act (NHPA) recognizes that federally funded undertakings, like the subject project, can affect historic properties to which your tribe attaches religious, cultural, and historic significance. In accordance with 36 CFR 800 regulations implementing compliance with Section 106 of the NHPA, I would like to know if you have information you could share with me about tribal concerns in the project area and if you wish to be a consulting party on the project? Early awareness of your concerns can serve to protect historic properties valued by your tribe.

If you act as a consulting party you will receive archaeological assessment reports and related documentation, be invited to attend project meetings with FHWA, TDOT, and the Tennessee State Historic Preservation Office (TN-SHPO), if any are held, and be asked to provide input throughout the process. If you choose to not act as a consulting party at this time, you can do so at a later date simply by notifying me.

Please respond to me via letter, telephone (615-741-5257), fax (615-741-1098), or E-mail (Gerald.Kline@tn.gov). I respectfully request responses (email is preferred) to project reports and other materials within thirty (30) days of receipt if at all possible. Thank you for your assistance.

Sincerely,

Gerald Kline
Transportation Specialist I
Archaeology Program Manager

Enclosure

cc Robin Dushane, Eastern Shawnee Tribe of Oklahoma
Kim Jumper, Shawnee Tribe
Lisa Baker, United Keetowah Band of Cherokee Indians

Tyler Howe, Eastern Band of Cherokee Indians
Emman Spain, Muscogee (Creek) Nation

TDOT PIN# 100241.00 – Region 1



Robbie D. Jones

From: Lisa LaRue-Baker - UKB THPO <ukbthpo-larue@yahoo.com>
Sent: Monday, October 28, 2013 1:31 PM
To: Robbie D. Jones
Cc: verna; Cindy Hair
Subject: Re: Section 106 Coordination, Blount & Knox Co., TN #100241.00

The United Keetoowah Band of Cherokee Indians in Oklahoma has reviewed your project under Section 106 of the NHPA, and at this time, have no comments or objections. However, if any human remains are inadvertently discovered, please cease all work and contact us immediately.

Thank you,

Lisa C. Baker

Acting THPO
United Keetoowah Band of Cherokee Indians in Oklahoma
PO Box 746
Tahlequah, OK 74465

c 918.822.1952
ukbthpo-larue@yahoo.com

[Please FOLLOW our historic preservation page and LIKE us on FACEBOOK](#)

From: Robbie D. Jones <Robbie.D.Jones@tn.gov>
To: "'ukbthpo-larue@yahoo.com'" <ukbthpo-larue@yahoo.com>
Cc: Robbie D. Jones <Robbie.D.Jones@tn.gov>
Sent: Monday, October 21, 2013 3:25 PM
Subject: Section 106 Coordination, Blount & Knox Co., TN #100241.00

Dear Ms. Baker:

I'm sending this email communication on behalf of Gerald Kline, Archaeology Program Manager for the Tennessee Department of Transportation. Please see the attached letters and maps for the following projects:

SR-115, Blount & Knox Counties, Tennessee (PIN# 100241.00)

If you have any questions or need additional information, please contact Gerald Kline at (615) 741-5257 or Gerald.Kline@tn.gov .

Thank you for your assistance in this matter.

Robbie



the
Chickasaw
Nation HEADQUARTERS

Arlington at Mississippi / Box 1548 / Ada, OK 74821-1548 / (580) 436-2603

Bill Anoatubby
Governor

Jefferson Keel
Lieutenant
Governor

August 21, 2000

Gerald Kline
Tennessee Department of Transportation
Suite 900, 505 Deaderick Street
Nashville, TN 37243-0334

Dear Mr. Kline:

This is in response to your letter regarding construction to propose the widening of U.S. 129/State Route 115 from I-140 (Pellissippi Parkway), near Maryville in Blount County, to the Tennessee River, near Knoxville in Knox County. At this time we are not aware of any culturally sensitive or sacred sites in or near the project site. However, this area was located in our historic hunting and trading area and the possibility for uncovering such sites is highly possible. We expect that if there is an inadvertent discovery that all work would cease and we would be notified as soon as possible, according to all applicable federal laws that apply.

Thank you for your sensitivity to these issues. If you have any questions, please contact Mrs. Rena Duncan, director of cultural resources, at (580) 332-8685.

Sincerely,

A handwritten signature in cursive script that reads "Jefferson Keel".

Jefferson Keel, Lt. Governor
The Chickasaw Nation



Attachment E:
Section 7 Coordination

Hayes, Robbie

From: Keven Brown <Keven.Brown@tn.gov>
Sent: Thursday, October 24, 2013 10:12 AM
To: Hayes, Robbie
Subject: RE: Reconfirmation of Ecology Study for SR-115 (PIN 100241.00)

Follow Up Flag: Follow up
Flag Status: Flagged

Robbie,

This information should still be valid. kb

From: Hayes, Robbie [<mailto:robbie.hayes@urs.com>]
Sent: Tuesday, October 22, 2013 9:17 AM
To: Keven Brown
Subject: RE: Reconfirmation of Ecology Study for SR-115 (PIN 100241.00)

Good morning Keven,

Just left a voice mail and thought that an e-mail might be easier for you to respond. FHWA is wanting a reconfirmation from TDOT staff that the Ecology Study and agency letters are still valid. Let me know if you need any of the supporting documentation to make this concurrence. The Ecology Study and BA for the Indiana Bat were both completed by Third Rock; URS took over this project and completed the EA in February 27, 2013.

Thanks Keven!

Robbie

Robbie Hayes, AICP
URS Corporation
1000 Corporate Centre Drive
One Corporate Centre, Suite 250
Franklin, TN 37067
Direct: 615.224.2147
Fax: 615.771.2459
robbie.hayes@urs.com

From: Hayes, Robbie
Sent: Tuesday, October 15, 2013 12:22 PM
To: 'Keven Brown'
Subject: Reconfirmation of Ecology Study for SR-115 (PIN 100241.00)

Keven,

I am working on the FONSI for this project, the EA was approved by FHWA on February 27, 2013. I wanted to confirm that the findings were still valid for Section 7 Coordination, which is summarized in the table below.

There have been no changes to the project since that time.

Please let me know if you need any supporting materials to accompany this request.

Thanks Keven!

Robbie

NATURAL RESOURCES	
Water Quality/Aquatic Resources	8 streams totaling 2,445 linear feet (l.f.) of impact, 5 wet weather conveyances totaling 645 l.f. of impact
Wetland	0.02 acre impact to 1 wetland
Channelization of Streams	None
Floodplains	13.27 acres
Threatened and Endangered Species (Federal and State)	Requirements of Section 7 have been met and "not likely to adversely affect" Indiana bat per letters from USFWS dated September 21, 2011 and November 15, 2011
Invasive Species	Privet (<i>Ligustrum</i> sp.), bush honeysuckle (<i>Lonicera maackii</i>), and multiflora rose (<i>Rosa multiflora</i>)
Wild and Scenic Rivers	None

Robbie Hayes, AICP
URS Corporation
1000 Corporate Centre Drive
One Corporate Centre, Suite 250
Franklin, TN 37067
Direct: 615.224.2147
Fax: 615.771.2459
robbie.hayes@urs.com

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United States Department of the Interior

FISH AND WILDLIFE SERVICE

446 Neal Street
Cookeville, TN 38501

April 18, 2013

Ms. Ann Andrews
Tennessee Department of Transportation
Environmental Planning and Permits
James K. Polk Building, Suite 900
505 Deaderick Street
Nashville, Tennessee 37243-0334

Subject: FWS #13-CPA-0372. Proposal to construct State Route 115 from Interstate 140 (Pellissippi Parkway) to just north of the Cherokee Trail Interchange; PIN #100241.00, Blount and Knox counties, Tennessee.

Dear Ms. Andrews:

Thank you for your letter dated March 25, 2013, transmitting an environmental assessment for the proposed construction to State Route 115 from Interstate 140 Pellissippi Parkway to just north of the Cherokee Trail Interchange in Blount and Knox counties, Tennessee. The purpose of the project is to correct roadway deficiencies, improve safety, and increase roadway capacity and level of service. Personnel of the U.S. Fish and Wildlife Service have reviewed the information provided and offer the following comments.

In a letter dated September 21, 2011, we concurred with the Tennessee Department of Transportation's (TDOT) determination of "not likely to adversely affect" for the Indiana bat (*Myotis sodalis*) due to negative survey results. Unless new information otherwise indicates Indiana bat use of the area, this survey will be valid until April 1, 2014. TDOT has additionally committed to a cutting timeframe restriction that requires removal of all trees with a diameter of five inches or greater between October 15 and March 31.

We are unaware of any federally listed or proposed species that would be impacted by this project. Therefore, based on the best information available at this time, we believe that the requirements of section 7 of the Endangered Species Act of 1973, as amended, are fulfilled for all species that currently receive protection under the Act. Obligations under section 7 of the Act must be reconsidered if (1) new information reveals impacts of the proposed action that may affect listed species or critical habitat in a manner not previously considered, (2) the proposed action is subsequently modified to include activities which were not considered during this consultation, or (3) new species are listed or critical habitat designated that might be affected by the proposed action.

If you have any questions regarding our comments, please contact John Griffith of my staff at 931/525-4995 or by email at john_griffith@fws.gov.

Sincerely,

A handwritten signature in cursive script that reads "Mary E. Jennings". The signature is written in black ink and is positioned above the printed name and title.

Mary E. Jennings
Field Supervisor



United States Department of the Interior

FISH AND WILDLIFE SERVICE

446 Neal Street
Cookeville, TN 38501

November 15, 2011

Mr. Jerry Melson
Tennessee Department of Transportation
Environmental Planning and Permits
James K. Polk Building, Suite 900
505 Deaderick Street
Nashville, Tennessee 37243-0334

Subject: FWS #11-CPA-0854. Proposal to construct State Route 115 from Interstate 140 Pellissippi Parkway to just north of the Cherokee Trail Interchange; PIN #100241.00, .02, .03, .04, P.E. 05005-1231-14, 47046-1269-14, 47026-1270-14, 47026-1263-14, Blount and Knox counties, Tennessee.

Dear Mr. Melson:

Thank you for your letter dated October 20, 2011, transmitting an environmental assessment for the proposed construction to State Route 115 from Interstate 140 Pellissippi Parkway to just north of the Cherokee Trail Interchange in Blount and Knox counties, Tennessee. Personnel of the U.S. Fish and Wildlife Service have reviewed the information provided and offer the following comments.

In a letter dated September 21, 2011, we concurred with the Tennessee Department of Transportation's (TDOT) determination of "not likely to adversely affect" for the Indiana bat (*Myotis sodalis*) due to negative survey results. As an additional protective measure, TDOT has committed to removal of any suitable Indiana bat roosting trees between the timeframe of October 15 and March 31. Therefore, based on the best information available at this time, we believe that the requirements of section 7 of the Endangered Species Act of 1973, as amended, are fulfilled. Obligations under section 7 of the Act must be reconsidered if (1) new information reveals impacts of the proposed action that may affect listed species or critical habitat in a manner not previously considered, (2) the proposed action is subsequently modified to include activities which were not considered during this consultation, or (3) new species are listed or critical habitat designated that might be affected by the proposed action.

If you have any questions regarding our comments, please contact John Griffith of my staff at 931/525-4995 or by email at john_griffith@fws.gov.

Sincerely,

A handwritten signature in cursive script that reads "Mary E. Jennings". The signature is written in black ink and is positioned above the typed name.

Mary E. Jennings
Field Supervisor



United States Department of the Interior

FISH AND WILDLIFE SERVICE
446 Neal Street
Cookeville, TN 38501

September 21, 2011

Mr. Keven Brown
Tennessee Department of Transportation
Environmental Planning and Permits
James K. Polk Building, Suite 900
505 Deaderick Street
Nashville, Tennessee 37243-0334

Subject: FWS #11-CPA-0854. Proposal to construct State Route 115 from Interstate 140 Pellissippi Parkway to just north of the Cherokee Trail Interchange; PIN #100241.00, .02, .03, .04, P.E. 05005-1231-14, 47046-1269-14, 47026-1270-14, 47026-1263-14, Blount and Knox counties, Tennessee.

Dear Mr. Brown:

Thank you for your letter dated August 31, 2011, transmitting acoustic and mist netting survey results for the proposed construction to State Route 115 from Interstate 140 Pellissippi Parkway to just north of the Cherokee Trail Interchange in Blount and Knox counties, Tennessee. At the request of our office, surveys were conducted along the proposed corridor to determine if the area is being utilized as summer roosting habitat by the federally endangered Indiana bat (*Myotis sodalis*). Personnel of the U.S. Fish and Wildlife Service have reviewed the information provided and offer the following comments.

Joint mist netting and acoustical studies were performed from July 19 through July 27, 2011, at six sites determined to contain suitable habitat for the Indiana bat. The acoustical study resulted in the recording of 2,904 bat calls, of which none were identified as Indiana bats. The mist netting efforts resulted in the capture of 11 individual bats, representing three common species. The Tennessee Department of Transportation (TDOT) has concluded that the project is "not likely to adversely affect" the Indiana bat because the no Indiana bats were recorded during the surveys.

Due to negative survey results for the Indiana bat, we concur with TDOT's finding of "not likely to adversely affect" for this species. Although it is likely that this project would have an insignificant effect on the Indiana bat, we would appreciate consideration given to the removal of trees with a DBH (diameter at breast height) of five inches or greater from October 15 through March 31 to further minimize potential for harm to the Indiana bat. Based on the best information available at this time, we believe that the requirements of section 7 of the Endangered Species Act of 1973, as

amended, are fulfilled. Obligations under the Act must be reconsidered if (1) new information reveals impacts of the proposed action that may affect listed species or critical habitat in a manner not previously considered, (2) the proposed action is subsequently modified to include activities which were not considered during this consultation, or (3) new species are listed or critical habitat designated that might be affected by the proposed action.

If you have any questions regarding our comments, please contact John Griffith of my staff at 931/525-4995 or by email at john_griffith@fws.gov.

Sincerely,

A handwritten signature in blue ink that reads "Mary E. Jennings". The signature is written in a cursive, flowing style.

Mary E. Jennings
Field Supervisor



United States Department of the Interior

FISH AND WILDLIFE SERVICE

446 Neal Street
Cookeville, TN 38501

June 11, 2010

Mr. Keven Brown
Tennessee Department of Transportation
Environmental Planning and Permits
James K. Polk Building, Suite 900
505 Deaderick Street
Nashville, Tennessee 37243-0334

Subject: Proposal to widen State Route 115 from Pellissippi Parkway to north of Cherokee Trail; PIN #100241.00, Blount and Knox counties, Tennessee.

Dear Mr. Brown:

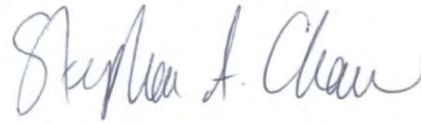
Thank you for your email of May 21, 2010, requesting an updated letter for the proposed widening of approximately 7 miles of State Route 115 in Blount and Knox counties, Tennessee. The Tennessee Department of Transportation proposes to widen the existing four-lane roadway to a six-lane facility. This project was originally coordinated with our office on August 3, 2000. Personnel of the U.S. Fish and Wildlife Service (Service) have reviewed the subject proposal and offer the following comments.

Information available to the Service does not indicate that wetlands exist in the vicinity of the proposed project. However, our wetland determination has been made in the absence of a field inspection and does not constitute a wetland delineation for the purposes of Section 404 of the Clean Water Act. The Corps of Engineers should be contacted if other evidence, particularly that obtained during an on-site inspection, indicates the potential presence of wetlands.

Our database indicates that potential roost habitat for the Indiana bat (*Myotis sodalis*) may exist within the project corridor and would likely be altered by the proposed action. A qualified biologist should assess potential impacts and determine if the proposed project may affect this species. If this project involves Federal funding, the lead Federal agency should submit a copy of your assessment and findings to this office for review and concurrence. A finding of "may affect" could require initiation of formal consultation by the lead Federal agency. If no Federal funding is involved, you may submit your assessment and findings directly to us.

If you have any questions regarding our comments, please contact John Griffith of my staff at 931/528-6481 (ext. 228) or by email at john_griffith@fws.gov.

Sincerely,

A handwritten signature in cursive script that reads "Stephen A. Chew".

for Mary E. Jennings
Field Supervisor



STATE OF TENNESSEE

DEPARTMENT OF ENVIRONMENT AND CONSERVATION

Division of Natural Areas
Natural Heritage Program
7th Floor L&C Tower
401 Church Street
Nashville, Tennessee 37243
Phone 615/532-0431 Fax 615/532-0046

May 9, 2013

Ann Andrews
Transportation Manager II
Tennessee Department of Transportation
Suite 1000, James K. Polk Building
Nashville, TN 37243-0349

Subject: State Route 115 (U.S. 129, Alcoa Highway) From I-140 (Pellissippi Parkway) to
North of the Cherokee Trail Interchange (TDOT_PIN100241)
Blount and Knox Counties, Tennessee
Rare Species Database Review

Dear Sir or Madam:

Thank you for the opportunity to perform a rare species database review for the widening of a section of State Route 115 (U.S. 129, Alcoa Highway) from I-140 (Pellissippi Parkway) to north of the Cherokee Trail Interchange project, located in Blount and Knox Counties, Tennessee.

We have reviewed the state's natural heritage database with regard to the project boundaries, and we find that the following rare species have been observed previously within one mile of the project:

Type	Scientific Name	Common Name	Global Rank	St. Rank	Fed. Prot.	St. Prot.	Habitat
Invertebrate Animal	Athearnia anthonyi	Anthony Riversnail	G1	S1	LE,XN	E	Larger rivers and downstream stretches of lg creeks, on cobble/boulder substrates adj. riffles; portions of upper TN River basin.
Vascular Plant	Aureolaria patula	Spreading False-foxglove	G3	S3	--	S	Oak Woods And Edges
Vascular Plant	Boechera patens	Spreading Rockcress	G3	S1	--	E	Moist Rocky Woods
Vascular Plant	Cardamine flagellifera	Running Bittercress	G3	S2	--	T	Mountain Stream Banks

Type	Scientific Name	Common Name	Global Rank	St. Rank	Fed. Prot.	St. Prot.	Habitat
Vascular Plant	<i>Cimicifuga rubifolia</i>	Appalachian Bugbane	G3	S3	--	T	Rich Woods
Invertebrate Animal	<i>Dromus dromas</i>	Dromedary Pearlymussel	G1	S1	LE	E	Medium-large rivers with riffles and shoals w/ relatively firm rubble, gravel, and stable substrates; Tennessee & Cumberland systems.
Vertebrate Animal	<i>Falco peregrinus</i>	Peregrine Falcon	G4	S1B	No Status	E	Varied habitats including farmlands, marshes, river mouths, and cities; often nests on ledges.
Other (Ecological)	Heron rookery	Heron Rookery	GNR	SNR	--	Rare, Not State Listed	
Vascular Plant	<i>Hydrastis canadensis</i>	Goldenseal	G4	S3	--	S-CE	Rich Woods
Invertebrate Animal	<i>Io fluvialis</i>	Spiny Riversnail	G2	S2	--	Rare, Not State Listed	Shallow waters of shoals that are rapid to moderate and well-oxygenated; Tennessee River & main tributaries; E Tennessee.
Vascular Plant	<i>Panax quinquefolius</i>	American Ginseng	G3G4	S3S4	--	S-CE	Rich Woods
Invertebrate Animal	<i>Plethobasus cooperianus</i>	Orangefoot Pimpleback	G1	S1	LE	E	Large rivers in sand-gravel-cobble substrates in riffles and shoals in deep flowing water; Cumberland & Tennessee river systems.
Vertebrate Animal	<i>Tyto alba</i>	Barn Owl	G5	S3	--	D	Open and partly open country, often around human habitation; farms.

Within four miles of the project the following additional rare species have been reported:

Type	Scientific Name	Common Name	Global Rank	St. Rank	Fed. Prot.	St. Prot.	Habitat
Vertebrate Animal	<i>Cryptobranchus alleganiensis</i>	Hellbender	G3G4	S3	No Status	D	Rocky, clear creeks and rivers with large shelter rocks.
Vertebrate Animal	<i>Etheostoma marmorpinnum</i>	Marbled Darter	G1	S1	LE	E	Pools and moderate runs with clean pebbles, cobble, & small boulders; lower Little River (Tennessee River drainage).
Nonvascular Plant	<i>Funaria americana</i>	A Moss	G3?	S1?	--	T	Limestone Bluffs And Barrens
Invertebrate Animal	<i>Fusconaia cuneolus</i>	Finerayed Pigtoe	G1	S1	LE	E	Riffles of fords and shoals of mod gradient streams in firm cobble and gravel substrates; middle & upper Tennessee River watershed.

Type	Scientific Name	Common Name	Global Rank	St. Rank	Fed. Prot.	St. Prot.	Habitat
Vertebrate Animal	<i>Gyrinophilus gulolineatus</i>	Berry Cave Salamander	G1Q	S1	--	T	Aquatic cave obligate; Ridge & Valley; formerly included with <i>G. palleucus</i> .
Vertebrate Animal	<i>Haliaeetus leucocephalus</i>	Bald Eagle	G5	S3	--	D	Areas close to large bodies of water; roosts in sheltered sites in winter; communal roost sites common.
Vertebrate Animal	<i>Hemitremia flammea</i>	Flame Chub	G3	S3	--	D	Springs and spring-fed streams with lush aquatic vegetation; Tennessee & middle Cumberland river watersheds.
Vertebrate Animal	<i>Ixobrychus exilis</i>	Least Bittern	G5	S2B	--	D	Marshes with scattered bushes or other woody growth; readily uses artificial wetland habitats.
Invertebrate Animal	<i>Lasmigona holstonia</i>	Tennessee Heelsplitter	G3	S2	--	Rare, Not State Listed	Spring runs, creeks, & small rivers, in subst of sand & mud; upper Tenn & Conasauga river watersheds; Blue Ridge & Ridge & Valley.
Vertebrate Animal	<i>Myotis grisescens</i>	Gray Myotis	G3	S2	LE	E	Cave obligate year-round; frequents forested areas; migratory.
Vertebrate Animal	<i>Noturus flavipinnis</i>	Yellowfin Madtom	G1	S1	LT,XN	E	Medium size to large creeks and small rivers that are unpolluted & relatively unsilted; upper Tennessee River watershed.
Vertebrate Animal	<i>Percina aurantiaca</i>	Tangerine Darter	G4	S3	--	D	Large-moderate size headwater tribs to Tennessee River, in clear, fairly deep, rocky pools, usually below riffles.
Vertebrate Animal	<i>Percina macrocephala</i>	Longhead Darter	G3	S2	--	T	Clear, larger upland creeks and small-med rivers, usually in rocky flowing pools upst/dnst rubble riffles; Tenn & Cumb river watersheds.
Vertebrate Animal	<i>Percina tanasi</i>	Snail Darter	G2G3	S2S3	LT	T	Sand and gravel shoals of moderately flowing, vegetated, large creeks; upper Tennessee River watershed.
Vertebrate Animal	<i>Rallus elegans</i>	King Rail	G4	S2	--	D	Marshes, upland-wetland marsh edges, flooded farmlands, shrub swamps.
Nonvascular Plant	<i>Rhachithecium perpusillum</i>	Budding Tortula	G4G5	SH	--	S	Bark of Hardwoods
Vertebrate Animal	<i>Sorex longirostris</i>	Southeastern Shrew	G5	S4	--	D	Various habitats including wet meadows, damp woods, and uplands; statewide.

May 9, 2013

Page 4

We note from the EA that the vast majority of the species above have been evaluated and determined to be unaffected or minimally affected by the action alternative. Since the original TDOT evaluation of the project area, the burrowing crayfish *Cambarus deweesae* (Valley Flame Crayfish) has been documented in southern Roane County on the south side of the Tennessee River. This discovery expands the prospect that this state endangered species may occur in Knox County in suitable wetlands or hydric soils. Our office would appreciate additional detail regarding the wetland described on p. 3-49 of the EA, specifically any documentation of the presence of burrowing crayfish and site photos. Staff from this office or that of the Tennessee Wildlife Resources Agency (TWRA) may investigate the site at a later date to determine what species are present. Additionally, we would appreciate copies of any bat survey reports produced during the summer July 2011 survey indicated on p. 3-54.

Should suitable habitat exist on or immediately downstream of the project area, we ask that plans provide for the protection of the species noted above. We ask that you coordinate this project with the TWRA (Rob Todd, rob.todd@tn.gov, 615-781-6577) to ensure that legal requirements for protection of state listed rare animals are addressed. Additionally, we ask that you contact the U.S. Fish and Wildlife Service Field Office, Cookeville, Tennessee (931-525-4970) for comments regarding federally listed species.

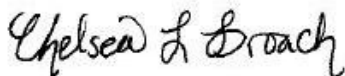
For stabilization of disturbed areas, the Tennessee Natural Heritage Program advocates the use of native trees, shrubs, and warm season grasses, where practicable. Care should be taken to prevent re-vegetation of disturbed areas with plants listed by the Tennessee Exotic Pest Plant Council as harmful exotic plants: <http://www.tneppc.org/>

Please keep in mind that not all of Tennessee has been surveyed and that a lack of records for any particular area should not be construed to mean that rare species necessarily are absent. For information regarding species protection status and ranks, please visit <http://www.tn.gov/environment/na/pdf/Status&Ranks.pdf>.

To assist in determining whether rare species are located at a given site, the Tennessee Natural Heritage Program has implemented a publicly accessible website where rare species data lists by county, quadrangle, watershed, and MS4 boundaries can be obtained: http://environment-online.state.tn.us:8080/pls/enf_reports/f?p=9014:3:3875605994273657.

Thank you for considering Tennessee's rare species throughout the planning of this project. Should you have any questions, please do not hesitate to contact David at (615) 532-0441 or david.withers@tn.gov.

Sincerely,



Chelsea L. Broach
Interim Data Manager



David Ian Withers
Natural Heritage Zoologist

Attachment F:
Hazardous Materials Coordination

Hayes, Robbie

From: Jeffrey Ballard <Jeffrey.Ballard@tn.gov>
Sent: Wednesday, October 16, 2013 11:36 AM
To: Hayes, Robbie
Subject: RE: Reconfirmation of Phase I Hazmat Study for SR-115 (PIN 100241.00)

Follow Up Flag: Follow up
Flag Status: Flagged

Robbie,

It doesn't look like there have been any substantial changes along the corridor other than a lot of building demolition in the .01 section. The only thing that might need to be added to your note below would be the requirement to conduct an Asbestos-Containing Material survey on the bridges if they are going to be modified or demolished.

Let me know if you have questions or comments.

Jeff

Jeffrey Ballard, P.E.
K.S. Ware & Associates

Hazmat Coordinator
Social and Cultural Resources Office
Environmental Division
Tennessee Department of Transportation
505 Deaderick Street – Suite 900
Nashville, TN 37243

615.532.8684
jeffrey.ballard@tn.gov

For Jim Ozment

From: Hayes, Robbie [<mailto:robbie.hayes@urs.com>]
Sent: Tuesday, October 15, 2013 12:50 PM
To: Jeffrey Ballard
Subject: RE: Reconfirmation of Phase I Hazmat Study for SR-115 (PIN 100241.00)

Here is the text we included in the EA document, should you find this helpful. Sorry for not including on previous e-mail.

Hazardous materials are substances that have, or would have (when combined with other materials) a harmful effect on humans or the natural environment. Hazardous materials are primarily regulated under the *Resource Conservation and Recovery Act (RCRA) of 1976*, as amended; the *Comprehensive Environmental Response, Compensation, and Liability Act (CERCLA) of 1980*; and the *Superfund Amendments and Reauthorization Act (SARA) of 1986*.

A *Phase I Preliminary Assessment Study* was conducted by TDOT for the project corridor (Arcadis, June 2010). The study

involved a visual examination of accessible properties located within 250 feet of the centerline for the presence of hazardous/toxic substances or petroleum storage. In addition, an Environmental Data Resources Inc. (EDR) hazardous materials database report provided information on the study corridor. Records were reviewed at the TDEC-Division of Underground Storage Tanks (DUST) and the Knoxville Environmental Field Office, underground storage tank (UST) records. TDEC's 303(d) list of impaired streams within the project corridor was also reviewed.

Based on visual observations during the site reconnaissance, eight sites along the project corridor were assigned low-risk rankings due to their distance from the project corridor, their current listing status with TDEC-DUST, or their identification as a 303(d) listed stream. Ten sites were assigned a high-risk ranking due to known impacts to subsurface media (soil or groundwater) from source areas such as leaking USTs. A high-risk ranking may be assigned based on indication of past and present management and handling of petroleum products contained in USTs. The majority of sites contain active or abandoned petroleum USTs.

The proposed project will be reevaluated prior to construction to determine if sites will require a Phase II site assessment. In the event hazardous substances/wastes are encountered within the proposed ROW, their disposition shall be subject to the applicable sections of the *Federal Resources Conservation and Recovery Act*, as amended; the *Comprehensive Environmental Response, Compensation, and Liability Act*, as amended; and the *Tennessee Hazardous Waste Management Act of 1983*.

Robbie Hayes, AICP
URS Corporation
1000 Corporate Centre Drive
One Corporate Centre, Suite 250
Franklin, TN 37067
Direct: 615.224.2147
Fax: 615.771.2459
robbie.hayes@urs.com

Attachment G:
Agency Comments Received on
February 2013 Environmental Assessment



STATE OF TENNESSEE
DEPARTMENT OF ENVIRONMENT AND CONSERVATION
KNOXVILLE ENVIRONMENTAL FIELD OFFICE
3711 MIDDLEBROOK PIKE
KNOXVILLE, TENNESSEE 37921-6538

PHONE (865) 594-6035 STATEWIDE 1-888-891-8332 FAX (865) 594-6105

April 3, 2013

Mrs. Ann Andrews
Department of Transportation
Environmental Division
Suite 900, James K. Polk Building
505 Deaderick Street
Nashville, TN 37243-1402

RE: State Route 115 (US 129, Alcoa Highway) from Interstate 140 (Pellissippi Parkway) to North of the Cherokee Trail Interchange, Blount and Knox Counties, TN
Environmental Impact Study

Dear Mrs. Andrews:

The Division of Solid Waste Management has reviewed the March 25, 2013, letter the March 21, 2013, Environmental Assessment Report with plans for the above referenced project. We have found no solid waste issues in the vicinity of this project location. Also, there is no hazardous waste Treatment-Storage-Disposal (TSD) facility located in the neighborhood of this project location. There are some hazardous waste generator facilities located in Blount and Knox County, TN, in the vicinity, but they will not have any adverse impact on this project. If any solid or hazardous waste is generated as a result of construction activities from this project, the Division of Solid Waste Management, Knoxville Environmental Field Office can provide assistance for proper management. The Division of Solid Waste Management, Knoxville EFO can be reached at (865) 594-6035.

There are no other concerns related to Tennessee Solid and Hazardous Waste Program.

Please contact me at (865) 594-5468, if there are questions or additional information is needed.

Sincerely,

Revendra Awasthi
Environmental Field Office Manager
Division of Solid Waste Management

cc: DSWM, KEFO Files
DSWM, Nashville Central Office
Lisa Hughey, DSWM, Nashville Central Office

RECEIVED

APR 10 2012

TDOT - ENVIRONMENTAL DIVISION

U.S. Department of
Homeland Security

United States
Coast Guard



Commander
Eighth Coast Guard District

1222 Spruce Street, Room 2.102D
St. Louis, MO 63103-2832
Staff Symbol: dwb
Phone: (314) 269-2382
Fax: (314) 269-2737
Email: david.a.orzechowski@uscg.mil
www.uscg.mil/d8/westerriversbridges

16591.1/635.5 TNR
April 11, 2013

Ms. Ann Andrews
Environmental Documentation Office
Tennessee Department of Transportation
Suite 900, James K. Polk Building
505 Deaderick Street
Nashville, TN 37243-0334

SUBJ: STATE ROUTE 115 (U.S. 129, ALCOA HIGHWAY) FROM I-140 (PELLISSIPPI PARKWAY) TO NORTH OF THE CHEROKEE TRAIL INTERCHANGE, BLOUNT AND KNOX COUNTIES, TENNESSEE

Dear Ms. Andrews:

This is in reply to your letter of March 21, 2013, concerning the Environmental Assessment (EA) for the subject project. The EA addresses the impacts the project will have on the natural surroundings and land usage for transportation, but the document does not include a discussion of impacts to navigation on the waterways that are crossed. We have reviewed the EA from a navigational perspective and offer the following comments:

- a. Title page: Ensure the U.S. Coast Guard is shown as a Cooperating Agency.
- b. Table of Contents: Include a section entitled "Navigation Impacts".
- c. The Little River is a tributary to the Tennessee River at mile 635.5, and Knob Creek is a tributary to the Tennessee River at mile 637.6. Little River and Knob Creek are located within the embayments of the Tennessee River and are considered to be navigable waterways of the United States for bridge administration purposes at the bridge sites. Improvements to existing bridges that will alter the navigational clearances provided by the bridges or significantly alter the structure of the bridges will require a bridge permit or bridge permit amendment.
- d. If bridge permits are required for the project, the Coast Guard will need a Water Quality Certificate from the appropriate state agency which states that the project complies with the provisions of Section 401 of the Clean Water Act.

We appreciate the opportunity to comment on the project in this early stage. You can contact David Orzechowski at the above telephone number if you have questions regarding our comments or requirements.

Sincerely,

A handwritten signature in black ink, appearing to read "Eric Washburn".

ERIC A. WASHBURN
Bridge Administrator, Western Rivers
By direction of the District Commander



STATE OF TENNESSEE
DEPARTMENT OF ENVIRONMENT AND CONSERVATION
DIVISION OF REMEDIATION
401 CHURCH STREET, 4TH FLOOR L&C ANNEX
NASHVILLE, TENNESSEE 37243

April 11, 2013

Jerry Melson
Transportation Planner 3
Department of Transportation, Environmental Division
James K. Polk Building
505 Deaderick Street, Suite 900
Nashville, Tennessee 37243

Re: SR 115, Alcoa EA Blount-Knox Counties

Dear Mr. Melson:

The Division of Remediation (DoR) received your environmental review request on April 9, 2013, regarding the State Route 115 (U.S. 129, Alcoa Highway) from Interstate 140 (Pellissippi Parkway) to north of the Cherokee Trail Interchange, Blount and Knox Counties, TN. After reviewing your maps and information, and our project files, we concluded that there are no known active DoR sites on or adjacent to the property in question.

If you have any further questions, do not hesitate to call Erin Sutton at our Knoxville Environmental Field Office at (865) 594-5480.

Sincerely,

Andy Binford
Director

RAB:RED

cc: Knoxville Environmental Field Office
Central Office files

RECEIVED

APR 15 2012

TDOT - ENVIRONMENTAL DIVISION



United States Department of the Interior

FISH AND WILDLIFE SERVICE

446 Neal Street
Cookeville, TN 38501

April 18, 2013

Ms. Ann Andrews
Tennessee Department of Transportation
Environmental Planning and Permits
James K. Polk Building, Suite 900
505 Deaderick Street
Nashville, Tennessee 37243-0334

Subject: FWS #13-CPA-0372. Proposal to construct State Route 115 from Interstate 140 (Pellissippi Parkway) to just north of the Cherokee Trail Interchange; PIN #100241.00, Blount and Knox counties, Tennessee.

Dear Ms. Andrews:

Thank you for your letter dated March 25, 2013, transmitting an environmental assessment for the proposed construction to State Route 115 from Interstate 140 Pellissippi Parkway to just north of the Cherokee Trail Interchange in Blount and Knox counties, Tennessee. The purpose of the project is to correct roadway deficiencies, improve safety, and increase roadway capacity and level of service. Personnel of the U.S. Fish and Wildlife Service have reviewed the information provided and offer the following comments.

In a letter dated September 21, 2011, we concurred with the Tennessee Department of Transportation's (TDOT) determination of "not likely to adversely affect" for the Indiana bat (*Myotis sodalis*) due to negative survey results. Unless new information otherwise indicates Indiana bat use of the area, this survey will be valid until April 1, 2014. TDOT has additionally committed to a cutting timeframe restriction that requires removal of all trees with a diameter of five inches or greater between October 15 and March 31.

We are unaware of any federally listed or proposed species that would be impacted by this project. Therefore, based on the best information available at this time, we believe that the requirements of section 7 of the Endangered Species Act of 1973, as amended, are fulfilled for all species that currently receive protection under the Act. Obligations under section 7 of the Act must be reconsidered if (1) new information reveals impacts of the proposed action that may affect listed species or critical habitat in a manner not previously considered, (2) the proposed action is subsequently modified to include activities which were not considered during this consultation, or (3) new species are listed or critical habitat designated that might be affected by the proposed action.

If you have any questions regarding our comments, please contact John Griffith of my staff at 931/525-4995 or by email at john_griffith@fws.gov.

Sincerely,

A handwritten signature in cursive script that reads "Mary E. Jennings". The signature is written in black ink and is positioned above the typed name.

Mary E. Jennings
Field Supervisor

Jerry Melson

From: Ann Andrews
Sent: Monday, April 22, 2013 4:12 PM
To: Jerry Melson
Subject: FW: TDOT Project #100241.00: NEPA EA State Route 115 (US 129, Alcoa Hwy)

From: Sagona, Frank - NRCS, Chattanooga, TN [<mailto:Frank.Sagona@tn.usda.gov>]
Sent: Thursday, April 11, 2013 10:17 AM
To: Ann Andrews
Cc: Shearron, Carol - NRCS, Nashville, TN; Chandler, Carol - NRCS, Nashville, TN
Subject: TDOT Project #100241.00: NEPA EA State Route 115 (US 129, Alcoa Hwy)

Mrs. Andrews:

Thank you for the copy of the approved NEPA EA for the proposed highway project in Knox and Blount County. We do not have any comments or questions to the document.

Frank Sagona, Resource Conservationist
USDA Natural Resources Conservation Service
Ecological Sciences & Planning
6183 Adamson Circle
Chattanooga TN 37416
423-894-1687 Ext. 100 (office)
423-453-1935 (cell and voice mail)

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STATE OF TENNESSEE
DEPARTMENT OF ENVIRONMENT AND CONSERVATION
Air Pollution Control Division
9th Floor L&C Annex, 401 Church Street
Nashville, TN 37243-1531

April 19, 2013

Mrs. Ann Andrews
Transportation Manager II
Environmental Division
Tennessee Department of Transportation
Suite 900 James K. Polk Building
505 Deaderick Street
Nashville, TN 37243-1402

Subject: State Route 115 (U.S. 129, Alcoa Highway) from Interstate 140 (Pellissippi Parkway) to North of Cherokee Trail Interchange, Blount and Knox Counties, Tennessee, Pin Number 100241.00.

Dear Mrs. Andrews:

The Division of Air Pollution Control has reviewed the information provided in the letter regarding the Environmental Assessment (EA) for State Route 115 (U.S. 129, Alcoa Highway) from Interstate 140 (Pellissippi Parkway) to North of Cherokee Trail Interchange, Blount and Knox Counties, Tennessee.

In section 3.6.2 of the EA, TDOT failed to mention that both Blount and Knox Counties are now nonattainment for the 2008 8-hour Ozone National Ambient Air Quality Standard, effective July 20, 2012 (*Federal Register* Vol. 77, No. 98 / Monday, May 21, 2012).

This agency's other interests, above what would be addressed through the standard NEPA process, concern the control of fugitive dust and equipment exhaust emissions during the construction phase. Additionally, our concerns include that any structures requiring demolition are asbestos free, as per the requirements of Chapter 1200-3-11, Hazardous Materials. Before the burning any wood waste, please refer to Chapter 1200-3-4; Open Burning at <http://www.tn.gov/sos/rules/1200/1200-03/1200-03-04.pdf>.

In Knox County, the Knox County Department of Air Quality Management has jurisdiction. We request that the local air pollution control program also have the opportunity to review the information on the project, as there might be additional local requirements not mentioned above. The Knox County Department of Air Quality Management can be contacted at (865) 215-5900.

We appreciate the opportunity to provide comments. If you have any questions or comments, please feel free to call me at (615) 532-0554.

Sincerely,

A handwritten signature in cursive script, appearing to read "Barry R. Stephens".

Barry R. Stephens, P.E.
Director

cc: David Owenby (David.Owenby@tn.gov)
Mary Parkman (Mary.Parkman@tn.gov)
Joe Sanders (Joseph.Sanders@tn.gov)
Marc Corrigan (Marc.Corrigan@tn.gov)



U.S. Department
of Transportation
**Federal Aviation
Administration**

Memphis Airports District Office
2862 Business Park Dr, Bldg G
Memphis, TN 38118-1555

Phone: 901-322-8180

April 26, 2013

Mrs. Ann Andrews
Environmental Division
Tennessee Department of Transportation
James K. Polk Building, Suite 900
505 Deaderick Street
Nashville, TN 37243

Re: Proposed SR 115 Improvements

Dear Mrs. Andrews:

We have reviewed the Environmental Assessment (EA) you provided related to the proposed SR 115 improvements in Knox and Blount Counties. Based on the EA and documents in our office it has been determined that the Sky Ranch Airport is the closest airport to your proposed road project.

Please keep this office notified as the project moves forward.

Thank you for the opportunity to comment on this project.

Sincerely,

Stephen Wilson
Community Planner



STATE OF TENNESSEE

DEPARTMENT OF ENVIRONMENT AND CONSERVATION

Division of Natural Areas
Natural Heritage Program
7th Floor L&C Tower
401 Church Street
Nashville, Tennessee 37243
Phone 615/532-0431 Fax 615/532-0046

May 9, 2013

Ann Andrews
Transportation Manager II
Tennessee Department of Transportation
Suite 1000, James K. Polk Building
Nashville, TN 37243-0349

Subject: State Route 115 (U.S. 129, Alcoa Highway) From I-140 (Pellissippi Parkway) to
North of the Cherokee Trail Interchange (TDOT_PIN100241)
Blount and Knox Counties, Tennessee
Rare Species Database Review

Dear Sir or Madam:

Thank you for the opportunity to perform a rare species database review for the widening of a section of State Route 115 (U.S. 129, Alcoa Highway) from I-140 (Pellissippi Parkway) to north of the Cherokee Trail Interchange project, located in Blount and Knox Counties, Tennessee.

We have reviewed the state's natural heritage database with regard to the project boundaries, and we find that the following rare species have been observed previously within one mile of the project:

Type	Scientific Name	Common Name	Global Rank	St. Rank	Fed. Prot.	St. Prot.	Habitat
Invertebrate Animal	Athearnia anthonyi	Anthony Riversnail	G1	S1	LE,XN	E	Larger rivers and downstream stretches of lg creeks, on cobble/boulder substrates adj. riffles; portions of upper TN River basin.
Vascular Plant	Aureolaria patula	Spreading False-foxglove	G3	S3	--	S	Oak Woods And Edges
Vascular Plant	Boechera patens	Spreading Rockcress	G3	S1	--	E	Moist Rocky Woods
Vascular Plant	Cardamine flagellifera	Running Bittercress	G3	S2	--	T	Mountain Stream Banks

Type	Scientific Name	Common Name	Global Rank	St. Rank	Fed. Prot.	St. Prot.	Habitat
Vascular Plant	<i>Cimicifuga rubifolia</i>	Appalachian Bugbane	G3	S3	--	T	Rich Woods
Invertebrate Animal	<i>Dromus dromas</i>	Dromedary Pearlymussel	G1	S1	LE	E	Medium-large rivers with riffles and shoals w/ relatively firm rubble, gravel, and stable substrates; Tennessee & Cumberland systems.
Vertebrate Animal	<i>Falco peregrinus</i>	Peregrine Falcon	G4	S1B	No Status	E	Varied habitats including farmlands, marshes, river mouths, and cities; often nests on ledges.
Other (Ecological)	Heron rookery	Heron Rookery	GNR	SNR	--	Rare, Not State Listed	
Vascular Plant	<i>Hydrastis canadensis</i>	Goldenseal	G4	S3	--	S-CE	Rich Woods
Invertebrate Animal	<i>Isofluvialis</i>	Spiny Riversnail	G2	S2	--	Rare, Not State Listed	Shallow waters of shoals that are rapid to moderate and well-oxygenated; Tennessee River & main tributaries; E Tennessee.
Vascular Plant	<i>Panax quinquefolius</i>	American Ginseng	G3G4	S3S4	--	S-CE	Rich Woods
Invertebrate Animal	<i>Plethobasus cooperianus</i>	Orangefoot Pimpleback	G1	S1	LE	E	Large rivers in sand-gravel-cobble substrates in riffles and shoals in deep flowing water; Cumberland & Tennessee river systems.
Vertebrate Animal	<i>Tyto alba</i>	Barn Owl	G5	S3	--	D	Open and partly open country, often around human habitation; farms.

Within four miles of the project the following additional rare species have been reported:

Type	Scientific Name	Common Name	Global Rank	St. Rank	Fed. Prot.	St. Prot.	Habitat
Vertebrate Animal	<i>Cryptobranchus alleganiensis</i>	Hellbender	G3G4	S3	No Status	D	Rocky, clear creeks and rivers with large shelter rocks.
Vertebrate Animal	<i>Etheostoma marmorpinnum</i>	Marbled Darter	G1	S1	LE	E	Pools and moderate runs with clean pebbles, cobble, & small boulders; lower Little River (Tennessee River drainage).
Nonvascular Plant	<i>Funaria americana</i>	A Moss	G3?	S1?	--	T	Limestone Bluffs And Barrens
Invertebrate Animal	<i>Fusconaia cuneolus</i>	Finerayed Pigtoe	G1	S1	LE	E	Riffles of fords and shoals of mod gradient streams in firm cobble and gravel substrates; middle & upper Tennessee River watershed.

Type	Scientific Name	Common Name	Global Rank	St. Rank	Fed. Prot.	St. Prot.	Habitat
Vertebrate Animal	<i>Gyrinophilus gulolineatus</i>	Berry Cave Salamander	G1Q	S1	--	T	Aquatic cave obligate; Ridge & Valley; formerly included with <i>G. palleucus</i> .
Vertebrate Animal	<i>Haliaeetus leucocephalus</i>	Bald Eagle	G5	S3	--	D	Areas close to large bodies of water; roosts in sheltered sites in winter; communal roost sites common.
Vertebrate Animal	<i>Hemitremia flammea</i>	Flame Chub	G3	S3	--	D	Springs and spring-fed streams with lush aquatic vegetation; Tennessee & middle Cumberland river watersheds.
Vertebrate Animal	<i>Ixobrychus exilis</i>	Least Bittern	G5	S2B	--	D	Marshes with scattered bushes or other woody growth; readily uses artificial wetland habitats.
Invertebrate Animal	<i>Lasmigona holstonia</i>	Tennessee Heelsplitter	G3	S2	--	Rare, Not State Listed	Spring runs, creeks, & small rivers, in subst of sand & mud; upper Tenn & Conasauga river watersheds; Blue Ridge & Ridge & Valley.
Vertebrate Animal	<i>Myotis grisescens</i>	Gray Myotis	G3	S2	LE	E	Cave obligate year-round; frequents forested areas; migratory.
Vertebrate Animal	<i>Noturus flavipinnis</i>	Yellowfin Madtom	G1	S1	LT,XN	E	Medium size to large creeks and small rivers that are unpolluted & relatively unsilted; upper Tennessee River watershed.
Vertebrate Animal	<i>Percina aurantiaca</i>	Tangerine Darter	G4	S3	--	D	Large-moderate size headwater tribs to Tennessee River, in clear, fairly deep, rocky pools, usually below riffles.
Vertebrate Animal	<i>Percina macrocephala</i>	Longhead Darter	G3	S2	--	T	Clear, larger upland creeks and small-med rivers, usually in rocky flowing pools upst/dnst rubble riffles; Tenn & Cumb river watersheds.
Vertebrate Animal	<i>Percina tanasi</i>	Snail Darter	G2G3	S2S3	LT	T	Sand and gravel shoals of moderately flowing, vegetated, large creeks; upper Tennessee River watershed.
Vertebrate Animal	<i>Rallus elegans</i>	King Rail	G4	S2	--	D	Marshes, upland-wetland marsh edges, flooded farmlands, shrub swamps.
Nonvascular Plant	<i>Rhachithecium perpusillum</i>	Budding Tortula	G4G5	SH	--	S	Bark of Hardwoods
Vertebrate Animal	<i>Sorex longirostris</i>	Southeastern Shrew	G5	S4	--	D	Various habitats including wet meadows, damp woods, and uplands; statewide.

May 9, 2013

Page 4

We note from the EA that the vast majority of the species above have been evaluated and determined to be unaffected or minimally affected by the action alternative. Since the original TDOT evaluation of the project area, the burrowing crayfish *Cambarus deweesae* (Valley Flame Crayfish) has been documented in southern Roane County on the south side of the Tennessee River. This discovery expands the prospect that this state endangered species may occur in Knox County in suitable wetlands or hydric soils. Our office would appreciate additional detail regarding the wetland described on p. 3-49 of the EA, specifically any documentation of the presence of burrowing crayfish and site photos. Staff from this office or that of the Tennessee Wildlife Resources Agency (TWRA) may investigate the site at a later date to determine what species are present. Additionally, we would appreciate copies of any bat survey reports produced during the summer July 2011 survey indicated on p. 3-54.

Should suitable habitat exist on or immediately downstream of the project area, we ask that plans provide for the protection of the species noted above. We ask that you coordinate this project with the TWRA (Rob Todd, rob.todd@tn.gov, 615-781-6577) to ensure that legal requirements for protection of state listed rare animals are addressed. Additionally, we ask that you contact the U.S. Fish and Wildlife Service Field Office, Cookeville, Tennessee (931-525-4970) for comments regarding federally listed species.

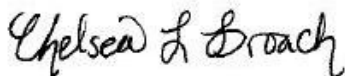
For stabilization of disturbed areas, the Tennessee Natural Heritage Program advocates the use of native trees, shrubs, and warm season grasses, where practicable. Care should be taken to prevent re-vegetation of disturbed areas with plants listed by the Tennessee Exotic Pest Plant Council as harmful exotic plants: <http://www.tneppc.org/>

Please keep in mind that not all of Tennessee has been surveyed and that a lack of records for any particular area should not be construed to mean that rare species necessarily are absent. For information regarding species protection status and ranks, please visit <http://www.tn.gov/environment/na/pdf/Status&Ranks.pdf>.

To assist in determining whether rare species are located at a given site, the Tennessee Natural Heritage Program has implemented a publicly accessible website where rare species data lists by county, quadrangle, watershed, and MS4 boundaries can be obtained: http://environment-online.state.tn.us:8080/pls/enf_reports/f?p=9014:3:3875605994273657.

Thank you for considering Tennessee's rare species throughout the planning of this project. Should you have any questions, please do not hesitate to contact David at (615) 532-0441 or david.withers@tn.gov.

Sincerely,



Chelsea L. Broach
Interim Data Manager



David Ian Withers
Natural Heritage Zoologist



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July 11, 2013

Public Information Meeting Comments
TN Department of Transportation
Suite 700, James K. Polk Building
505 Deaderick St.
Nashville, TN 37243-0332
and
Mike Russell
TDOT Region 1, Knoxville
7345 Region Lane
Knoxville, TN 37914

Dear Mike,

On behalf of the TPO staff, I appreciate the opportunity to respond to TDOT's request for comments on its Environmental Assessment of the SR-115 (Alcoa Highway) widening project. Many of our comments relate to the design details discussed and illustrated in the EA. While we recognize that these design elements are preliminary, we are taking this opportunity to comment on them to make sure that they are addressed when TDOT enters the design phase for this project. This letter will briefly describe several areas of concern. It's followed by a set of more detailed comments regarding specific sections of the EA.

The first area is access control. We applaud TDOT efforts through this project to address the need for greater access control to improve safety along SR-115 through the prohibition of left turns. As the design progresses, we would prefer to see an even greater limitation of direct access to Alcoa Highway. It would be preferred to limit access to the highway only to the proposed interchanges to the extent possible through the development of a more complete frontage road system that would tie existing public roads and private driveways together. This would have several benefits including reduction of conflict points, improved safety, improved traffic operations and the ability to better manage future land use development in the corridor and its need for safe, efficient access.

The second area is bicycle and pedestrian accommodation. It has long been the understanding of the TPO, the City of Knoxville and Knox County that TDOT would build two segments of greenway along SR-115 as part of this project. These segments would link up existing and planned greenway segments to create a greenway from downtown Knoxville to the Knox/Blount county line. A map illustrating this plan is included on page 3-64 of the EA. It is our understanding that TDOT had agreed to construct the segments labeled on that map as TDOT Piece 1 and TDOT Piece 2. In addition to that understanding, TDOT is obligated to provide accommodations to bicyclists and pedestrians based on its 2010 Bicycle and Pedestrian Policy. The policy states: "Bicycle and Pedestrian access along corridors served by new or reconstructed roadways shall not be made more difficult or impossible by roadway improvements. ... Addressing the need for bicyclists and pedestrians to cross corridors as well as travel along them, the design of intersections and interchanges shall accommodate bicyclists and pedestrians in a manner that is safe, accessible and convenient." Based on this policy, bicycle and pedestrian accommodation along all new collector and access roads and through all interchanges in order to provide full access for those modes is also needed.

The TPO coordinates a comprehensive, multi-modal transportation planning process for the Knoxville regional area.

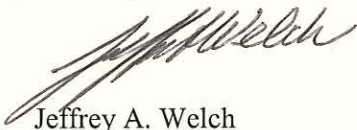
The third area is transit. We anticipate that traffic congestion and safety will be a serious concern during construction of this project. With that in mind, we request TDOT's support for provision of public transit through the SR-115 corridor during construction. This service may involve the use of traditional buses or vanpools. The TPO's Smart Trips program would be able to provide coordination of and support to this effort to keep traffic flowing safely while construction occurs. We would also like to see this project accommodate future transit and rideshare use along the corridor by providing park-and-ride lots at key points, potentially on property that TDOT would already have to acquire for the road project. We request that you coordinate with the TPO when it comes time to choose those locations.

The final two areas relate to specific elements of the preliminary design:

1. At the southern end of the project, there is an overlap between this project and the proposed Alcoa Bypass project. The EA should more completely address how this overlap area will be handled by discussing the timing of the potential build scenarios and associated impact on the roadway design for each project. For example, if the Alcoa Bypass is constructed first then it would appear that additional lanes would not be necessary on the segment of existing Alcoa Highway between Pellissippi Parkway and Singleton Station Road due to a projected decrease in traffic volume.
2. Between Marine Park and Woodson Drive, the right of way for SR-115 is severely constrained, with a steep rock bluff on one side and the Tennessee River on the other. The EA should include additional discussion of the environmental impacts of widening SR-115 through this sensitive area.

I appreciate your attention to these comments and to the more detailed comments in the attachment, and trust that you'll contact me with any questions you may have.

Sincerely,



Jeffrey A. Welch
Executive Director

cc: Jim Ozment, TDOT
Daniel Oliver, TDOT
Jessica Wilson, TDOT

EA page number	EA section	Comments	Questions
S-i	Summary	Under "Alternatives" heading, it should be mentioned that the Build Alternative includes a trail for bicycle/pedestrian accommodation along parts of the highway routes.	
S-iii	Table S-1	Under the Impact Category of Recreational Resources, note that the Build Alternative includes construction of a trail for bicycle/pedestrian accommodation along 2 segments of SR-115: from the north end of the SR-115 bridge over the little river up to I.C. King Park, and another from the northern terminus of the Knox County portion of the Knox/Blount Greenway (see Figure 3-8 on Page 3-64) to Marine Park.	
2-7	2.3.1	"No bicycle or pedestrian facilities are included in the project design" should be removed. TDOT's 2010 Bicycle and Pedestrian Policy says that "Provisions for bicycles and pedestrians shall be integrated into new construction and reconstruction of roadway projects through design features appropriate for the context and function of the transportation facility."	
2-9	2.3.2	The new section of Singleton Station Road, including the bridge over SR-115, needs to be a Complete Street (that is, safely accommodate bicycle and pedestrian traffic).	
2-10	2.3.2	The new frontage road between Lakemont Dr and Hillside Dr, and the new sections of those roads east of the frontage road, need to be Complete Streets. The intersections of the frontage road with Lakemont and Hillside need to be designed to safely accommodate both pedestrians and bicyclists.	
2-11	2.3.2	The new section of Topside Road (Blount County) needs to be a Complete Street.	

EA page number	EA section	Comments	Questions
2-12	2.3.2	The new sections of Topside Road (Knox County) and the overpass need to be Complete Streets. If the intersection of the two-way ramp and the greenway is at grade, it needs to be made safe for bicyclists and pedestrians. (For guidance on safe pedestrian crossings at uncontrolled locations, see FHWA-HRT-04-100.) Assuming the two-way ramp intersects with the greenway, it needs to be a Complete Street so that bicyclists and pedestrians can use it to access the greenway.	
2-13	2.3.2	The intersection of the access road and greenway needs to be made safe for bicyclists and pedestrians.	
2-14	2.3.2	New sections of Maloney Road and the overpass need to be Complete Streets. The two intersections of the new ramps with Maloney (one on either side of SR-115) need to be designed to safely accommodate bicycle and pedestrian traffic. They should not allow high-speed turning movements.	Will the auxiliary lane on the east side of SR-115, in the context of multiple access points, possibly lead to dangerous weaving behavior by motorists?
2-17	2.3.2, Figure 2-9B	Collector 5 connects neighborhoods and businesses, so it needs to be a Complete Street. The Mt. Vernon Dr overpass and new street sections need to be Complete Streets. There needs to be safe access from the greenway across Collector 6 to the overpass so that the greenway and neighborhoods are connected.	
2-18	2.3.2, Figure 2-9C	Collector 3, between Barter Hill Ln and Woodson Dr, needs to be a Complete Street. New sections of Barber Hill and underpass need to be Complete Streets. There needs to be safe access from the greenway to the underpass and across Montlake Dr.	
2-19	2.3.2	New sections of Medical Center Rd, Cherokee Trail and the two new service roads need to be Complete Streets.	

EA page number	EA section	Comments	Questions
2-20	2.3.3	12-foot lanes are excessive for the approaches to and from SR-115. 11 feet provides the same safety and capacity in urban settings.	
2-20	2.3.3, Bullet 2	New section of Vista Dr needs to be a Complete Street.	
2-20	2.3.3, Bullet 3	New section of Singleton Station Rd needs to be a Complete Street.	What data suggests that Singleton Station Rd needs to be a 5-lane cross-section?
2-21	2.3.3, Bullet 11		This refers to "Connector 2." Should that be "Collector" instead? And where is it?
2-21	2.3.3, Bullet 13	The new "business entrance" needs to be a Complete Street.	
2-21	2.3.3, Bullet 15	Two new sections of greenway are to be built by TDOT as part of the SR-115 widening: one from the north end of the SR-115 bridge over the little river up to I.C. King Park, and another from the northern terminus of the Knox County portion of the Knox/Blount Greenway (see Figure 3-8 on Page 3-64) to Marine Park.	
3-1	3.1		Is the characterization of the project area as "rapidly urbanizing" accurate? What data supports this?
3-5	3.1		What's the basis for the statement: "The Build Alternative is not anticipated to affect future land use"?
3-5	3.1		Were the relevant MPC Sector Plans consulted when it was determined that this project is "consistent with the land use plans and policies adopted by Knoxville-Knox County"?
3-7	3.3.1		Where is the data to back up the statement about "increased population density" in Blount County? Increasing population does not necessarily mean increasing density.

EA page number	EA section	Comments	Questions
3-7	3.3.1: Table 3 1	This table (or another table) should show population changes over time for the project area.	
3-8	3.3.1	The statement about minority population data that "only one Block Group in Blount County exceeded the (county) average" does not agree with the data in Table 3-2, which show that all four Blount County Block Groups in the project area exceed the county for percentage of minority population.	
3-12	3.3.2	Paragraph 3 restates the error from Page 3-8 that only one Blount County Block Group in the project area exceeds the countywide percentage of minority population.	
3-12	3.3.2	The statement that "two block groups in Blount County ... exceeded the county poverty level percentages" does not agree with the data in Table 3-2, which show that three block groups in Blount County exceed the countywide percentage of population in poverty.	
3-12	3.3.2	Paragraph 5 states that Census Tract 103, Block Group 3 has higher poverty levels and minority population than state averages. According to the data in Table 3-2, this is not accurate.	
3-13	3.3.2	The statement that "the project would benefit all populations by providing safer access to/from, and better mobility on, SR-115" should specify that it's referring to "driving populations."	
3-40	3.9	Paragraph 2 states that the project will "accommodate a segment of the Knox/Blount Greenway." The project needs to include two segments of this greenway, as described in previous comments.	

EA page number	EA section	Comments	Questions
3-59	3.15		<p>Paragraph 5 states that "Under the Build Alternative, improved traffic flow and reduced travel time in the design year will result in a decrease in energy use compared to existing conditions." But Section 1-3 stated that traffic volume is predicted to increase compared with the No-Build Alternative in all four scenarios. How does the increased energy use from increased traffic volume and VMT offset the energy savings that are projected to result from improved traffic flow and reduced travel time?</p>
3-62	3.17	<p>Paragraph 1 notes that SR-115 as it stands today is "acceptable for bicycle travel." Paragraph 2 states that the proposed "grade separations on SR-115 do not present a favorable environment for bicycles or pedestrians." As this project is essentially taking away an adequate bicycle facility, TDOT needs to replace that facility with a form of bicycle and pedestrian accommodation that's safest in the context of a grade-separated, limited-access facility, which is a greenway. In the past, TDOT has committed to building two greenway segments, described above, to complete the connection from the Knoxville greenway system to the Knox/Blount County line. Those segments need to be included in this project.</p>	
3-62, 63	3.17	<p>These pages contain several descriptions of greenway segments to be built by Knox County, City of Knoxville and TDOT that are inaccurate. Figure 3-8 on Page 3-64 correctly depicts the greenway segments to be built in the vicinity of SR-115. Phase 1 and 2 and funded separately and are to be built by City of Knoxville and Knox County, respectively. TDOT Piece 1 and TDOT Piece 2 are to be built by TDOT as part of this SR-115 project.</p>	



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July 11, 2013

Public Information Meeting Comments

TN Department of Transportation
Suite 700, James K. Polk Building
505 Deaderick St.
Nashville, TN 37243-0332
and
Mike Russell
TDOT Region 1, Knoxville
7345 Region Lane
Knoxville, TN 37914

Dear Mr. Russell,

On behalf of the MPC staff, I appreciate the opportunity to respond to TDOT's request for comments on its Environmental Assessment (EA) of the SR-115 (Alcoa Highway) widening project. Many of our comments relate to land use and community/neighborhood impacts, as well as design details discussed and illustrated in the EA. Many of our concerns regarding the Knox-Blount Greenway, neighborhood connectivity and complete streets are in line with the comments from the Knoxville TPO, and are also important components of building strong and healthy communities and neighborhoods in Knoxville and Knox County.

The City of Knoxville and Knox County have land use plans embedded within 12 planning sectors. These sectors are referenced in the 2033 General Plan, however, to address potential impacts to land use the appropriate sector plan for that area should be referenced because sector plans serve as the long range land use plan and each sector plan is updated on a 5 to 7-year basis. The land use plan for this area is the South County Sector Plan. It is available on the MPC website at <http://archive.knoxmpc.org/plans/sector/southcounty2012.pdf> for TDOT's review and in forming a basis for impacts to land use.

We would like to request that impacts to the South County Sector Plan, the Knoxville-Knox County land use plan for the corridor, be considered and evaluated as TDOT enters into the design phase for this project. We welcome collaboration and cooperation between TDOT, TPO, MPC, City of Knoxville and Knox County as design plans continue to develop from the conceptual plan presented by the EA.

This letter will briefly describe several areas of concern. It's followed by a set of more detailed comments regarding specific sections of the EA.

The first area is lack of coordination and impacts to land use. While updating the South County Sector Plan, MPC staff invited TDOT representatives to attend a public meeting and comment on the proposed land

use plan alternatives for the Alcoa Highway Small Area Plan, however, the invitation was declined by a TDOT representative. The potential impact to land use in this area by the proposed Build Alternative design is significant to the extent that two land use plan alternatives were developed and adopted by the City of Knoxville and Knox County in 2012 and 2013 that would depend on varying TDOT design options.

Previous TDOT preliminary designs (circa 2000) demonstrated the possibility of frontage roads for access along the commercial section of both the eastern and western side of the corridor. However, the most recent Build Alternative, as proposed in this EA, only demonstrates a two-way collector road on the eastern side of the highway for access to commercial and office uses and a connection to the Martha Washington Heights neighborhood via Collector 5. A similar collector road should be provided on the western side of the highway for access to properties zoned for commercial, office and residential uses to maintain redevelopment viability in this area.

Community input gathered by MPC during the Alcoa Highway Small Area plan noted that the area has been subjected to increased rates of vacancy in the strip commercial centers and population decline in adjacent neighborhoods. The area has recently become designated a Food Desert by the USDA due to limited access to a grocery store and low-income status, having lost a major grocery store retailer in the last five years. Providing options for increased residential growth in the area to support commercial and office redevelopment is needed. A mixed use district was proposed, however two alternatives were developed due to uncertainty on the design of frontage roads.

Alternative 1 for the Alcoa Highway Small Area Plan relies on the construction of frontage roads for both sides of the highway. Without these frontage roads medium to high density residential development would not be recommended by MPC and commercial and office redevelopment may continue to languish along this corridor in Knoxville-Knox County without increased population growth potential.

The Build Alternative, as envisioned currently, does conflict with the Alcoa Highway Small Area Plan for the 2012 South Knox County Sector Plan. It would limit the type of land use that could be supported by MPC in the land use plan, including an expansion of commercial or medium density residential, particularly for adjacent lands where frontage or collector roads are not provided for existing commercial and office zoned properties. It would not enhance connectivity between adjacent properties. The following principles are recommended as part of the Mixed Use Special District (MU-SCo-3) within the Alcoa Highway Small Area Plan.

- Consolidate access points onto highway or frontage roads and/or create interparcel access between properties.

- Create more intense buffers for the adjacent established neighborhoods.

- Diversify development to include commercial, office and, if frontage roads are built, medium to high density residential.

- Reduce parking requirements to increase outparcel development potential for smaller office and commercial buildings.

- Landscape parking and frontage areas to increase aesthetic appeal and reduce stormwater runoff.

- Provide safe pedestrian and cyclist circulation throughout the area with walking and biking connections for the established neighborhoods and the Knox-Blount greenway.

TDOT's current "Build alternative to be carried forward in the NEPA process" does not mention safe pedestrian and cyclist connections between the established neighborhoods, commercial areas and the Knox-Blount Greenway. This project could also impact the aesthetic appeal of adjacent properties without adequate screening.

The second area is bicycle and pedestrian accommodation. Our comments in regard to bicycle and pedestrian accommodation are inline with the Knoxville TPO comments, particularly with the TDOT

obligation to construct two segments of Knox-Blount Greenway. Where collector roads and other local street improvements are proposed, they should be designed as complete streets, to provide safe pedestrian and bicycle connectivity between the diverse land uses in the corridor. This would reduce trip generation in adjacent neighborhoods and increasing redevelopment viability and connectivity to the Knox-Blount Greenway facility.

The third area is access control. Our comments are in line with the Knoxville TPO in regard to access control along the corridor and we would like to reiterate the need for a more complete frontage road system, tying existing local roads and private driveways together, that would encourage the redevelopment potential of the area.

The fourth area is transit. Our comments are inline with the Knoxville TPO as well, particularly with regard to the need for this project to accommodate future transit and rideshare use along the corridor. Providing park-and-ride lots at key points, potentially on property that TDOT would already have to acquire for the road project would help with implementation of the Alcoa Highway express bus service, as recommended by the Knoxville Regional Transit Corridor Study (March 2013). We request that you coordinate with the MPC and TPO when it comes time to choose those locations.

The final area relates to specific elements of the preliminary design:

Between Marine Park and Woodson Drive, the right of way for SR-115 is severely constrained, with a steep rock bluff on one side and the Tennessee River on the other. The EA should include additional discussion of the environmental impacts of widening SR-115 through this sensitive area.

I appreciate your attention to these comments and to the more detailed comments in the attachment, and trust that you'll contact me with any questions you may have.

Sincerely,

Mike Carberry, AICP
Comprehensive Planning Manager on behalf of Mark Donaldson, Executive Director

cc: Jim Ozment, TDOT, Daniel Oliver, TDOT, Jessica Wilson, TDOT

ATTACHMENT:

EA Page Number	EA Section	MPC Comments	MPC Questions
S-i	Summary	Under "Alternatives" heading, it should be mentioned that the Build Alternative includes a trail for bicycle/pedestrian accommodation along parts of the highway routes.	
S-ii	Table S-1	Under the Impact Category of Economic, the Build Alternative is noted that the "improved regional transportation network will enhance area for new and existing businesses." This project may negatively impact existing businesses if the Build Alternative design does not include collector roads for properties zoned for commercial and office uses on both the eastern and western sides of Alcoa Highway between Maloney Road and Mt. Vernon Drive. Because of this concern, two separate land use plans were adopted depending on the Build Alternative's final design. If collector roads are not provided, medium to high density residential uses would not be recommended as part of the proposed mixed use districts in the 2012 South County Sector Plan (p.31-33).	Where is the analysis that demonstrates economic impact to existing and new businesses if collector roads are provided versus not provided as part of the Build Alternative design? Where is the analysis that demonstrates that local land use plans were used in developing economic impact measures in regard to new business potential and population densities needed to support businesses in the corridor?
S-iii	Table S-1	Under the Impact Category of Recreational Resources, note that the Build Alternative includes construction of a trail for bicycle/pedestrian accommodation along 2 segments of SR-115: from the north end of the SR-115 bridge over the Little River up to I.C. King Park, and another from the northern terminus of the Knox County portion of the Knox/Blount Greenway (see Figure 3-8 on Page 3-64) to Marine Park. This should be noted, as this connection has been recommended as part of the Knox/Blount Greenway by the Knoxville-Knox County Park, Recreation and Greenways Plan (p.57-59, adopted 2010), a proposal for expansion of I.C. King Park has also been recommended. This park expansion could accommodate play fields, a playground and trails.	
S-iii	Table S-2	<p>Pedestrian and bicycle access across SR-115 should be integrated into the design and implemented as part of the proposed interchange facilities and over/underpasses to maintain pedestrian and bicycle access between the Lakemoor Hills and Martha Washington Heights neighborhoods and other residential areas and commercial properties on either side of the proposed Build Alternative. Pedestrian and bicycle access across SR-115 should be provided as part of the interchange facilities, over/underpasses to maintain pedestrian and bicycle access between neighborhoods and commercial properties on either side of the proposed improvement.</p> <p>Pedestrian facilities should be provided on frontage road/access road facilities, pedestrian facilities should be provided when vehicle capacity of local roads are increased and/or safe connections to greenway facility should be established. Creating walkable neighborhoods and connections</p>	

EA Page Number	EA Section	MPC Comments	MPC Questions
		<p>between neighborhoods and mixed use centers are critical to maintaining and enhancing development potential that could be severely impacted by the proposed SR-115 freeway widening.</p> <p>Neighborhood groups expressed concerns regarding development potential and vacancy due to traffic safety and access issues along Alcoa Highway to the Knoxville-Knox County Metropolitan Planning Commission (MPC) during the South County Sector Plan update 2012-2013. The Lakemoor Hills Neighborhood Association requested a small area land use plan be conducted as part of the plan update. Transportation and land development principles were adopted as part of the Alcoa Highway Small Area Mixed Use Special District (MU-SCo-3) for areas adjacent to Alcoa Highway within the Segment 3: Maloney Road to Woodson Drive and are as follows (see also pages 31-33 of the 2012 South County Sector Plan):</p> <ul style="list-style-type: none"> -Consolidate access points onto highway or frontage roads and/or create interparcel access between properties. -Create more intense buffers for the adjacent established neighborhoods. -Diversify development to include commercial, office and, if frontage roads are built, medium to high density residential. -Reduce parking requirements to increase outparcel development potential for smaller office and commercial buildings. -Landscape parking and frontage areas to increase aesthetic appeal and reduce stormwater runoff. -Provide safe pedestrian and cyclist circulation throughout the area with walking and biking connections for the established neighborhoods and the Knox-Blount greenway. <p>At the time of drafting the sector plan update, MPC and the Knoxville Regional Transportation Planning Organization (TPO) were aware of two possible TDOT design alternatives for the Alcoa Highway improvements. One design alternative included frontage roads for the largely commercially zoned parcels adjacent to Alcoa Highway between Maloney Road and Montlake Road, a second design alternative included a lack of frontage roads for these commercially zoned parcels. Thus, two potential land use plans were adopted since a lack of frontage roads could severely impact the development potential of the corridor.</p> <p>If frontage roads, accessing Alcoa Highway are not provided as part of the improvement to service the existing commercial corridor on both the eastern and western sides of the highway between Maloney Road and Montlake Road, then mixed use redevelopment, including commercial uses,</p>	

EA Page Number	EA Section	MPC Comments	MPC Questions
		office and medium density residential is less likely to be successful.	
1-1, 1-2	Figure 1-1 and Table 1-1	Figure 1-1: General Location Map has the TIP Project Segments mislabeled. Section 03 and 04 should be reversed to reflect the labeling demonstrated on Table 1-1: Project segments.	
1-17	1.5	<p>This section should acknowledge the recently adopted 2012 South Knox County Sector Plan, which establishes the land use plan for this section of the City of Knoxville and Knox County. It currently does not mention the existence of this plan. The land use plan and development and redevelopment potential of adjacent properties will be affected by the proposed highway improvements. TDOT representatives were requested to attend a public meeting when the Alcoa Highway Small Area Plan was reviewed and comments were received by MPC staff. MPC staff spoke to Mike Russell on 9/8/12 to invite him and/or any other representative of TDOT to the Alcoa Highway Small Area Plan meeting on 9/27/12, however, TDOT staff declined our invitation.</p> <p>This project, as envisioned currently, does conflict with the Alcoa Highway Small Area Plan for the 2012 South Knox County Sector Plan local land use plans in that pedestrian and bicycle access between adjacent established neighborhoods in that it would not enhance connectivity between adjacent properties. It would also limit the type of land use that could be supported by MPC in the land use plan, limiting an expansion of commercial or medium density residential, particularly for adjacent lands where frontage or collector roads are not provided for existing commercial and office zoned properties.</p> <p>TDOT's current "Build alternative to be carried forward in the NEPA process" does not mention safe pedestrian and cyclist connections between the established neighborhoods, commercial areas and the Knox-Blount Greenway. This project could also impact the aesthetic appeal of adjacent properties. The Build Alternative as outlined by TDOT in this plan does conflict with the South County Sector Plan, as it is currently proposed. Pedestrian facilities should be provided on frontage road/access road facilities, pedestrian facilities should be provided when vehicle capacity of local roads are increased and/or safe connections to greenway facility should be established. Creating walkable neighborhoods and connections between neighborhoods and mixed use centers are critical to maintaining</p>	

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		<p>and enhancing development potential that could be severely impacted by the proposed SR-115 freeway widening.</p> <p>Neighborhood groups expressed concerns regarding development potential and vacancy due to traffic safety and access issues along Alcoa Highway to the Knoxville-Knox County Metropolitan Planning Commission (MPC) during the South County Sector Plan update 2012-2013. The Lakemoor Hills Neighborhood Association requested a small area land use plan be conducted as part of the plan update. Transportation and land development principles were adopted as part of the Alcoa Highway Small Area Mixed Use Special District (MU-SCo-3) for areas adjacent to Alcoa Highway within the Segment 3: Maloney Road to Woodson Drive and are as follows (see also pages 31-33 of the 2012 South County Sector Plan):</p> <ul style="list-style-type: none"> -Consolidate access points onto highway or frontage roads and/or create interparcel access between properties. -Create more intense buffers for the adjacent established neighborhoods. -Diversify development to include commercial, office and, if frontage roads are built, medium to high density residential. -Reduce parking requirements to increase outparcel development potential for smaller office and commercial buildings. -Landscape parking and frontage areas to increase aesthetic appeal and reduce stormwater runoff. -Provide safe pedestrian and cyclist circulation throughout the area with walking and biking connections for the established neighborhoods and the Knox-Blount greenway. <p>At the time of drafting the sector plan update, MPC and the Knoxville Regional Transportation Planning Organization (TPO) were aware of two possible TDOT design alternatives for the Alcoa Highway improvements. One design alternative included frontage roads for the largely commercially zoned parcels adjacent to Alcoa Highway between Maloney Road and Montlake Road, a second design alternative included a lack of frontage roads for these commercially zoned parcels. Thus, two potential land use plans were adopted since a lack of frontage roads could severely impact the development potential of the corridor.</p> <p>If frontage roads, accessing Alcoa Highway are not provided as part of the improvement to service the existing commercial corridor on both the eastern and western sides of the highway between Maloney Road and Montlake Road, then mixed use redevelopment, including commercial uses, office and medium density residential is less likely to be successful. It also</p>	

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		conflicts with the 5-year plan of the Knoxville-Knox County Parks, Recreation and Greenways Plan, as it does not demonstrate TDOT's commitment to construct their segment of the Knox-Blount Greenway in conjunction with the City of Knoxville and Knox County's commitment to construct their segments.	
2-7	2.3.1	No bicycle or pedestrian facilities are included in the project design should be removed. TDOT's 2010 Bicycle and Pedestrian Policy says that "Provisions for bicycles and pedestrians shall be integrated into new construction and reconstruction of roadway projects through design features appropriate for the context and function of the transportation facility."	
2-12	2.3.2	The new sections of Topside Road (Knox County) and the overpass need to be Complete Streets. If the intersection of the two-way ramp and the greenway is at grade, it needs to be made safe for bicyclists and pedestrians. (For guidance on safe pedestrian crossings at uncontrolled locations, see FHWA-HRT-04-100.) Assuming the two-way ramp intersects with the greenway, it needs to be a Complete Street so that bicyclists and pedestrians can use it to access the greenway.	
2-13	2.3.2	The intersection of the access road and greenway needs to be made safe for bicyclists and pedestrians.	
2-14	2.3.2	New sections of Maloney Road and the overpass need to be Complete Streets. The two intersections of the new ramps with Maloney (one on either side of SR-115) need to be designed to safely accommodate bicycle and pedestrian traffic. They should not allow high-speed turning movements.	Will the auxiliary lane on the east side of SR-115, in the context of multiple access points, possibly lead to dangerous weaving behavior by motorists?
2-16	2.3.2, Figure 2-9A	A two-way complete street collector road should be provided for adjacent properties on the western side of the Build Alternative, as is provided on eastern side with the demonstration of Collector 5. The 2012 South County Sector Plan (pg. 32-33) demonstrates the mixed use potential for the area. As adjacent neighborhoods have seen negative population growth in the last ten years, they have also lost neighborhood retailers, including the area's only grocery store. Due to this issue, this area is now identified as a USDA food desert. For potential growth to occur and development to diversify along this corridor, residential growth in this area is needed to sustain commercial and mixed use redevelopment along the corridor. Not providing a collector road for this side of the Build Alternative would severely limit the redevelopment, particularly for mixed use, including medium-high density residential and commercial uses and conflicts with the 2012 South County Sector Plan. It would also reduce walkability for adjacent neighborhoods and commercial and businesses along the western side of the Build Alternative in this area.	Was the 2012 South County Sector Plan reviewed when forming this Build Alternative conceptual design?

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2-17	2.3.2, Figure 2-9B	Collector 5 connects neighborhoods and businesses, so it needs to be a Complete Street. The Mt. Vernon Dr overpass and new street sections need to be Complete Streets. There needs to be safe access from the greenway across Collector 6 to the overpass so that the greenway and neighborhoods are connected. Collector 5 should also be a two-way complete street to allow for efficient movement between properties presently zoned for commercial and office uses and those planned for mixed use, including commercial, office and residential development, see 2012 South County Sector Plan (pg. 32-33, Alternative 1).	Was the 2012 South County Sector Plan reviewed when forming this Build Alternative conceptual design?
2-18	2.3.2, Figure 2-9C	Collector 3, between Barber Hill Ln and Woodson Dr, needs to be a Complete Street. New sections of Barber Hill and underpass need to be Complete Streets. There needs to be safe access from the greenway to the underpass and across Montlake Dr.	
2-19	2.3.2	New sections of Medical Center Rd, Cherokee Trail and the two new service roads need to be Complete Streets.	
2-20	2.3.3	12-foot lanes are excessive for the approaches to and from SR-115. 11 feet provides the same safety and capacity in urban settings.	
2-21	2.3.3, Bullet 11		This refers to "Connector 2." Should that be "Collector" instead? And where is it?
2-21	2.3.3, Bullet 13	The new "business entrance" needs to be a Complete Street.	
2-21	2.3.3, Bullet 15	Two new sections of greenway are to be built by TDOT as part of the SR-115 widening: one from the north end of the SR-115 bridge over the little river up to I.C. King Park, and another from the northern terminus of the Knox County portion of the Knox/Blount Greenway (see Figure 3-8 on Page 3-64) to Marine Park. This is noted as part of the 5-year program of the Knoxville-Knoxville Park, Recreation and Greenways Plan (p. 79, adopted 2010).	
3-1	3.1	The section of corridor in Knox County has actually remained relatively stable in the last 10 years, while commercial vacancy has risen, the 2012 South County Sector Plan notes on page 31, that the population has remained relatively close to 3600 persons living in Census Tract 35 surrounding the Alcoa Highway Small Area Plan area in Knox County.	Is the characterization of the project area as "rapidly urbanizing" accurate? What data supports this?

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3-5	3.1	<p>The Knoxville-Knox County General Plan (2003), points to Sector Plans that provide the land use plan for areas of Knoxville and Knox County. The South County Sector Plan, the land use plan for this area was recently updated and TDOT representatives were requested to attend a public meeting and provide comments, particularly regarding the Alcoa Highway Small Area Plan, embedded within the larger land use plan. However, TDOT representatives declined the invitation to attend the meeting and to comment.</p> <p>This section should acknowledge the recently adopted 2012 South Knox County Sector Plan, which establishes the land use plan for this section of the City of Knoxville and Knox County. It currently does not mention the existence of this plan. The land use plan and development and redevelopment potential of adjacent properties will be affected by the proposed highway improvements. TDOT representatives were requested to attend a public meeting when the Alcoa Highway Small Area Plan was reviewed and comments were received by MPC staff. MPC staff spoke to Mike Russell on 9/8/12 to invite him and/or any other representative of TDOT to the Alcoa Highway Small Area Plan meeting on 9/27/12, however, TDOT staff declined our invitation.[1]</p> <p>This project, as envisioned currently, does conflict with the Alcoa Highway Small Area Plan for the 2012 South Knox County Sector Plan local land use plans in that pedestrian and bicycle access between adjacent established neighborhoods in that it would not enhance connectivity between adjacent properties. It would also limit the type of land use that could be supported by MPC in the land use plan, limiting an expansion of commercial or medium density residential, particularly for adjacent lands where frontage or collector roads are not provided for existing commercial and office zoned properties.</p> <p>TDOT's current "Build alternative to be carried forward in the NEPA process" does not mention safe pedestrian and cyclist connections between the established neighborhoods, commercial areas and the Knox-Blount Greenway. This project could also impact the aesthetic appeal of adjacent properties. The Build Alternative as outlined by TDOT in this plan does conflict with the South County Sector Plan, as it is currently proposed. Pedestrian facilities should be provided on frontage road/access road facilities, pedestrian facilities should be provided when vehicle capacity of local roads are increased and/or safe connections to greenway facility should be established. Creating walkable neighborhoods and connections between neighborhoods and mixed use centers are critical to maintaining</p>	<p>What's the basis for the statement: "The Build Alternative is not anticipated to affect future land use"?</p> <p>Were the relevant MPC Sector Plans consulted when it was determined that this project is "consistent with the land use plans and policies adopted by Knoxville-Knox County"?</p>

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		<p>and enhancing development potential that could be severely impacted by the proposed SR-115 freeway widening.</p> <p>Neighborhood groups expressed concerns regarding development potential and vacancy due to traffic safety and access issues along Alcoa Highway to the Knoxville-Knox County Metropolitan Planning Commission (MPC) during the South County Sector Plan update 2012-2013. The Lakemoorhills Neighborhood Association requested a small area land use plan be conducted as part of the plan update. Transportation and land development principles were adopted as part of the Alcoa Highway Small Area Mixed Use Special District (MU-SCo-3) for areas adjacent to Alcoa Highway within the Segment 3: Maloney Road to Woodson Drive and are as follows (see also pages 31-33 of the 2012 South County Sector Plan):</p> <ul style="list-style-type: none"> -Consolidate access points onto highway or frontage roads and/or create interparcel access between properties. -Create more intense buffers for the adjacent established neighborhoods. -Diversify development to include commercial, office and, if frontage roads are built, medium to high density residential. -Reduce parking requirements to increase outparcel development potential for smaller office and commercial buildings. -Landscape parking and frontage areas to increase aesthetic appeal and reduce stormwater runoff. -Provide safe pedestrian and cyclist circulation throughout the area with walking and biking connections for the established neighborhoods and the Knox-Blount greenway. <p>At the time of drafting the sector plan update, MPC and the Knoxville Regional Transportation Planning Organization (TPO) were aware of two possible TDOT design alternatives for the Alcoa Highway improvements. One design alternative included frontage roads for the largely commercially zoned parcels adjacent to Alcoa Highway between Maloney Road and Montlake Road, a second design alternative included a lack of frontage roads for these commercially zoned parcels. Thus, two potential land use plans were adopted since a lack of frontage roads could severely impact the development potential of the corridor.</p> <p>If frontage roads, accessing Alcoa Highway are not provided as part of the improvement to service the existing commercial corridor on both the eastern and western sides of the highway between Maloney Road and Montlake Road, then mixed use redevelopment, including commercial uses, office and medium density residential is less likely to be successful. It also</p>	

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3-7	3.3.1: Table 3-1	This table (or another table) should show population changes over time for the project area.	
3-13	3.3.2	The statement that "the project would benefit all populations by providing safer access to/from, and better mobility on, SR-115" should specify that it's referring to "driving populations."	
3-15	3.3.4	The Build Alternative, as proposed in the EA, would create a barrier to social interaction or community cohesion. Walkable, bikeable connections between neighborhoods and the commercial areas on either side of the proposed Build Alternative are not demonstrated in this proposal. The 2012 South County Sector Plan demonstrated the need for these connections in the development of the Alcoa Highway Small Area Plan.	What is the basis for saying that "[t]he Build Alternative would not create a barrier to social interaction or community cohesion?"
3-18	3.5		What is the basis for the statement that "[c]hanges in access to business (which include remove or relocation of a driveway or reconstruction in turning movement) do not independently have a negative impact on businesses?"
3-40	3.9	Paragraph 2 states that the project will "accommodate a segment of the Knox/Blount Greenway." The project needs to include two segments of this greenway, as described in previous comments.	
3-58	3.14	Landscaping should be incorporated into the Build Alternative design to mitigate visual impacts to surrounding neighborhoods and businesses.	What is the basis for the statement "visual impacts of the proposed project to the surrounding landscape will be minimal?"

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3-59	3.15		Paragraph 5 states that "Under the Build Alternative, improved traffic flow and reduced travel time in the design year will result in a decrease in energy use compared to existing conditions." But Section 1-3 stated that traffic volume is predicted to increase compared with the No-Build Alternative in all four scenarios. How does the increased energy use from increased traffic volume and VMT offset the energy savings that are projected to result from improved traffic flow and reduced travel time?
3-62	3.17	Paragraph 1 notes that SR-115 as it stands today is "acceptable for bicycle travel." Paragraph 2 states that the proposed "grade separations on SR-115 do not present a favorable environment for bicycles or pedestrians." As this project is essentially taking away an adequate bicycle facility, TDOT needs to replace that facility with a form of bicycle and pedestrian accommodation that's safest in the context of a grade-separated, limited-access facility, which is a greenway. In the past, TDOT has committed to building two greenway segments, described above, to complete the connection from the Knoxville greenway system to the Knox/Blount County line. Those segments need to be included in this project.	
3-62, 63	3.17	These pages contain several descriptions of greenway segments to be built by Knox County, City of Knoxville and TDOT that are inaccurate. Figure 2-8 on Page 3.64 correctly depicts the greenway segments to be built in the vicinity of SR-115. Phase 1 and 2 are funded separately and are to be built by City of Knoxville and Knox County, respectively. TDOT Piece 1 and TDOT Piece 2 are to be built by TDOT as part of this SR-115 project.	